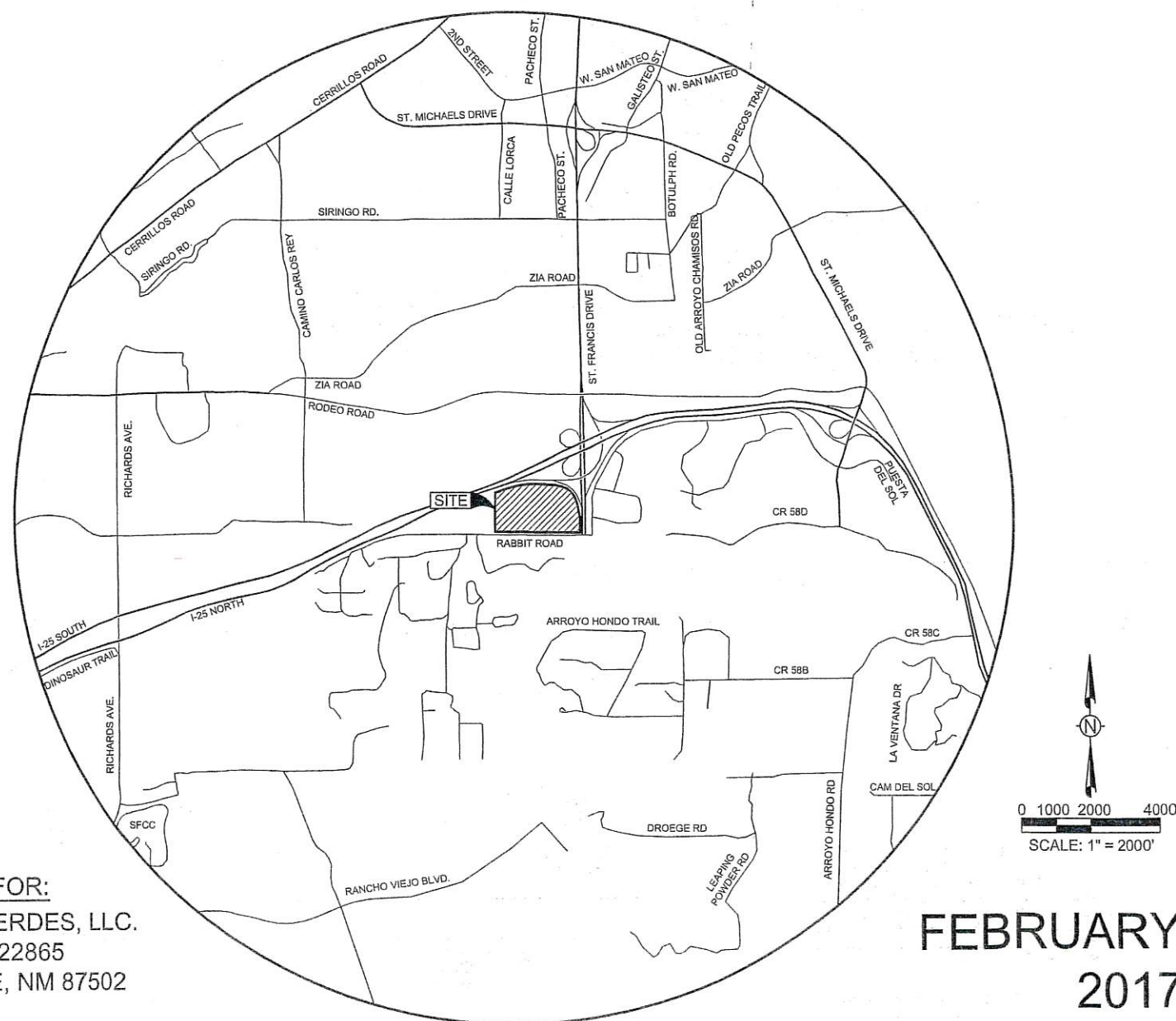


PRELIMINARY PLAT SUBMITTAL FOR ST. FRANCIS SOUTH, PHASES 1 TO 4

FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH, PHASES 1 & 2



PREPARED FOR:
VEGAS VERDES, LLC.
P.O. BOX 22865
SANTA FE, NM 87502

FEBRUARY
2017

SANTA FE COUNTY, NEW MEXICO
SECTION 11, TOWNSHIP 16 N, RANGE 9 E NMPM

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11-4 TO 11-7	STRUCTURE SECTIONS
12-1 TO 12-6	STORM WATER POLLUTION PREVENTION PLANS
13-1 TO 13-8	STANDARD DRAWINGS AND DETAILS

CASE #

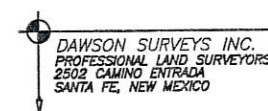
COUNTY LAND USE ADMINISTRATOR DATE

SFCO UTILITIES DATE

COUNTY PUBLIC WORKS DIRECTOR DATE

RURAL ADDRESSING DATE

COUNTY FIRE MARSHALL DATE



COUNTY OF SANTA FE } SS
STATE OF NEW MEXICO }
I hereby certify that this instrument was filed
for record on the _____ day of _____ A.D.,
20____, at _____ o'clock _____ m.
and was duly recorded in book _____ of the records of
page _____ Santa Fe County

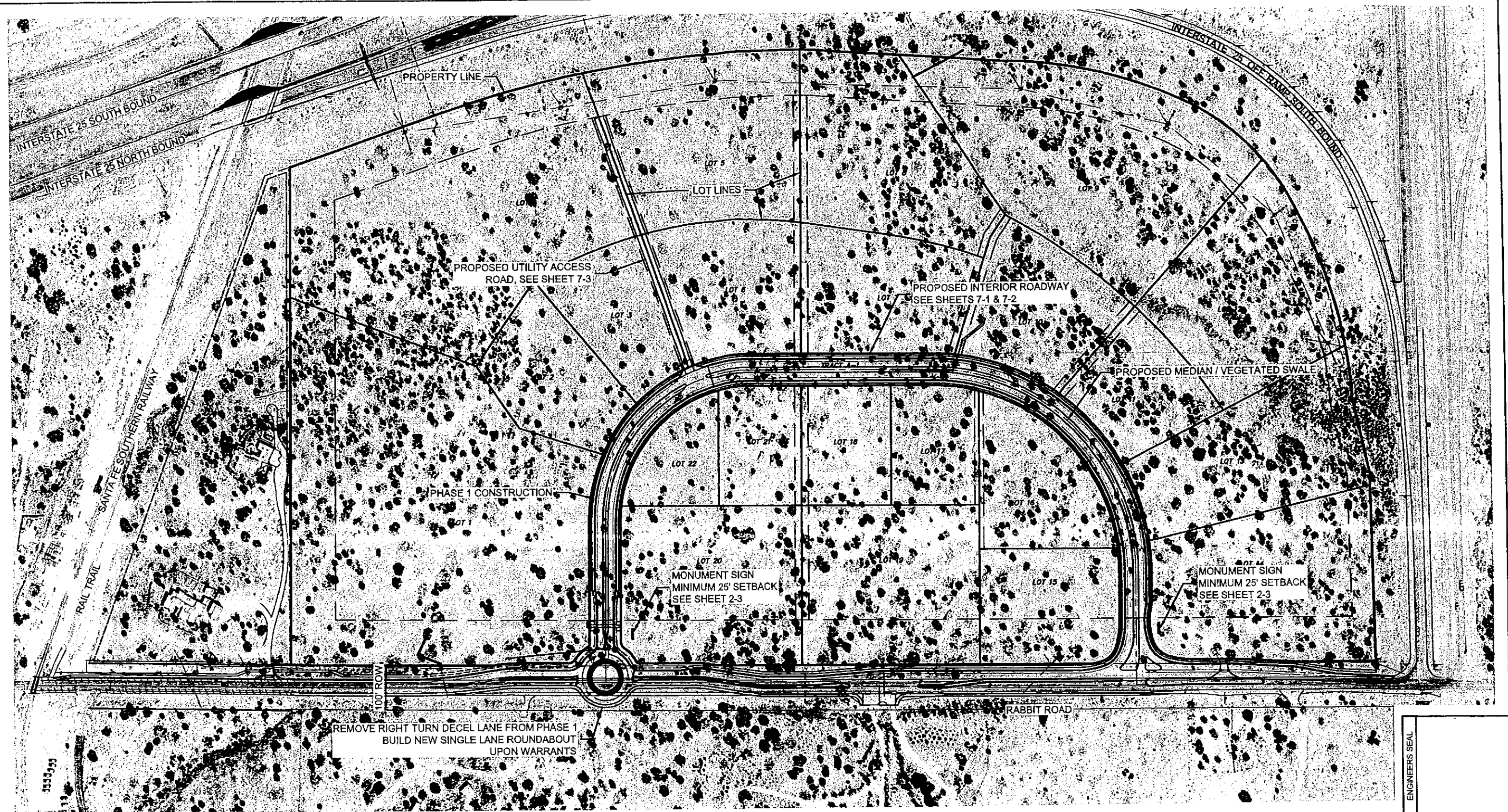
Witness my Hand and Seal of Office
Valerie Espinoza
County Clerk, Santa Fe County, N.M.

Santa Fe Engineering
Consultants, LLC
1599 St. Francis Drive, Suite B
Santa Fe, N. M. 87505
(505) 982-2845 Fax (505) 982-2641
<http://www.SFENGR.com>

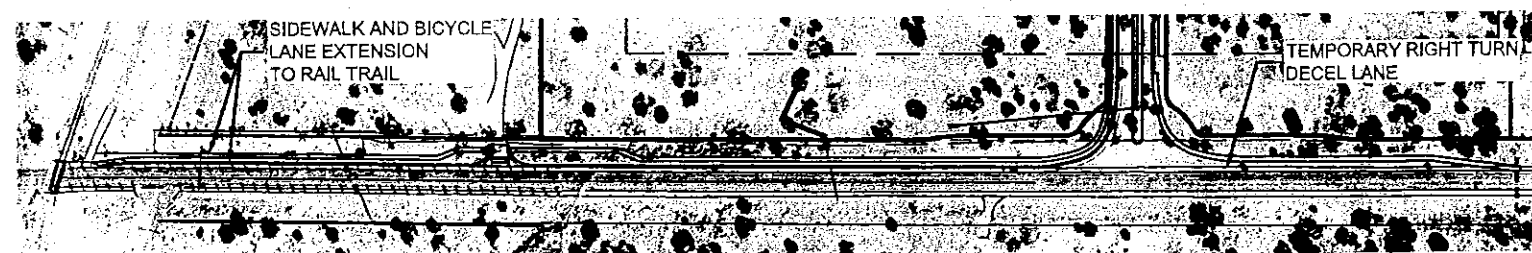
EXHIBIT

2

SHEET 1-1

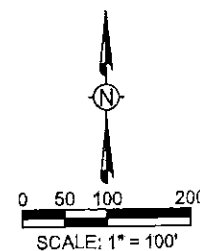


SITE PLAN - ULTIMATE CONDITION
SCALE: 1" = 100'



RABBIT ROAD - PHASE 1 DETAIL
SCALE: 1" = 100'

AERIAL PHOTOGRAPHY FROM
SANTA FE COUNTY GIS, DATED 2011



NOTES:

1. LEFT TURN LANES ARE TO BE PROVIDED IF REQUIRED, BASED UPON THE ACTUAL TRAFFIC VOLUMES FOR THE PROPOSED LAND USE.
2. MEDIAN DROP INLETS ARE TO BE INSTALLED IN THE MEDIAN / VEGETATED SWALE TO PROVIDE MAXIMUM INFILTRATION.

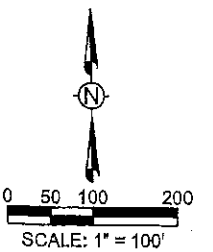
ENGINEER'S SEAL	
DATE	
REVISIONS	

SFE C Santa Fe Engineering
Consultants, LLC
1599 St. Francis Drive, Suite B
Santa Fe, N. M. 87505
(505) 982-2845 Fax (505) 982-2641
<http://www.SFENGR.com>

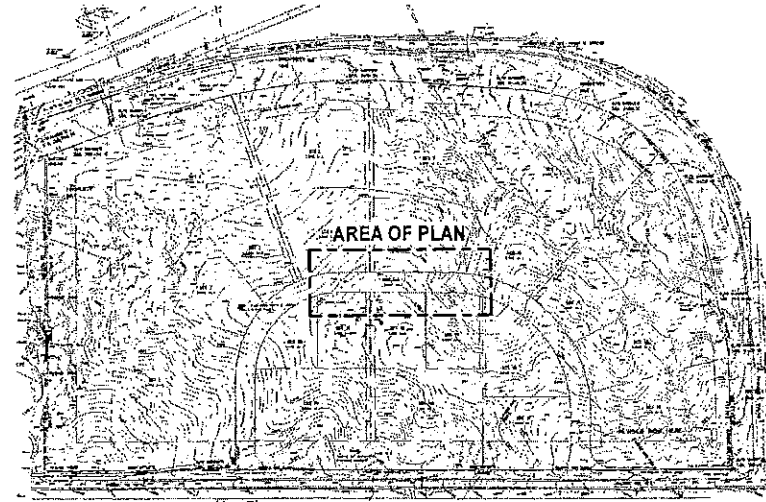
PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

SITE PLAN

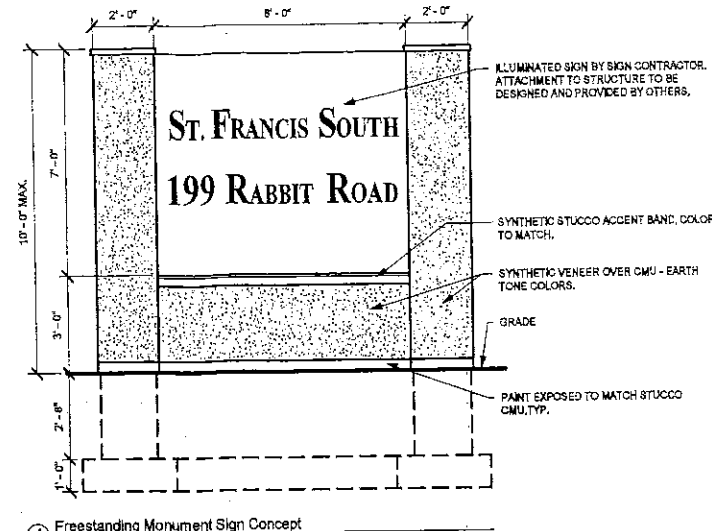
DATE	SCALE	SHEET
FEBRUARY 2017	1" = 100'	2-1



DATE: FEBRUARY 2017	SCALE: 1"=100'	SHEET: 2-2
------------------------	-------------------	---------------



③ Key Plan
1" = 300'-0"



④ Freestanding Monument Sign Concept
3/8" = 1'-0"

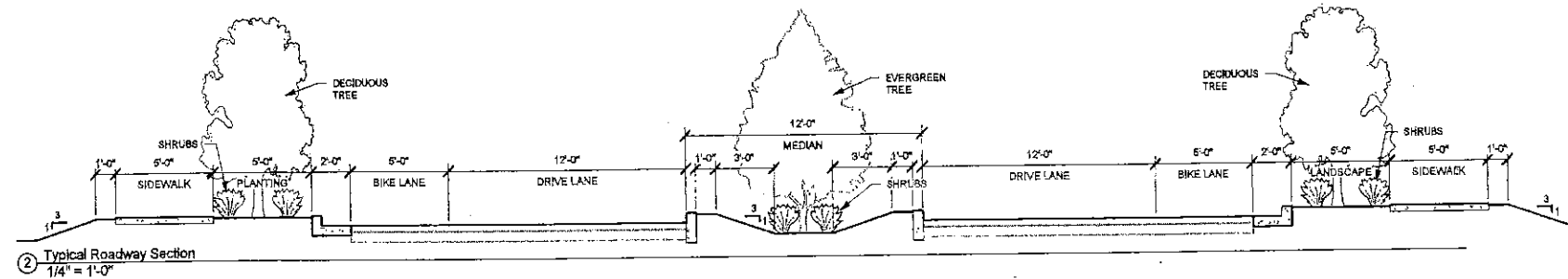
NOTE: ACTUAL SIGN DESIGN TO BE DETERMINED AT TIME OF SIGN PERMIT

PLANT LEGEND

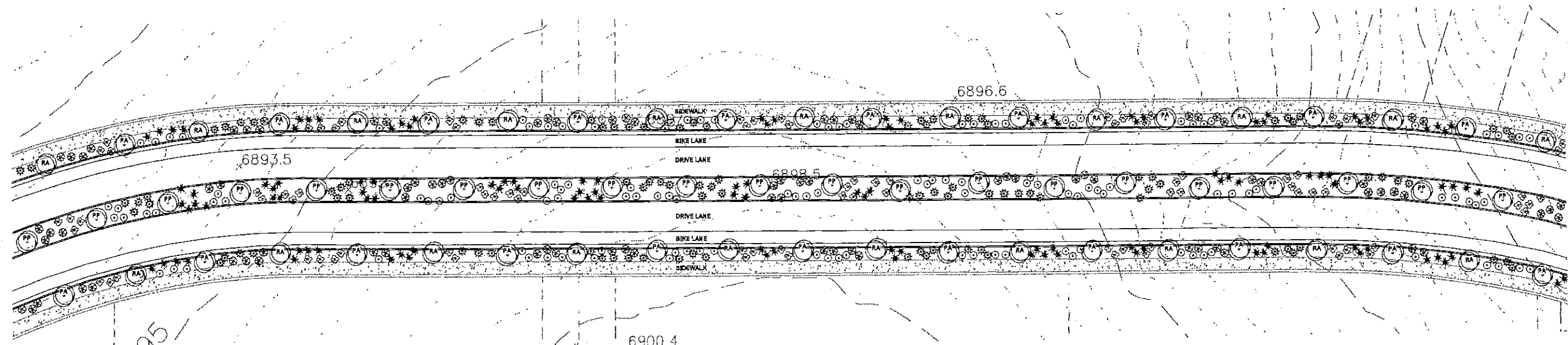
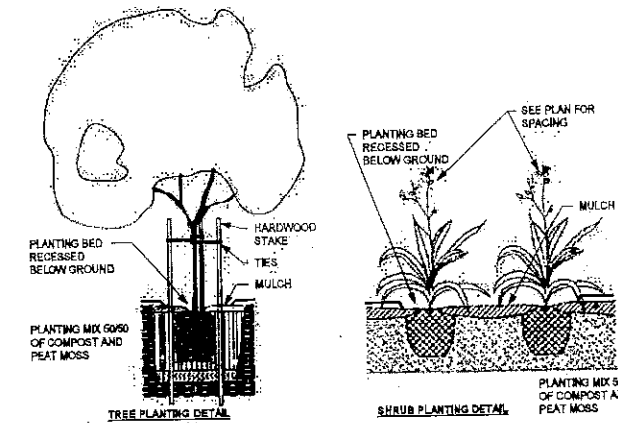
SYMBOL	SCIENTIFIC	COMMON	SIZE
TREES:			
RA	FRAXINUS	RAYWOOD ASH	1 1/2" GAL / 8'
PA	FRAXINUS AMERICANA 'AUTUMN PURPLE'	AUTUMN PURPLE ASH	1 1/2" GAL / 8'
PP	PINUS EDULIS	PINON PINE	1 1/2" GAL / 8'
SHRUBS / GROUND COVER:			
	PINUS MUGO	MUGO PINE	1 1/2" GAL / 24"
	RHUS AROMATICA	GRO LOW SUMAC	1 1/2" GAL / 24"
	PEROVSKIA	RUSSIAN SAGE	1 1/2" GAL / 24"
	RHUS TRILOBATA	3 LEAF SUMAC	1 1/2" GAL / 24"
	LAVANDULA	LAVENDER	1 1/2" GAL / 24"

NOTES:

- ALL TREES AND SHRUBS TO BE IRRIGATED WITH AN AUTOMATIC DRIP IRRIGATION SYSTEM. ALL LINES MUST BE UNDERGROUND. ALL IRRIGATION VALVES AND EQUIPMENT MUST BE HOUSED IN BELOW GROUND VAULTS.
- ALL PLANTER BEDS TO HAVE WEED BARRIER FABRIC AND MIN. 2" BARK MULCH.
- IN ACCORDANCE WITH THE PROVISIONS OF SDCG SECTION 7.8.4.1, A LANDSCAPE BUFFER SHALL BE PROVIDED ALONG THE RABBIT ROAD RIGHT-OF-WAY AT THE TIME LOTS 1, 14, 15, 19, AND 20 ARE DEVELOPED.
- ALL DISTURBED AREAS WILL BE RESEED WITH NATIVE GRASS.



② Typical Roadway Section
1/4" = 1'-0"



① Typical Roadway Segment Landscape Plan
1" = 20'-0"

St. Francis South Landscape and Signage Plan

Date
Issue Date

JENKINS GAVIN
LAND USE/PROJECT MANAGEMENT



Landscape Plan

Scale As Indicated

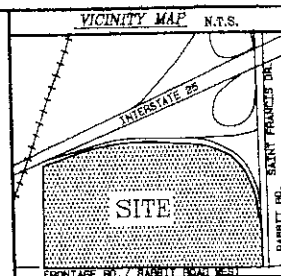
Surveyors Certificate

I HEREBY CERTIFY THAT THIS SURVEY PLAT AND THE NOTES SHOWN HEREON WERE PREPARED BY ME OR UNDER MY PERSONAL DIRECTION AND ARE A TRUE & ACCURATE REPRESENTATION OF A FIELD SURVEY WHICH WAS COMPLETED ON 2/5/2008. TO THE BEST OF MY INFORMATION, KNOWLEDGE AND BELIEF, THIS PLAT MEETS OR EXCEEDS THE "MINIMUM STANDARDS FOR LAND SURVEYING IN NEW MEXICO." I FURTHER CERTIFY THAT THIS SURVEY IS NOT A LAND DIVISION OR SUBDIVISION AS DEFINED IN THE NEW MEXICO SUBDIVISION ACT AND THAT THIS IS A BOUNDARY SURVEY PLAT OF AN EXISTING TRACT OR TRACTS.

DEAN L. SHRAEDER NEW MEXICO PROFESSIONAL SURVEYOR No. 12451



INTERSTATE 25
ACCESS CONTROLLED R/W
I-025-5(36)272



BOUNDARY SURVEY PLAT of TRACT A 6740037 on RABBIT ROAD WEST

LYING & BEING SITUATE WITHIN
THE NW 1/4 SW 1/4, NE 1/4 SW 1/4 OF
SECTION 11, T 16 N, R 9 E, NMPM
SANTA FE COUNTY, NEW MEXICO

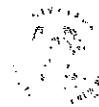
Containing 68.944 Acres ±

NOTES

- 1) BASIS OF BEARINGS IS TAKEN FROM GPS OBSERVATION; WGS 84
- 2) REFER TO WARRANTY DEEDS, SAID "DEEDS" FOR PARCELS 8-9-A & 8-9-B WERE ACQUIRED AT THE NEW MEXICO HIGHWAY & TRANSPORTATION DEPARTMENT. THIS SURVEYOR DID NOT FIND AND WAS NOT SUPPLIED WITH A RECORDED COPY OF SAID DEEDS.
- 3) REFER TO "NEW MEXICO STATE HIGHWAY COMMISSION RIGHT OF WAY MAP" NEW MEXICO PROJECT NO. I-025-5(36)272, SHEETS 8, 8A, 8B, 9 & 9B OF 18. DATA FROM SAID MAP SHOWN IN SINGLE () PARENTHESES.

FLOOD NOTE:

TRACT A LIES WITHIN ZONE X; AREAS DETERMINED TO BE OUTSIDE THE 500 YEAR FLOOD PLAIN; REFER TO FEMA FIRM PANEL No. 350069 02508 DATED 11/4/88.

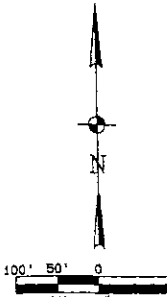
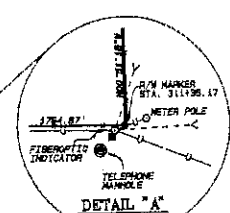


INSTAFF
1514600

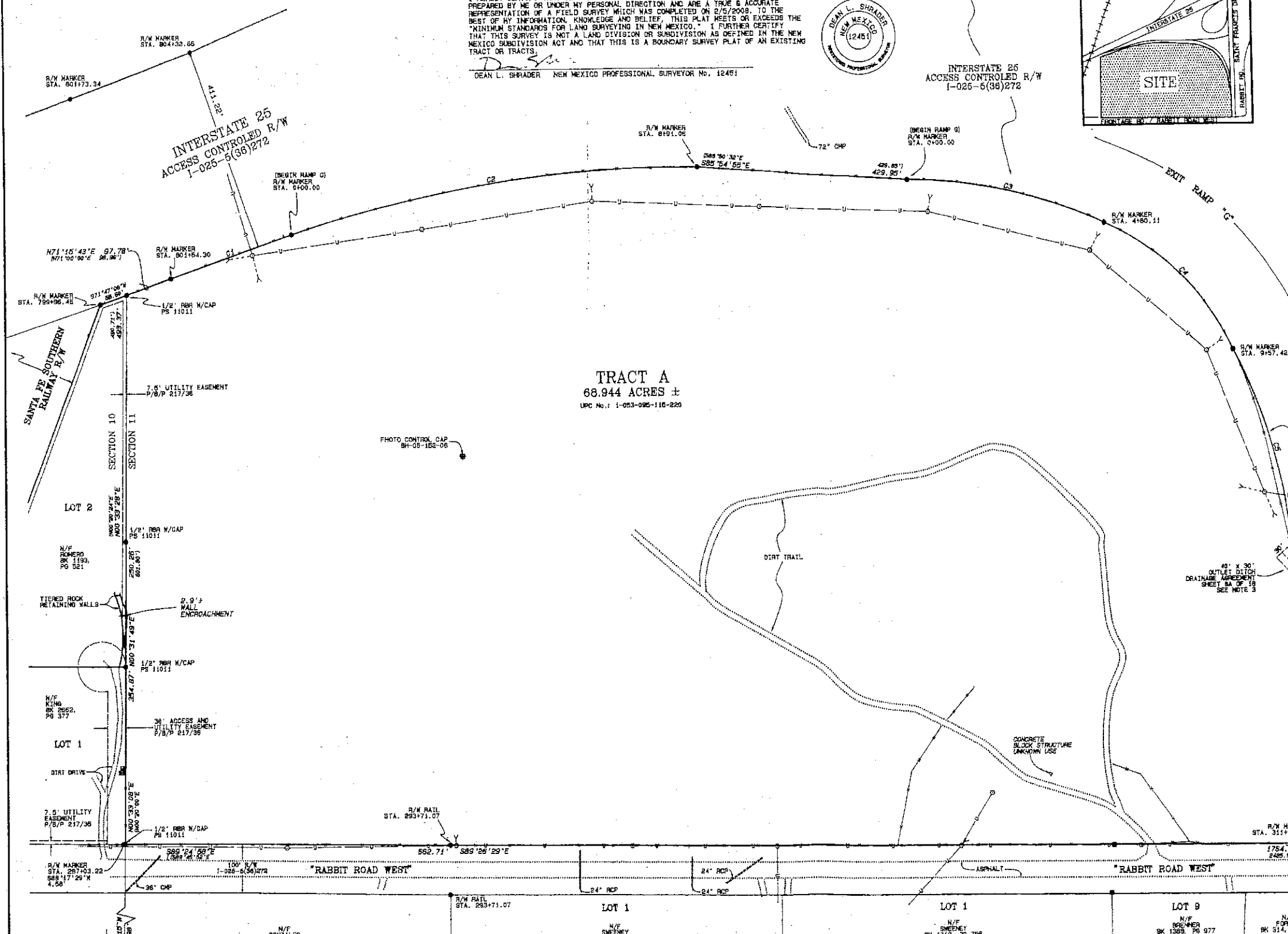
COUNTY OF SANTA FE
STATE OF NEW MEXICO
I hereby certify that this instrument was filed for record on the 16 day of Feb A.D. 2008, at 1:44 o'clock P.M. and was duly recorded in book 674 of the records of SANTA FE COUNTY.

Witness my Hand and Seal of Office
Patricia Espinoza
County Clerk, Santa Fe County, N.M.
Gretchen Romero
Deputy

ST. FRANCIS DRIVE



TRACT A
68.944 ACRES ±
UPC No.: I-053-095-110-220



BOUNDARY DATA

CURVE	CHORD	BEARING	ARC	DELTA	RADIUS	CHORD
G1		N70°57'47"E	265.42'	0°52'02"	17330.74'	262.42'
G2		N81°09'52"E	846.55'	19°21'08"	2506.42'	842.55'
G3		S77°20'48"E	415.45'	23°57'37"	995.92'	413.45'
G4		S45°08'12"E	370.85'	40°32'57"	524.07'	363.20'
G5		S16°22'58"E	420.26'	24°38'14"	995.92'	424.95'

RECORD PLAT DATA (DEED OF NOTE 2)

CURVE	ARC	DELTA	RADIUS	CHORD
G1	84.25'	0°52'02"	17330.74'	262.42'
G2	84.25'	19°21'08"	2506.42'	842.55'
G3	415.45'	23°57'37"	995.92'	413.45'
G4	370.85'	40°32'57"	524.07'	363.20'
G5	420.26'	24°38'14"	995.92'	424.95'

LEGEND

- FOUND POINT
- ELECTRIC METER
- ELECTRIC TRANSFORMER
- TELEPHONE PEDESTAL
- COMMUNICATIONS MANHOLE
- ROAD OR DRIVE
- CULVERT
- BARBED WIRE FENCE
- UTILITY POLE, OVERHEAD LINES & ANCHOR GUY

PUBLIC NOTICE:
THE SANTA FE COUNTY LAND USE ADMINISTRATOR HAS NOT REVIEWED THIS PLAT OF SURVEY BEFORE ITS FILING IN THE OFFICE OF THE COUNTY CLERK. THIS PLAT IS NOT BEING FILED FOR THE PURPOSE OF CREATING A SUBDIVISION OR NEW LOTS, ALTERING THE BOUNDARIES OF ANY EXISTING LOTS OR FOR THE PURPOSE OF "DEVELOPMENT" AS THE TERM IS USED IN THE SANTA FE COUNTY LAND DEVELOPMENT CODE. EXTRAJURISDICTIONAL ZONING ORDINANCE OR EXTRAJURISDICTIONAL REGULATIONS. THIS STATEMENT DOES NOT IN ANYWAY REPRESENT OFFICIAL COUNTY APPROVAL OF THIS PLAT.

INDEXING INFORMATION FOR COUNTY CLERK

OWNER	SECTION	TOWNSHIP	RANGE	ADDRESS
B & S LTD PARTNERSHIP	SEC. 11	T 16 N	R 9 E	190 RABBIT RD. WEST

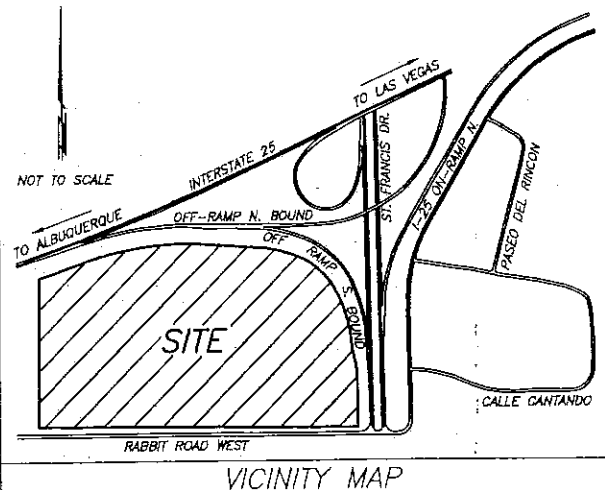
HIGH DESERT SURVEYING, INC.
PROFESSIONAL SURVEYING

1925 ASPEN DRIVE, SUITE 401
SANTA FE, NM. 87505
PHONE: (505) 438-8094
FAX: (505) 424-1709

DWG. NAME: BOUNDARY FOR TRACT A

PROJECT No. 07007

SHEET 3-1



FLOOD ZONE:

THIS PROPERTY IS LOCATED WITHIN OTHER AREAS ZONES "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS SHOWN ON FIRM PANEL 3504900414E, MAP REVISED DECEMBER 04, 2012.

REFERENCE DOCUMENTS:

BOUNDARY SURVEY PLAT OF TRACT A ON RABBIT ROAD WEST, PREPARED BY DEAN L. SHRAIDER NMPLS No. 12451, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK AS PLAT BOOK 674, PAGE 037.

NOTES:

THIS PLAT IS SUBJECT TO ANY RESTRICTIONS, COVENANTS AND EASEMENTS OF RECORD.

LEGEND:

BEARINGS ARE REFERRED TO THE NEW MEXICO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE. DISTANCES SHOWN ARE GROUND LEVEL REFERRED TO 6800' AMSL DATUM. TO CONVERT DISTANCES SHOWN TO GRID DISTANCES MULTIPLY BY A SCALE FACTOR OF 0.999598.

- DENOTES FOUND MONUMENT AS SHOWN
- DENOTES REBAR TO BE SET UPON RECORDING
- DENOTES CALCULATED POINT NOT SET
- DENOTES UTILITY POLE WITH ANCHOR
- OH — DENOTES OVERHEAD UTILITY LINES
- DENOTES EXISTING BARBED WIRE FENCE
- DENOTES BOUNDARY OF LANDS DEALT WITH BY THIS PLAT. INFORMATION OUTSIDE THIS BOUNDARY IN BACKGROUND INFORMATION ONLY, NOT VERIFIED FOR ACCURACY AND IS NOT PART OF THIS PLAT.
- /// DENOTES OPEN SPACE (13.747 Ac±)
- /// DENOTES DRAINAGE EASEMENT GRANTED THIS PLAT

SURVEYORS CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE REPRESENTATION OF A SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN DECEMBER 2016. TO THE BEST OF MY KNOWLEDGE, THE SURVEY AND PLAT ARE CORRECT, TRUE AND MEET THE MINIMUM STANDARDS FOR LAND SURVEYS IN NEW MEXICO.

PRELIMINARY FOR REVIEW

BARRY S. PHILLIPS, N.M.P.L.S. 15517

SANTA FE COUNTY APPROVAL NOTES AND CONDITIONS:

- 1) BUILDING SITES AND DRIVEWAY LOCATIONS ARE SUBJECT TO THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS.
- 2) PRIOR TO THE ISSUANCE OF BUILDING PERMITS, THE SANTA FE COUNTY FIRE MARSHALL MUST FIND THE ACCESS ROADS ACCEPTABLE FOR EMERGENCY VEHICLE ACCESS AND THE FIRE HYDRANTS MUST BE ACCEPTABLE AND OPERABLE.
- 3) THESE LOTS ARE SUBJECT TO SANTA FE COUNTY FIRE AND RESCUE IMPACT FEES AT THE TIME OF BUILDING PERMIT. SUCH FEES SHALL BE PAID BY THE PARTY SEEKING THE BUILDING PERMIT.
- 4) ALL UTILITY LINES SHALL BE PLACED UNDERGROUND. UPON THE COMPLETION OF THE INSTALLATION OR REPAIR OF THE SAID UTILITIES, THE GRADE OF THE UTILITY EASEMENT SHALL BE RESTORED BY THE UTILITY COMPANY TO ITS CONDITION PRIOR TO SAID INSTALLATION OR REPAIR TO ALLOW FOR THE NATURAL DRAINAGE OF STORM WATERS. HOWEVER, TEMPORARY, ABOVE GROUND UTILITIES, NOT TO EXCEED ONE YEAR SHALL BE ALLOWED.
- 5) THESE LOTS ARE SUBJECT TO SECTION 7.17, OF THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS AT THE TIME OF LOT DEVELOPMENT.
- 6) THE APPROVAL OF THIS PLAT DOES NOT CONSTITUTE THE APPROVAL OF ANY FURTHER DEVELOPMENT, INCLUDING BUILDING PERMITS.
- 7) THIS PLAT LIES WITHIN THE PLANNING AND PLATTING JURISDICTION OF THE COUNTY OF SANTA FE, NEW MEXICO.
- 8) SANTA FE COUNTY'S APPROVAL OF THIS PLAT DOES NOT INCLUDE THE CONSTRUCTION OF THE PRIVATE EASEMENTS OR ROADS AS SHOWN. PRIOR TO CONSTRUCTION OF SAID PRIVATE EASEMENTS OR ROADS, IT IS REQUIRED THAT AN ADDITIONAL DEVELOPMENT PERMIT BE APPLIED FOR AND APPROVED BY THE SANTA FE COUNTY LAND USE ADMINISTRATOR.

COUNTY APPROVAL:

APPROVED BY THE BOARD OF COUNTY COMMISSIONERS OF SANTA FE COUNTY AT THEIR MEETING OF _____

CHAIRMAN _____ DATE _____
ATTESTED BY _____ COUNTY CLERK _____

COUNTY REVIEW:

COUNTY LAND USE ADMINISTRATOR _____ DATE _____
DEVELOPMENT PERMIT # _____
COUNTY PUBLIC WORKS _____ DATE _____
COUNTY FIRE MARSHALL _____ DATE _____
COUNTY RURAL ADDRESSING _____ DATE _____
COUNTY WATER UTILITY _____ DATE _____

UTILITY COMPANIES:

CENTURY LINK COMMUNICATIONS _____ DATE _____
COMCAST CABLE _____ DATE _____

IN APPROVING THIS PLAT, PNM ELECTRIC SERVICES DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, PNM DOES NOT WAIVE NOR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLATS, REPLATS OR OTHER DOCUMENTS WHICH ARE NOT SHOWN ON THIS PLAT.

PUBLIC SERVICE CO. OF NEW MEXICO _____ DATE _____

IN APPROVING THIS PLAT, NM GAS COMPANY DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, NM GAS COMPANY DOES NOT WAIVE NOR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLATS, REPLATS OR OTHER DOCUMENTS WHICH ARE NOT SHOWN ON THIS PLAT.

NM GAS COMPANY _____ DATE _____

DEDICATION AND AFFIDAVIT:

KNOW ALL MEN BY THESE PRESENTS, THAT THE UNDERSIGNED OWNERS OF THOSE LANDS LYING WITHIN SANTA FE COUNTY CONTAINING AN AREA OF 68.932 ACRES, MORE OR LESS, BEING A PART OF SECTION 11, T16N, R9E, N.M.P.M., HAS CAUSED THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAT AND THAT THE SAID SUBDIVISION IS NAMED AND SHALL BE KNOWN AS "ST. FRANCIS SOUTH". ALL THAT APPEARS ON THIS PLAT IS MADE WITH THE FREE CONSENT, AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS.

THE DRAINAGE, ACCESS AND UTILITY EASEMENTS SHOWN HEREON ARE HEREBY GRANTED FOR THE CONSTRUCTION, MAINTENANCE, REPAIR AND OPERATION OF UTILITIES. THE EASEMENTS ARE SUBJECT TO THE PROVISIONS ON THIS PLAT.

THE INTERNAL LOT ACCESS ROADWAY ROW, TRACT A-1, SHALL BE OWNED AND MAINTAINED BY THE ST. FRANCIS SOUTH LOT OWNERS ASSOCIATION.

PUBLIC UTILITY EASEMENTS SHOWN ON THIS PLAT ARE GRANTED FOR THE COMMON AND JOINT USE OF SANTA FE COUNTY FOR THE OWNERSHIP AND MAINTENANCE OF THE WATER AND WASTEWATER COLLECTION SYSTEM SERVING THE SUBDIVISION, INCLUDING SEWER SERVICE LATERALS TO THE RESIDENTIAL LOT PROPERTY LINES.

VEGAS VERDE, LLC.

BY: DAVID GURULE, PRESIDENT

STATE OF NEW MEXICO
COUNTY OF SANTA FE

THE FOREGOING WAS SWORN, ACKNOWLEDGED AND SUBSCRIBED BEFORE ME BY
DAVID GURULE, PRESIDENT OF VEGAS VERDES, LLC
THIS _____ DAY OF _____, 2017

NOTARY PUBLIC MY COMMISSION EXPIRES _____

"TITLE AND INDEXING INFORMATION FOR COUNTY CLERK"

PRELIMINARY SUBDIVISION PLAT PREPARED FOR
VEGAS VERDES, LLC
CREATING
ST. FRANCIS SOUTH
BEING A SUBDIVISION OF TRACT A, PLAT BOOK 674 PAGE 037
WITHIN SECTION 11, T16N, R9E, N.M.P.M., SANTA FE COUNTY, NEW MEXICO

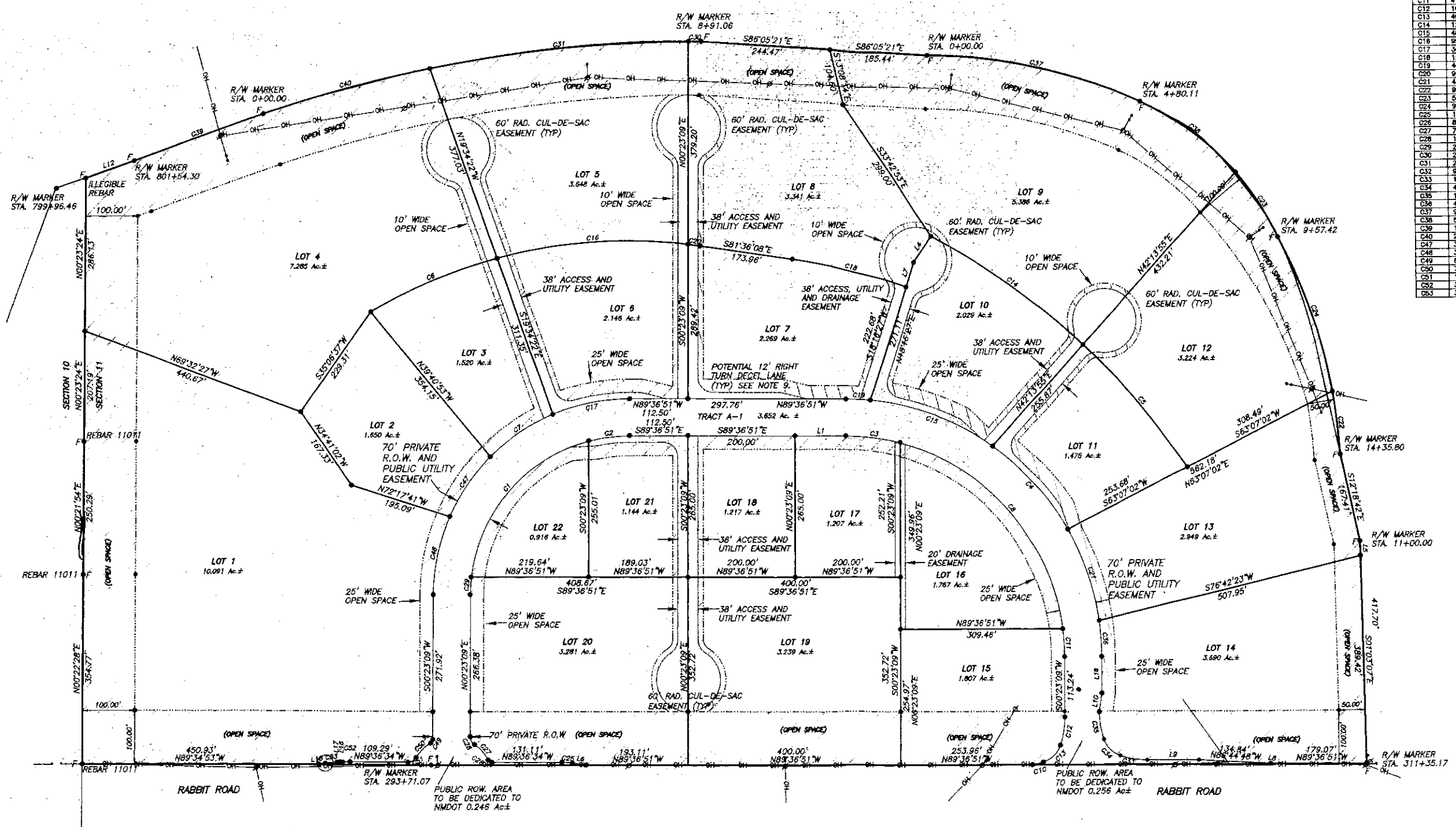
PURPOSE OF THIS PLAT:
TO CREATE 22 RESIDENTIAL LOTS, WITH OPEN SPACE WITHIN LOTS, AND 1
RIGHT OF WAY TRACT, BEING PART OF THE COUNTY APPROVED ST.
FRANCIS SOUTH MASTER PLAN.

UPC: 1-053-095-115-220

DAWSON SURVEYS INC.
PROFESSIONAL LAND SURVEYORS
2502 CAMINO ENTRADA
SANTA FE, NEW MEXICO
FILE# 93755UBD DATE: 1-19-17

SHEET 1 OF 2

SHEET 3-2



CURVE	RADIUS	ARC LENGTH	CHORD LENGTH	CHORD BEARING	DELTA ANGLE
C1	298.00	367.53	336.55	N41°07'27"E	68°45'40"
C2	298.00	77.40	77.18	N82°56'43"E	14°52'52"
C3	415.00	103.30	103.03	S82°29'00"E	14°18'43"
C4	485.00	211.89	210.21	N41°38'18"W	25°01'58"
C5	1296.91	302.67	302.28	S32°53'37"E	132°14'42"
C6	953.03	280.36	280.55	N67°26'31"E	15°38'09"
C7	368.00	142.65	141.78	S56°14'43"W	27°12'35"
C8	415.00	496.19	467.15	S41°05'58"E	86°30'18"
C10	169.50	19.38	19.35	S79°31'36"W	6°32'45"
C11	415.00	52.39	52.36	S03°13'51"E	7°13'59"
C12	169.50	53.83	53.60	S09°28'59"W	18°11'42"
C13	49.51	46.37	44.89	S49°25'03"W	53°39'46"
C14	1296.91	354.72	353.61	S54°27'58"E	15°40'59"
C15	485.00	253.99	250.71	N69°08'07"W	29°57'08"
C16	953.03	385.95	363.71	N86°18'07"E	22°00'04"
C17	368.00	147.95	146.96	S78°52'05"W	23°02'08"
C18	1219.44	224.21	223.89	S70°03'31"E	10°32'04"
C19	485.00	46.44	46.42	N86°52'17"W	5°29'10"
C20	953.03	22.61	22.61	S82°02'42"E	1°22'17"
C21	485.00	182.24	181.17	N16°22'28"W	21°31'45"
C22	995.92	119.37	119.30	S06°39'19"E	6°52'04"
C23	524.07	145.13	144.67	S32°53'39"E	15°52'00"
C24	995.92	308.86	307.63	S19°58'23"E	17°46'09"
C25	140.00	19.56	19.56	N85°36'12"W	6°00'45"
C26	8.00	6.77	8.33	N58°13'16"W	62°48'54"
C27	77.50	39.32	38.90	N41°22'06"W	29°04'16"
C28	18.00	17.68	16.88	N27°45'33"W	58°17'23"
C29	298.00	33.07	33.05	N03°33'53"E	6°21'28"
C30	2506.42	24.48	24.48	S89°38'50"E	0°33'35"
C31	2506.42	494.91	494.10	N84°28'13"E	1°11'48"
C32	952.90	365.95	363.71	S86°18'10"W	22°00'16"
C33	168.00	53.35	53.15	N80°27'12"W	18°11'42"
C34	48.00	44.86	43.24	N44°34'57"W	53°32'48"
C35	168.00	55.65	52.44	N06°19'52"W	1°57'23"
C36	485.00	67.68	67.62	N03°38'42"W	7°59'42"
C37	995.92	416.42	413.40	S77°31'59"E	2°57'25"
C38	524.07	225.71	223.97	S53°11'57"E	2°40'36"
C39	17338.74	262.42	262.42	N70°45'04"E	0°52'02"
C40	2506.42	327.15	326.83	N75°24'27"E	7°28'44"
C41	368.00	136.64	135.96	S34°30'12"W	31°18'56"
C42	368.00	150.81	149.76	S12°07'34"W	23°28'50"
C43	8.00	8.35	7.98	S30°17'23"W	58°48'29"
C44	77.50	38.01	38.60	S45°46'21"W	28°50'33"
C45	18.00	18.55	17.74	S80°52'15"W	58°02'22"
C46	368.00	13.47	13.47	S89°20'11"W	2°05'29"
C47	368.00	22.75	22.75	S84°49'04"W	5°33'42"

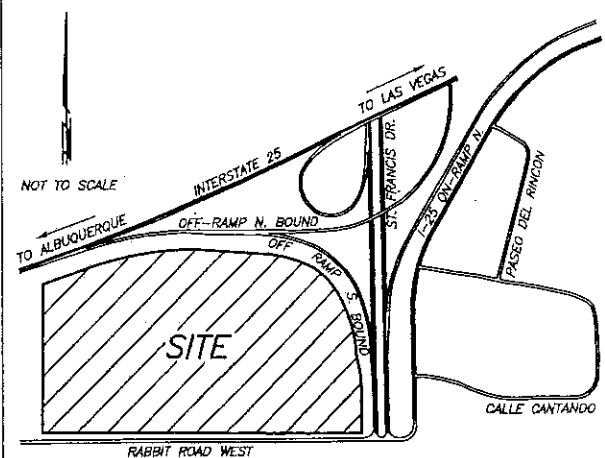
LINE	BEARING	DISTANCE
L1	S89°36'51"E	97.78
L2	N34°44'51"E	56.77
L3	S01°03'07"E	27.28
L4	N81°35'50"W	25.99
L5	S15°16'27"W	48.43
L6	N79°37'17"W	2.41
L7	N89°33'02"W	96.08
L8	N09°30'17"E	35.38
L9	N00°23'09"E	68.65
L10	N71°11'21"E	97.78
L11	S87°51'51"W	5.34
L12	S87°01'21"W	3.41
L13	S81°05'30"W	11.81

LEGEND:
 BEARINGS ARE REFERRED TO THE NEW MEXICO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE. DISTANCES SHOWN ARE GROUND LEVEL REFERRED TO 6600' AMSL DATUM. TO CONVERT DISTANCES SHOWN TO GRID DISTANCES MULTIPLY BY A SCALE FACTOR OF 0.999599.

- DENOTES FOUND MONUMENT AS SHOWN
- DENOTES REBAR TO BE SET UPON RECORDING
- DENOTES CALCULATED POINT NOT SET
- DENOTES UTILITY POLE WITH ANCHOR
- OH — DENOTES OVERHEAD UTILITY LINES
- DENOTES EXISTING BARBED WIRE FENCE
- DENOTES OPEN SPACE (20.999 Ac±)
- DENOTES DRAINAGE EASEMENT GRANTED THIS PLAT

PRELIMINARY FOR REVIEW

DAWSON SURVEYS INC.
 PROFESSIONAL LAND SURVEYORS
 2502 CAMINO ENTRADA
 SANTA FE, NEW MEXICO
 FILE# 9375SUB0 DATE: 8-30-17



VICINITY MAP

FLOOD ZONE:

THIS PROPERTY IS LOCATED WITHIN OTHER AREAS ZONES "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS SHOWN ON FIRM PANEL 35049C04-14E, MAP REVISED DECEMBER 04, 2012.

REFERENCE DOCUMENTS:

BOUNDARY SURVEY PLAT OF TRACT A ON RABBIT ROAD WEST, PREPARED BY DEAN L. SHRAIDER N.M.P.L.S. No. 12451, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK AS PLAT BOOK 674, PAGE 037.

NOTES:

THIS PLAT IS SUBJECT TO ANY RESTRICTIONS, COVENANTS AND EASEMENTS OF RECORD.

LEGEND:

BEARINGS ARE REFERRED TO THE NEW MEXICO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE. DISTANCES SHOWN ARE GROUND LEVEL REFERRED TO 6800' AMSL DATUM. TO CONVERT DISTANCES SHOWN TO GRID DISTANCES MULTIPLY BY A SCALE FACTOR OF 0.999598.

- DENOTES FOUND MONUMENT AS SHOWN
- DENOTES REBAR TO BE SET UPON RECORDING
- DENOTES CALCULATED POINT NOT SET
- DENOTES UTILITY POLE WITH ANCHOR
- DENOTES OVERHEAD UTILITY LINES
- DENOTES EXISTING BARBED WIRE FENCE
- DENOTES OPEN SPACE (13.747 Ac±)

SURVEYORS CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE REPRESENTATION OF A SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN DECEMBER 2016. TO THE BEST OF MY KNOWLEDGE, THE SURVEY AND PLAT ARE CORRECT, TRUE AND MEET THE MINIMUM STANDARDS FOR LAND SURVEYS IN NEW MEXICO.

PRELIMINARY FOR REVIEW

BARRY S. PHILLIPS, N.M.P.L.S. 16517

VEGAS VERDE, LLC.

BY: DAVID GURULE, PRESIDENT

STATE OF NEW MEXICO
COUNTY OF SANTA FE

THE FOREGOING WAS SWORN, ACKNOWLEDGED AND SUBSCRIBED BEFORE ME BY DAVID GURULE, PRESIDENT OF VEGAS VERDES, LLC
THIS _____ DAY OF _____, 2013

NOTARY PUBLIC MY COMMISSION EXPIRES _____

SANTA FE COUNTY APPROVAL NOTES AND CONDITIONS:

- 1) BUILDING SITES AND DRIVEWAY LOCATIONS ARE SUBJECT TO THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS.
- 2) PRIOR TO THE ISSUANCE OF BUILDING PERMITS, THE SANTA FE COUNTY FIRE MARSHALL MUST FIND THE ACCESS ROADS ACCEPTABLE FOR EMERGENCY VEHICLE ACCESS AND THE FIRE HYDRANTS MUST BE ACCEPTABLE AND OPERABLE.
- 3) THESE LOTS ARE SUBJECT TO SANTA FE COUNTY FIRE AND RESCUE IMPACT FEES AT THE TIME OF BUILDING PERMIT. SUCH FEES SHALL BE PAID BY THE PARTY SEEKING THE BUILDING PERMIT.
- 4) ALL UTILITY LINES SHALL BE PLACED UNDERGROUND, UPON THE COMPLETION OF THE INSTALLATION OR REPAIR OF THE SAID UTILITIES, THE GRADE OF THE UTILITY EASEMENT SHALL BE RESTORED BY THE UTILITY COMPANY TO ITS CONDITION PRIOR TO SAID INSTALLATION OR REPAIR TO ALLOW FOR THE NATURAL DRAINAGE OF STORM WATERS. HOWEVER, TEMPORARY ABOVE GROUND UTILITIES, NOT TO EXCEED ONE YEAR SHALL BE ALLOWED.
- 5) THESE LOTS ARE SUBJECT TO SECTION 7.17, OF THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS AT THE TIME OF LOT DEVELOPMENT.
- 6) THE APPROVAL OF THIS PLAT DOES NOT CONSTITUTE THE APPROVAL OF ANY FURTHER DEVELOPMENT, INCLUDING BUILDING PERMITS.
- 7) THIS PLAT LIES WITHIN THE PLANNING AND PLATTING JURISDICTION OF THE COUNTY OF SANTA FE, NEW MEXICO.
- 8) SANTA FE COUNTY'S APPROVAL OF THIS PLAT DOES NOT INCLUDE THE CONSTRUCTION OF THE PRIVATE EASEMENTS OR ROADS AS SHOWN. PRIOR TO CONSTRUCTION OF SAID PRIVATE EASEMENTS OR ROADS, IT IS REQUIRED THAT AN ADDITIONAL DEVELOPMENT PERMIT BE APPLIED FOR AND APPROVED BY THE SANTA FE COUNTY LAND USE ADMINISTRATOR.

COUNTY APPROVAL:

APPROVED BY THE BOARD OF COUNTY COMMISSIONERS OF SANTA FE COUNTY AT THEIR MEETING OF _____

CHAIRMAN _____ DATE _____

ATTESTED BY _____ COUNTY CLERK _____

COUNTY REVIEW:

COUNTY LAND USE ADMINISTRATOR _____ DATE _____
DEVELOPMENT PERMIT # _____

COUNTY PUBLIC WORKS _____ DATE _____

COUNTY FIRE MARSHALL _____ DATE _____

COUNTY RURAL ADDRESSING _____ DATE _____

COUNTY WATER UTILITY _____ DATE _____

UTILITY COMPANIES:

CENTURY LINK COMMUNICATIONS _____ DATE _____

COMCAST CABLE _____ DATE _____

PUBLIC SERVICE CO. OF NEW MEXICO _____ DATE _____

IN APPROVING THIS PLAT, NM GAS COMPANY DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, NM GAS COMPANY DOES NOT WAIVE NOR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLATS, REPLATS OR OTHER DOCUMENTS WHICH ARE NOT SHOWN ON THIS PLAT.

NM GAS COMPANY _____ DATE _____

DEDICATION AND AFFIDAVIT:

KNOW ALL MEN BY THESE PRESENTS, THAT THE UNDERSIGNED OWNERS OF THOSE LANDS LYING WITHIN SANTA FE COUNTY CONTAINING AN AREA OF 68.932 ACRES, MORE OR LESS, BEING A PART OF SECTION 11, T16N, R9E, N.M.P.M., HAS CAUSED THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAT AND THAT THE SAID SUBDIVISION IS NAMED AND SHALL BE KNOWN AS "ST. FRANCIS SOUTH". ALL THAT APPEARS ON THIS PLAT IS MADE WITH THE FREE CONSENT, AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS.

THE DRAINAGE, ACCESS AND UTILITY EASEMENTS SHOWN HEREON ARE HEREBY GRANTED FOR THE CONSTRUCTION, MAINTENANCE, REPAIR AND OPERATION OF UTILITIES. THE EASEMENTS ARE SUBJECT TO THE PROVISIONS ON THIS PLAT.

THE INTERNAL LOT ACCESS ROADWAY ROW, TRACT A-1, SHALL BE OWNED AND MAINTAINED BY THE ST. FRANCIS SOUTH LOT OWNERS ASSOCIATION.

OWNERS:

COUNTY OF SANTA FE } SS
STATE OF NEW MEXICO }
I hereby certify that this instrument was filed for record on the _____ day of _____, 20____ A.D. at _____ o'clock _____ M., and was duly recorded in book _____ page(s) _____ of the records of Santa Fe County.
Witness my Hand and Seal of office
GERALDINE SALAZAR
County Clerk, Santa Fe County, N.M.

Deputy

"TITLE AND INDEXING INFORMATION FOR COUNTY CLERK"

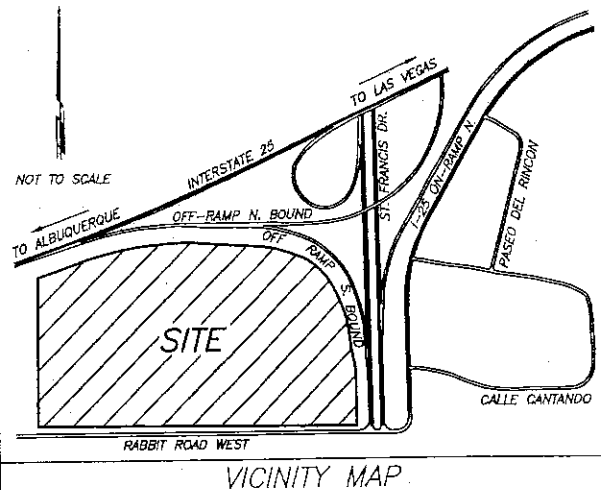
FINAL SUBDIVISION PLAT PREPARED FOR
VEGAS VERDES, LLC
CREATING
ST. FRANCIS SOUTH, PHASE 1
BEING A SUBDIVISION OF TRACT A, PLAT BOOK 674 PAGE 037
WITHIN SECTION 11, T16N, R9E, N.M.P.M., SANTA FE COUNTY, NEW MEXICO

PURPOSE OF THIS PLAT:
TO CREATE 4 RESIDENTIAL LOTS, WITH OPEN SPACE WITHIN LOTS, AND 1 RIGHT OF WAY TRACT, BEING PART OF THE COUNTY APPROVED ST. FRANCIS SOUTH MASTER PLAN.

UPC: 1-053-095-115-220

DAWSON SURVEYS INC.
PROFESSIONAL LAND SURVEYORS
2502 CAMINO ENTRADA
SANTA FE, NEW MEXICO
FILE# 9375 PH1 DATE: 2-22-17

SHEET 1 OF 2



FLOOD ZONE:

THIS PROPERTY IS LOCATED WITHIN OTHER AREAS ZONES "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS SHOWN ON FIRM PANEL 3504900414E, MAP REVISED DECEMBER 04, 2012.

REFERENCE DOCUMENTS:

BOUNDARY SURVEY PLAT OF TRACT A ON RABBIT ROAD WEST, PREPARED BY DEAN L. SHRAIDER N.M.P.L.S. No. 12451, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK AS PLAT BOOK 674, PAGE 037.

NOTES:

THIS PLAT IS SUBJECT TO ANY RESTRICTIONS, COVENANTS AND EASEMENTS OF RECORD.

LEGEND:

BEARINGS ARE REFERRED TO THE NEW MEXICO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE. DISTANCES SHOWN ARE GROUND LEVEL REFERRED TO 6800' AMSL DATUM. TO CONVERT DISTANCES SHOWN TO GRID DISTANCES MULTIPLY BY A SCALE FACTOR OF 0.999999.

- DENOTES FOUND MONUMENT AS SHOWN
- DENOTES REBAR TO BE SET UPON RECORDING
- DENOTES CALCULATED POINT NOT SET
- DENOTES UTILITY POLE WITH ANCHOR
- DENOTES OVERHEAD UTILITY LINES
- DENOTES EXISTING BARBED WIRE FENCE
- DENOTES BOUNDARY OF LANDS DEALT WITH BY THIS PLAT. INFORMATION OUTSIDE THIS BOUNDARY IN BACKGROUND INFORMATION ONLY, NOT VERIFIED FOR ACCURACY AND IS NOT PART OF THIS PLAT.
- /// DENOTES OPEN SPACE (13.747 Ac±)
- /// DENOTES DRAINAGE EASEMENT

SURVEYORS CERTIFICATE

I HEREBY CERTIFY THAT THIS PLAT IS A TRUE REPRESENTATION OF A SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN DECEMBER 2016, TO THE BEST OF MY KNOWLEDGE, THE SURVEY AND PLAT ARE CORRECT, TRUE AND MEET THE MINIMUM STANDARDS FOR LAND SURVEYS IN NEW MEXICO.

PRELIMINARY FOR REVIEW

BARRY S. PHILLIPS, N.M.P.L.S. 15517

SANTA FE COUNTY APPROVAL NOTES AND CONDITIONS:

- 1) BUILDING SITES AND DRIVEWAY LOCATIONS ARE SUBJECT TO THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS.
- 2) PRIOR TO THE ISSUANCE OF BUILDING PERMITS, THE SANTA FE COUNTY FIRE MARSHALL MUST FIND THE ACCESS ROADS ACCEPTABLE FOR EMERGENCY VEHICLE ACCESS AND THE FIRE HYDRANTS MUST BE ACCEPTABLE AND OPERABLE.
- 3) THESE LOTS ARE SUBJECT TO SANTA FE COUNTY FIRE AND RESCUE IMPACT FEES AT THE TIME OF BUILDING PERMIT. SUCH FEES SHALL BE PAID BY THE PARTY SEEKING THE BUILDING PERMIT.
- 4) ALL UTILITY LINES SHALL BE PLACED UNDERGROUND. UPON THE COMPLETION OF THE INSTALLATION OR REPAIR OF THE SAID UTILITIES, THE GRADE OF THE UTILITY EASEMENT SHALL BE RESTORED BY THE UTILITY COMPANY TO ITS CONDITION PRIOR TO SAID INSTALLATION OR REPAIR TO ALLOW FOR THE NATURAL DRAINAGE OF STORM WATERS. HOWEVER, TEMPORARY, ABOVE GROUND UTILITIES, NOT TO EXCEED ONE YEAR SHALL BE ALLOWED.
- 5) THESE LOTS ARE SUBJECT TO SECTION 7.17, OF THE SANTA FE COUNTY TERRAIN MANAGEMENT REGULATIONS AT THE TIME OF LOT DEVELOPMENT.
- 6) THE APPROVAL OF THIS PLAT DOES NOT CONSTITUTE THE APPROVAL OF ANY FURTHER DEVELOPMENT, INCLUDING BUILDING PERMITS.
- 7) THIS PLAT LIES WITHIN THE PLANNING AND PLATTING JURISDICTION OF THE COUNTY OF SANTA FE, NEW MEXICO.
- 8) SANTA FE COUNTY'S APPROVAL OF THIS PLAT DOES NOT INCLUDE THE CONSTRUCTION OF THE PRIVATE EASEMENTS OR ROADS AS SHOWN. PRIOR TO CONSTRUCTION OF SAID PRIVATE EASEMENTS OR ROADS, IT IS REQUIRED THAT AN ADDITIONAL DEVELOPMENT PERMIT BE APPLIED FOR AND APPROVED BY THE SANTA FE COUNTY LAND USE ADMINISTRATOR.

COUNTY APPROVAL:

APPROVED BY THE BOARD OF COUNTY COMMISSIONERS OF SANTA FE COUNTY AT THEIR MEETING OF _____

CHAIRMAN _____ DATE _____

ATTESTED BY _____ COUNTY CLERK _____

COUNTY REVIEW:

COUNTY LAND USE ADMINISTRATOR _____ DATE _____
DEVELOPMENT PERMIT # _____

COUNTY PUBLIC WORKS _____ DATE _____

COUNTY FIRE MARSHALL _____ DATE _____

COUNTY RURAL ADDRESSING _____ DATE _____

COUNTY WATER UTILITY _____ DATE _____

UTILITY COMPANIES:

CENTURY LINK COMMUNICATIONS _____ DATE _____

COMCAST CABLE _____ DATE _____

IN APPROVING THIS PLAT, PNM ELECTRIC SERVICES DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, PNM DOES NOT WAIVE NOR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLATS, REPLATS OR OTHER DOCUMENTS WHICH ARE NOT SHOWN ON THIS PLAT.

PUBLIC SERVICE CO. OF NEW MEXICO _____ DATE _____

IN APPROVING THIS PLAT, NM GAS COMPANY DID NOT CONDUCT A TITLE SEARCH OF THE PROPERTIES SHOWN HEREON. CONSEQUENTLY, NM GAS COMPANY DOES NOT WAIVE NOR RELEASE ANY EASEMENT OR EASEMENT RIGHTS WHICH MAY HAVE BEEN GRANTED BY PRIOR PLATS, REPLATS OR OTHER DOCUMENTS WHICH ARE NOT SHOWN ON THIS PLAT.

NM GAS COMPANY _____ DATE _____

DEDICATION AND AFFIDAVIT:

KNOW ALL MEN BY THESE PRESENTS, THAT THE UNDERSIGNED OWNERS OF THOSE LANDS LYING WITHIN SANTA FE COUNTY CONTAINING AN AREA OF 68.932 ACRES, MORE OR LESS, BEING A PART OF SECTION 11, T16N, R9E, N.M.P.M., HAS CAUSED THE LANDS TO BE SUBDIVIDED AS SHOWN ON THIS PLAT AND THAT THE SAID SUBDIVISION IS NAMED AND SHALL BE KNOWN AS "ST. FRANCIS SOUTH". ALL THAT APPEARS ON THIS PLAT IS MADE WITH THE FREE CONSENT, AND IN ACCORDANCE WITH THE DESIRES OF THE UNDERSIGNED OWNERS.

THE DRAINAGE, ACCESS AND UTILITY EASEMENTS SHOWN HEREON ARE HEREBY GRANTED FOR THE CONSTRUCTION, MAINTENANCE, REPAIR AND OPERATION OF UTILITIES. THE EASEMENTS ARE SUBJECT TO THE PROVISIONS ON THIS PLAT.

THE INTERNAL LOT ACCESS ROADWAY ROW, TRACT A-1, SHALL BE OWNED AND MAINTAINED BY THE ST. FRANCIS SOUTH LOT OWNERS ASSOCIATION.

PUBLIC UTILITY EASEMENTS SHOWN ON THIS PLAT ARE GRANTED FOR THE COMMON AND JOINT USE OF SANTA FE COUNTY FOR THE OWNERSHIP AND MAINTENANCE OF THE WATER AND WASTEWATER COLLECTION SYSTEM SERVING THE SUBDIVISION, INCLUDING SEWER SERVICE LATERALS TO THE RESIDENTIAL LOT PROPERTY LINES.

VEGAS VERDE, LLC.

BY: DAVID GURULE, PRESIDENT

STATE OF NEW MEXICO
COUNTY OF SANTA FE

THE FOREGOING WAS SWORN, ACKNOWLEDGED AND SUBSCRIBED BEFORE ME BY
DAVID GURULE, PRESIDENT OF VEGAS VERDES, LLC
THIS _____ DAY OF _____, 2017

NOTARY PUBLIC MY COMMISSION EXPIRES _____

"TITLE AND INDEXING INFORMATION FOR COUNTY CLERK"

FINAL SUBDIVISION PLAT PREPARED FOR
VEGAS VERDES, LLC
CREATING
ST. FRANCIS SOUTH, PHASE 2
BEING A SUBDIVISION OF TRACT A, PLAT BOOK 674 PAGE 037
WITHIN SECTION 11, T16N, R9E, N.M.P.M., SANTA FE COUNTY, NEW MEXICO

PURPOSE OF THIS PLAT:
TO CREATE 8 RESIDENTIAL LOTS, WITH OPEN SPACE WITHIN LOTS, AND 1
RIGHT OF WAY TRACT, BEING PART OF THE COUNTY APPROVED ST.
FRANCIS SOUTH MASTER PLAN.

UPC: 1-053-095-115-220

DAWSON SURVEYS INC.
PROFESSIONAL LAND SURVEYORS
2002 CAMINO ENTRADA
SANTA FE, NEW MEXICO
FILE# 9375 PH2 DATE: 2-22-17

SHEET 1 OF 2

SHEET 3-8

TOPOGRAPHIC MAP

SHOWING TRACT A ON RABBIT ROAD WEST

PREPARED FOR

SANTA FE ENGINEERING CONSULTANTS, LLC

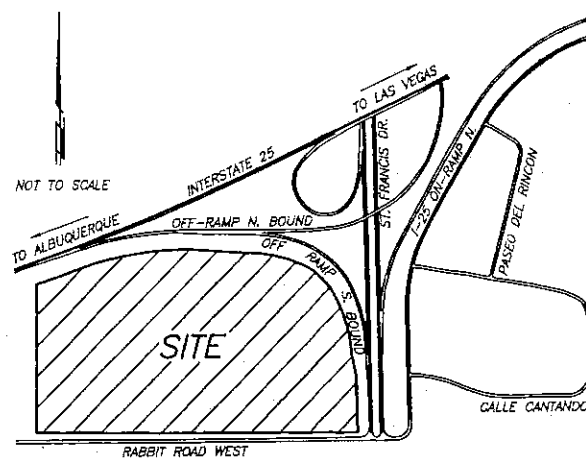
SITUATED WITHIN PROJECTED SECTION 11

T. 18 N., R. 9 E., N.M.P.M.

COUNTY OF SANTA FE, NEW MEXICO

FLOOD ZONE

THIS PROPERTY IS LOCATED WITHIN OTHER AREAS ZONE "X" (AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN) AS SHOWN ON FIRM MAP PANEL 3504900414E, MAP REVISED DECEMBER 04, 2012.



VICINITY MAP

REFERENCE DOCUMENTS

BOUNDARY SURVEY PLAT OF TRACT A ON RABBIT ROAD WEST, PREPARED BY DEAN L. SHRADER N.M.P.S. No. 12451, RECORDED IN THE OFFICE OF THE SANTA FE COUNTY CLERK AS PLAT BOOK 674, PAGE 037.

SCALE: 1"=100 FEET
100 0 100

NOTES

THIS PLAT SUBJECT TO ANY RESTRICTIONS, COVENANTS AND EASEMENTS OF RECORD.

UTILITIES SHOWN LARGER THAN ACTUAL SIZE FOR VIEWING PURPOSES.

NO UNDERGROUND UTILITIES WERE LOCATED.

THIS IS NOT A BOUNDARY SURVEY, APPARENT PROPERTY CORNERS ARE SHOWN FOR ORIENTATION ONLY. ALL IMPROVEMENTS MAY NOT BE SHOWN.

LEGEND

BEARINGS ARE GRID, N.M. STATE PLANE CENTRAL ZONE. DISTANCES ARE REFERRED TO 6800' AMSL TO CONVERT TO N.M. STATE PLANE CENTRAL ZONE, MULTIPLY DISTANCES BY 0.999583883.

ELEVATIONS ARE NAVD83, GEOID '99 DERIVED FROM CONTINUAL OPERATING BASE STATION DAWSON2 HAVING AN ELEVATION OF 6572.376', DERIVED FROM STATIC GPS OBSERVATIONS ON 1992 SANTA FE COUNTY GIS MAPPING CONTROL.

- DENOTES REBAR, OR AS SHOWN FOUND
- DENOTES SIGNS
- DENOTES 1' CONTOUR INTERVAL
- DENOTES 5' CONTOUR INTERVAL
- DENOTES UTILITY POLE WITH ANCHOR

SURVEYORS CERTIFICATE

I HEREBY CERTIFY THAT THIS MAP IS A TRUE REPRESENTATION OF A TOPOGRAPHIC SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN NOVEMBER 2013. TO THE BEST OF MY KNOWLEDGE, THE SURVEY AND MAP ARE CORRECT, TRUE AND MEET THE MINIMUM NATIONAL MAPPING STANDARDS.

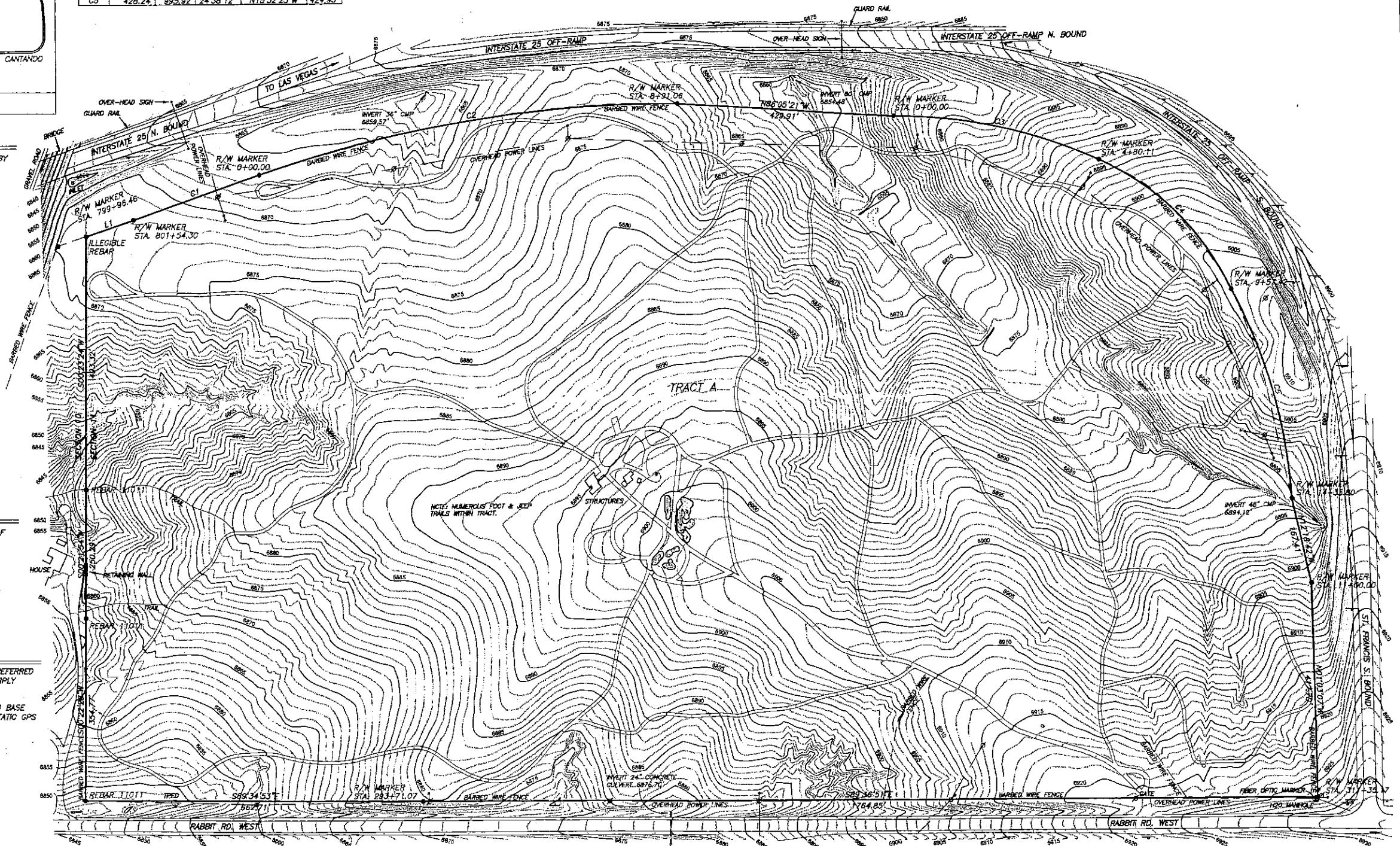
BARRY S. PHILLIPS,

N.M.P.L.S. #15517



LINE TABLE		
LINE	LENGTH	BEARING
L1	97.78'	S71°11'21"W

CURVE TABLE				
CURVE	LENGTH	RADIUS	DELTA	CHD BEARING
C1	282.42'	17338.74'	0°52'02"	S70°45'04"W
C2	846.55'	2506.42'	19°21'07"	S81°00'38"W
C3	416.42'	995.92'	23°57'28"	N77°31'59"W
C4	370.84'	524.07'	40°32'36"	N45°15'57"W
C5	429.24'	995.92'	24°38'12"	N15°32'23"W



SHEET 3-8

DAWSON SURVEYS INC.
PROFESSIONAL LAND SURVEYORS
2502B CAMINO ENTRADA
SANTA FE, NEW MEXICO 87507
FILE #9375TOPO DATE:12/04/13

LEGEND

- DENOTES FOUND #4 REBAR WITH GAP 5338
- ++ DENOTES FENCE
- DENOTES 5' CONTOUR INTERVAL
- - - DENOTES 1' CONTOUR INTERVAL
- + DENOTES SPOT ELEVATION
- FIRE HYDRANT
- MANHOLE
- WATER VALVE
- SEWER MKR.
- FIBER OPTIC MKR.
- GAS MKR.
- WATER-LOCATES
- SEWER APPROX.

NOTE

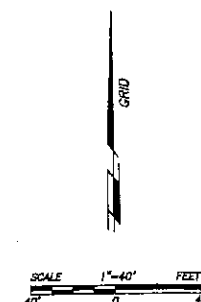
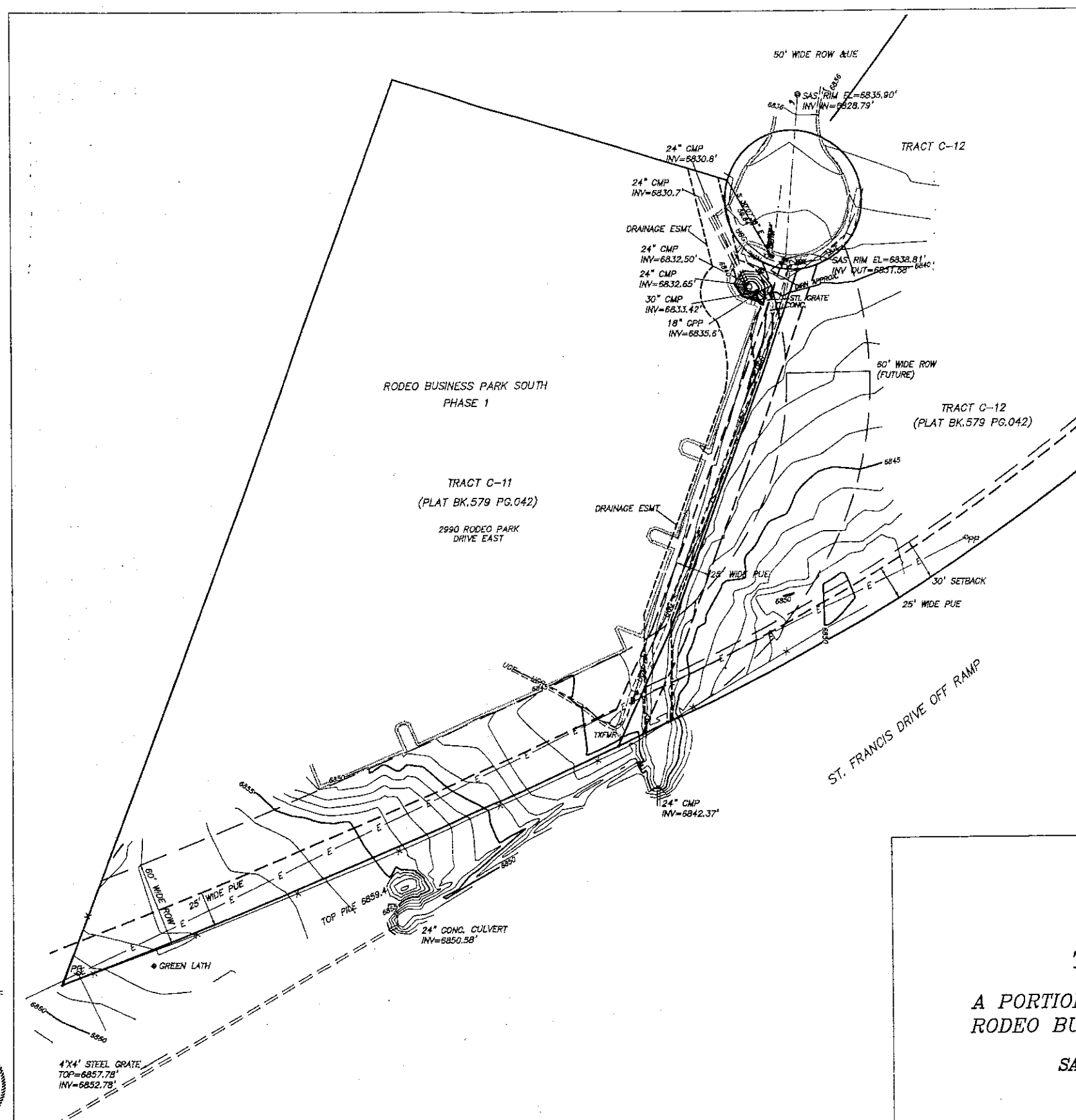
THIS IS NOT A PROPERTY SURVEY, APPARENT PROPERTY CORNERS ARE SHOWN FOR ORIENTATION ONLY, BOUNDARY DATA SHOWN IS FROM PREVIOUS SURVEY REFERENCED HEREON.

SURVEYORS CERTIFICATE

I HEREBY CERTIFY THAT THIS MAP IS A TRUE REPRESENTATION OF A TOPOGRAPHIC SURVEY COMPLETED UNDER MY PERSONAL SUPERVISION IN FEBRUARY OF 2016, THE SURVEY AND DRAWING ARE TRUE AND CORRECT, TO THE BEST OF MY KNOWLEDGE, AND MEET THE MINIMUM STANDARDS FOR TOPOGRAPHIC SURVEYS IN NEW MEXICO.

BARRY S. PHILLIPS

N.M.P.L.S. #18317

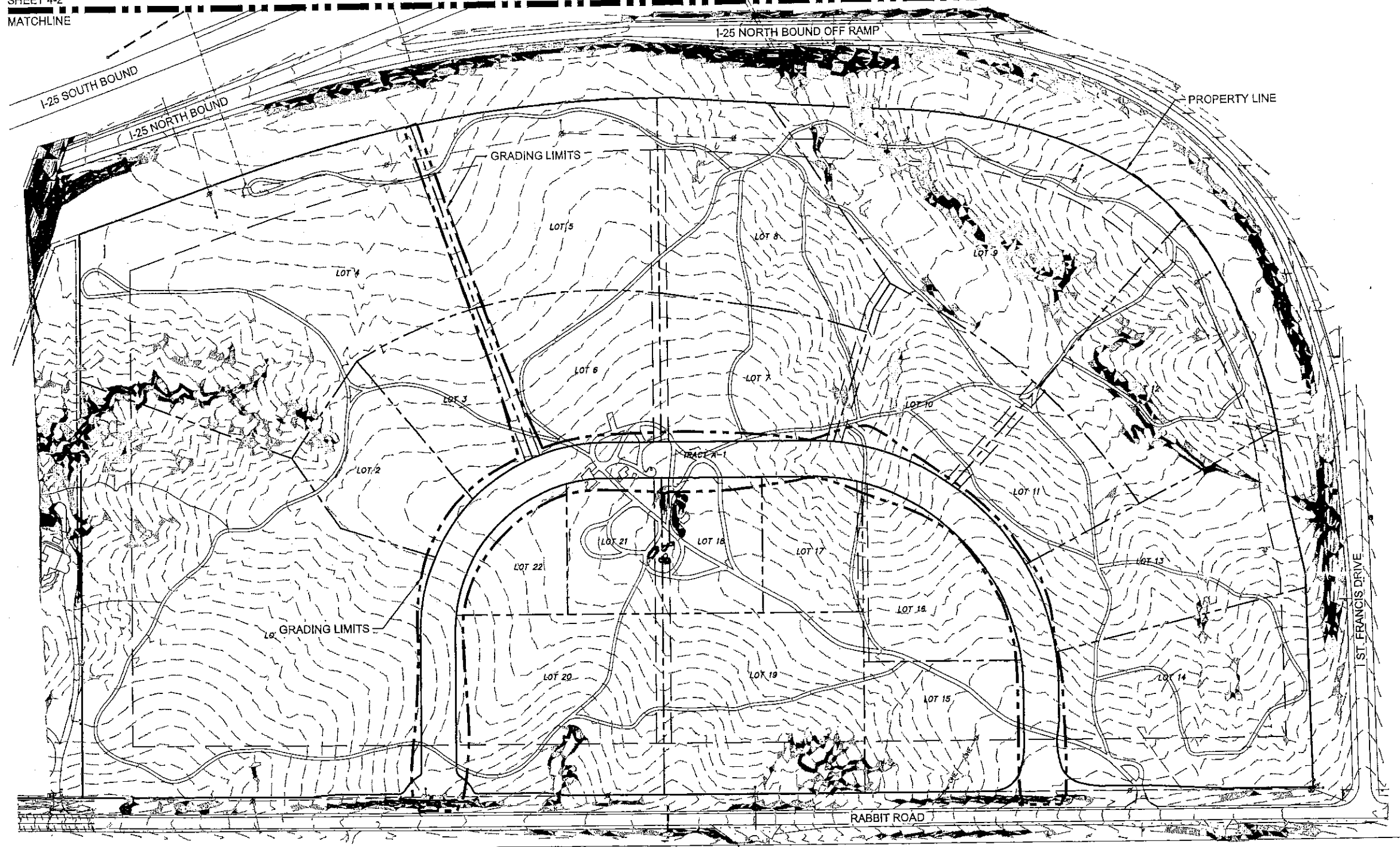


TOPOGRAPHIC MAP
 SHOWING
 A PORTION OF THE TRACTS C-11, C-12
 RODEO BUSINESS PARK SOUTH-PHASE 1
 PREPARED FOR
 SANTA FE ENGINEERING, LLC.

SHEET 3-9





DAWSON SURVEYS INC.
 PROFESSIONAL LAND SURVEYORS
 2502 B CAMINO ENTRADA
 SANTA FE, NEW MEXICO
 FILE #93757500 DATE 01/14/2016

SHEET 4-2
MATCHLINE

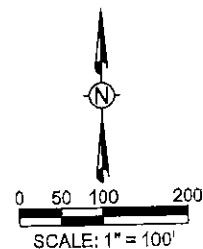


SLOPE ANALYSIS MAP SOUTH OF I-25
SCALE: 1" = 100'

LEGEND

-  AREA OF 15% TO 20% SLOPE
-  AREA OF 20% TO 30% SLOPE
-  AREA OF 30% AND GREATER SLOPE
-  GRADING LIMITS

THIS SLOPE ANALYSIS MAP IS BASED
UPON A 2 FOOT CONTOUR INTERVAL



**S
F
E
C**

**Santa Fe Engineering
Consultants, LLC**

1599 St. Francis Drive, Suite B
Santa Fe, N.M. 87505
(505) 982-2845 Fax (505) 982-2841
<http://www.SFENGR.com>

PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

SLOPE ANALYSIS MAP

DATE: FEBRUARY 2017	SCALE: 1"=100'	SHEET: 4-1
------------------------	-------------------	---------------

ENGINEER'S SEAL	
DATE	
REVISIONS	
1	
2	
3	
4	
5	
6	
7	
8	
9	
10	

LEGEND

1. **Introduction**
 2. **Background**
 3. **Methodology**
 4. **Results**
 5. **Conclusion**
 6. **References**

AREA OF 15% TO 20% SLOPE

AREA OF 20% TO 30% SLOPE

AREA OF 30% AND GREATER SLOPE

THIS SLOPE ANALYSIS MAP IS BASED
UPON A 2 FOOT CONTOUR INTERVAL



SCALE: 1" = 40'



SCALE: 1" = 40'

S
F
E
C

Santa Fe Engineering
Consultants, LLC

1599 St. Francis Drive, Suite B
Santa Fe, N. M. 87505
(505) 982-2845 Fax (505) 982-2641
<http://www.SFENGR.com>

PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

SLOPE ANALYSIS MAP (CONT'D)

DATE: FEBRUARY 2017

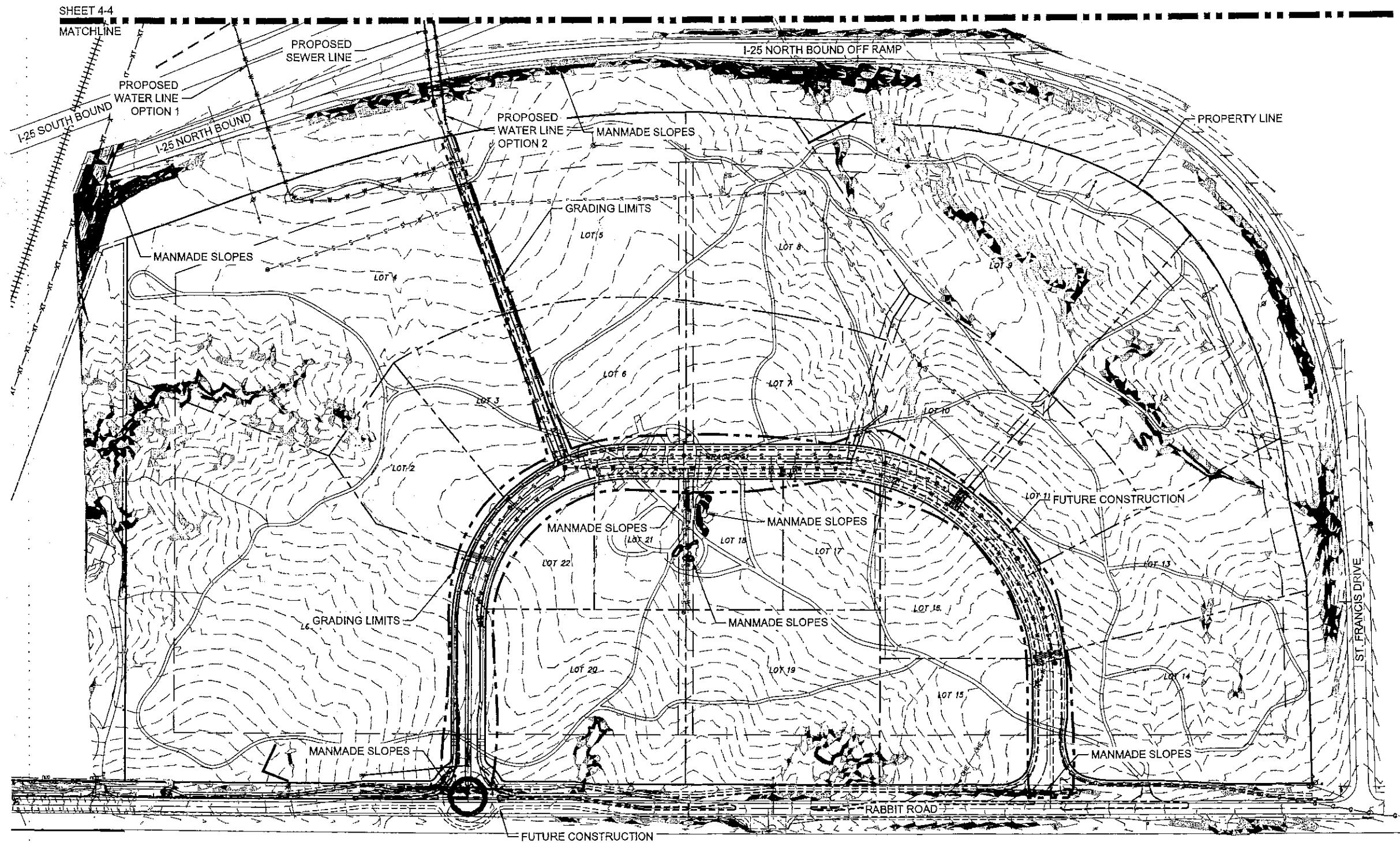
SCALE:
1"=40'

SHEET:
4-2

ENGINEERS SEAL

DATE _____

REVISIONS



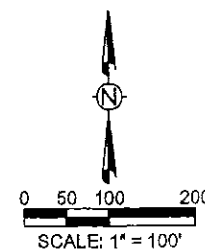
LEGEND

AREA OF 15% TO 20% SLOPE
 AREA OF 20% TO 30% SLOPE
 AREA OF 30% AND GREATER SLOPE
 GRADING LIMITS

THIS SLOPE ANALYSIS MAP IS BASED UPON A 2 FOOT CONTOUR INTERVAL

THERE ARE NO 30% AND GREATER NATURAL SLOPES WITHIN THE GRADING LIMITS ON THIS PROJECT.

SLOPE ANALYSIS CALCULATIONS SOUTH OF I-25
SCALE: 1" = 100'

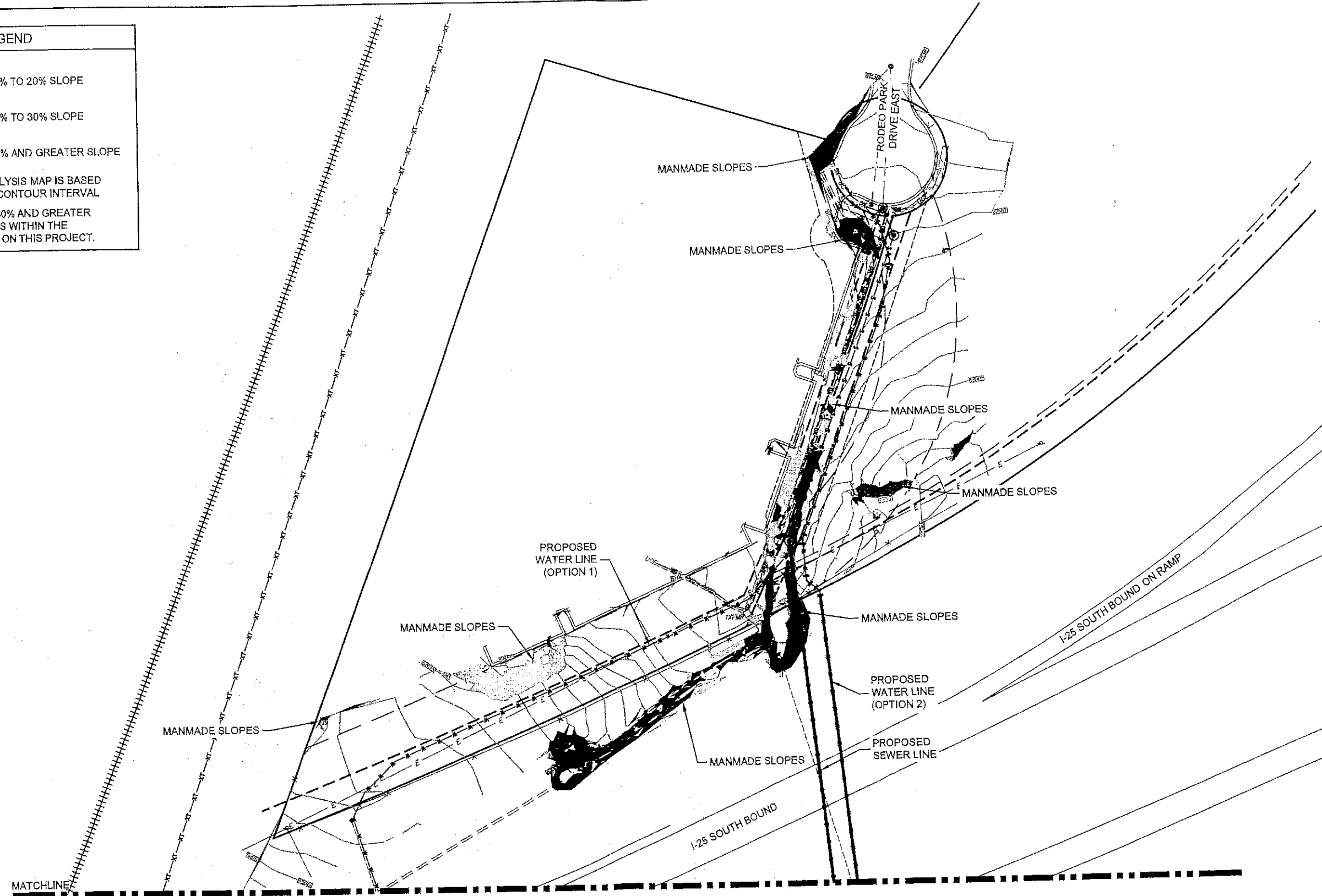


S F E C	Santa Fe Engineering Consultants, LLC 1599 St. Francis Drive, Suite B Santa Fe, N. M. 87505 (505) 982-2845 Fax (505) 982-2841 http://www.SFENGR.com		PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4 FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2	
	SLOPE ANALYSIS CALCULATIONS		DATE: FEBRUARY 2017 SCALE: 1"=100' SHEET: 4-3	
	ENGINEER'S SEAL DATE REVISIONS			

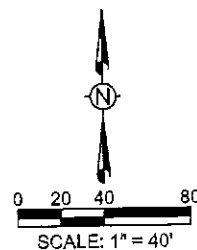
LEGEND

- AREA OF 15% TO 20% SLOPE
- AREA OF 20% TO 30% SLOPE
- AREA OF 30% AND GREATER SLOPE

THIS SLOPE ANALYSIS MAP IS BASED UPON A 2 FOOT CONTOUR INTERVAL
THERE ARE NO 30% AND GREATER NATURAL SLOPES WITHIN THE GRADING LIMITS ON THIS PROJECT.



SLOPE ANALYSIS CALCULATIONS NORTH OF I-25
SCALE: 1" = 40'



MATCHLINE
SHEET 4-3

ENGINEER'S SEAL	
DATE	
REVISIONS	

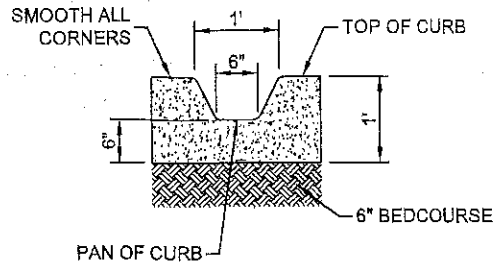
SFE C	Santa Fe Engineering Consultants, LLC 1599 St. Francis Drive, Suite B Santa Fe, N.M. 87505 (505) 982-2845 Fax (505) 982-2841 http://www.SFENGR.com		PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4 FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2	
	SLOPE ANALYSIS CALCULATIONS (CONT'D)			
	DATE:	SCALE:	SHEET:	
	FEBRUARY 2017	1"=40'	4-4	

CONSTRUCTION NOTES

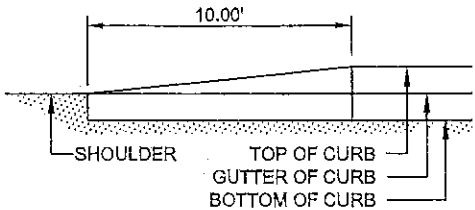
1. STREET CONSTRUCTION WORK SHALL CONFORM WITH THE NEW MEXICO STATE HIGHWAY DEPARTMENT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2007 EDITION. UTILITY CONSTRUCTION SHALL CONFORM TO THE AMERICAN PUBLIC WORKS ASSOCIATION, PUBLIC WORKS CONSTRUCTION MANUAL, 1987 EDITION. ALL CONSTRUCTION SHALL CONFORM WITH COUNTY STANDARDS AND SPECIFICATIONS AS APPLICABLE.
2. THE CONTRACTOR SHALL NOT COMMENCE CONSTRUCTION WITHOUT CONSTRUCTION PLAN APPROVAL BY THE COUNTY. A COPY OF THE APPROVED PLANS SHALL BE AVAILABLE AT THE CONSTRUCTION SITE AT ALL TIMES DURING WORKING HOURS AND THE DEVELOPMENT PERMIT SHALL BE POSTED.
3. THE CONTRACTOR SHALL NOTIFY THE COUNTY PROJECTS DIVISION OF THE PROPOSED COMMENCEMENT OF CONSTRUCTION AT LEAST 24 HOURS PRIOR TO THE START UP. A PRE-CONSTRUCTION CONFERENCE SHALL BE HELD AT THE COUNTY PUBLIC WORKS DEPARTMENT PRIOR TO THE START OF CONSTRUCTION. CALL 996-6222 TO SCHEDULE THE MEETING DATE, 10 DAYS IN ADVANCE.
4. THE DEVELOPER'S ENGINEER SHALL SUBMIT TO THE COUNTY THE APPROPRIATE WORKING DRAWINGS AND DESIGN CRITERIA USED TO JUSTIFY ANY CHANGES IN THE CONSTRUCTION WORK RESULTING FROM SPECIAL FIELD CONDITIONS. ALL CHANGE ORDERS SHALL BE INITIATED BY THE PROJECT DESIGN ENGINEER WITH FINAL DESIGN DRAWINGS CERTIFIED BY A NEW MEXICO PROFESSIONAL ENGINEER AND RECEIVE COUNTY APPROVAL PRIOR TO IMPLEMENTING CHANGE ORDER CONSTRUCTION.
5. THE OWNER SHALL BE RESPONSIBLE THROUGH HIS ENGINEER FOR MAKING ALL ENGINEERING PLAN CHANGES AND REVISIONS TO THE ORIGINAL APPROVED ENGINEERING DRAWINGS. FINAL "AS-BUILT" DRAWINGS SHALL BE FILED IN THE OFFICE OF THE PUBLIC WORKS DEPARTMENT AND LAND USE DEPARTMENT BEFORE COUNTY PROJECT ACCEPTANCE.
6. WHERE THE WATER AND SEWER LINES ARE LAID PARALLEL, THE HORIZONTAL SEPARATION SHALL BE AT LEAST 10 FEET APART, AND THE WATER LINE SHALL BE AT LEAST 3 FEET HIGHER THAN THE SEWER LINE. IF IT IS ALL POSSIBLE, SEPARATE TRENCHES WILL BE REQUIRED IN ALL CASES (THIS SHALL BE EFFECTIVE EVEN THOUGH ONE LINE HAS ALREADY BEEN INSTALLED PRIOR TO THE OTHER) AND THE WATER LINE SHALL BE AT LEAST 3 FEET ABOVE THE SEWER LINE.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION OF ALL EXISTING UTILITIES AND SHOULD NOT RELY SOLELY ON THESE CONSTRUCTION PLANS FOR UTILITY LOCATIONS. NOTIFICATION TO THE UTILITY COMPANIES IS REQUIRED PRIOR TO COMMENCING WORK. CALL ONE CALL AT 1-800-321-2537, 48 HOURS PRIOR TO DIGGING.
8. THE CONTRACTOR SHALL CONFINE HIS OPERATIONS TO THE CONSTRUCTION LIMITS OF THE PROJECT AND IN NO WAY SHALL ENCROACHMENT OCCUR ONTO ADJACENT PROPERTIES UNLESS LEGAL EASEMENTS ARE OBTAINED. ALL FILL AND CUT SLOPES SHALL BE SETBACK FROM THE PROPERTY LINE IN ACCORDANCE WITH CHAPTER 70 OF THE UNIFORM BUILDING CODE. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY AGREEMENTS NECESSARY OR DAMAGE CAUSED BY CONSTRUCTION ACTIVITIES TO PUBLIC OR PRIVATE PROPERTY, INCLUDING UTILITIES.
9. THE CONTRACTOR SHALL IMPLEMENT THE NECESSARY SITE EROSION CONTROL DEVICES FOR INHIBITING DUST WIND AND AIR SEDIMENT MOVEMENT OFFSITE DURING ALL PHASES OR STAGES OF CONSTRUCTION. SEE SECTION 211 OF THE NEW MEXICO STATE HIGHWAY DEPARTMENT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION
10. A GRADING PERMIT IS REQUIRED PRIOR TO COMMENCING GRADING OPERATIONS. CALL 986-6222
11. A COMPLETION LETTER FROM EACH UTILITY COMPANY MUST BE SUBMITTED TO THE SUBDIVISION INSPECTOR BEFORE PAVING IS INSTALLED OR BEFORE A SEWER T.V. INSPECTION CAN OCCUR.
12. ASTM OR AASHTO CERTIFICATES OF MATERIAL COMPLIANCE ARE TO BE SUBMITTED TO THE SUBDIVISION INSPECTOR.
13. SUBGRADE, BASE MATERIAL, ASPHALT TREATED BASE AND ASPHALT SURFACE COURSE REQUIRE COMPACTION TESTS FOR EACH 100 LINEAR FEET. ASPHALT SAMPLES FOR EACH 500 TONS INSTALLED OR ONE SAMPLE PER DAY IS REQUIRED TO BE ANALYZED WITH TEST RESULTS SENT TO THE SUBDIVISION INSPECTOR.
14. THE CONTRACTOR SHALL PROVIDE AN AREA TO STORE CONSTRUCTION DEBRIS WHERE IT WILL NOT BE A NUISANCE TO THE SURROUNDING NEIGHBORHOOD. ALL DEBRIS SHALL BE CONTAINED IN SUCH A MANNER THAT WILL PREVENT SCATTERING. ALL DEBRIS, INCLUDING TREES AND UNDERGROWTH SHALL BE DISPOSED OF PROPERLY WITHIN THE CITY OR COUNTY LANDFILL. ALL DEBRIS SHALL BE REMOVED FROM THE SITE PRIOR TO FINAL SITE INSPECTION.
15. ALL UTILITY APPURTENANCES SUCH AS TELEPHONE PEDESTALS, ELECTRICAL TRANSFORMERS, GAS AND CABLE T.V. APPURTENANCES SHALL BE PLACED OUTSIDE THE PUBLIC RIGHT-OF-WAY AND WITHIN UTILITY EASEMENTS. THE DEVELOPER WILL BE RESPONSIBLE FOR RELOCATING MISPLACED UTILITY STRUCTURES PRIOR TO PROJECT ACCEPTANCE. WATER METERS AND FIRE HYDRANTS SHALL BE COORDINATED WITH THE EDGEWOOD WATER UTILITY COMPANY. WATER VALVES AND METER BOXES ARE NOT TO BE PLACED WITHIN MAINTENANCE AREAS OF GRAVEL ROADS OR IN BORROW DITCHES.
16. SANTA FE ENGINEERING CONSULTANTS WAIVES ANY AND ALL RESPONSIBILITY AND IS NOT LIABLE FOR PROBLEMS WHICH ARISE FROM FAILURE TO FOLLOW THESE PLANS, SPECIFICATIONS AND THE DESIGN INTENT THEY CONVEY OR FOR PROBLEMS WHICH ARISE FROM FAILURE TO OBTAIN AND/OR FOLLOW SFEC GUIDANCE WITH RESPECT TO ANY ERRORS, OMISSIONS, INCONSISTENCIES, AMBIGUITIES OR CONFLICTS.

NOTES

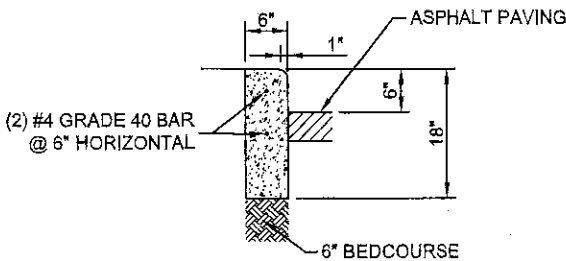
1. THE CONTRACTOR'S SURVEYOR SHALL COORDINATE WITH DAWSON SURVEYS INC. THE CONTRACTORS SURVEYOR SHALL VERIFY PROPOSED GRADES, INVERT ELEVATIONS, FLOW LINES, ALIGNMENTS, PROPERTY LINES, RIGHT OF WAY, SETBACKS, AND TOPOGRAPHY PRIOR TO CONSTRUCTION. ANY DEVIATIONS SHALL BE REPORTED TO THE ENGINEER.
2. THE CONTRACTOR SHALL MAINTAIN EXISTING FENCING AND SHALL ENSURE THAT THE SITE IS SECURE AT ALL TIMES.
3. IT IS SOLELY THE CONTRACTORS RESPONSIBILITY TO MEET OSHA REQUIREMENTS AND TO MAINTAIN A SAFE WORKING CONDITION.



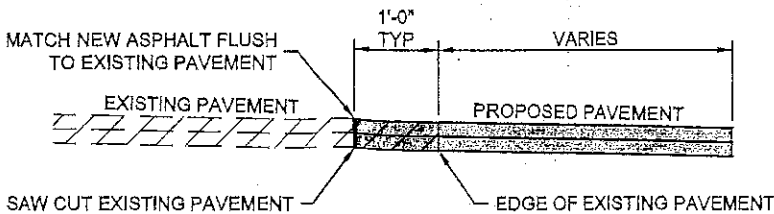
CURB OPENING LONGITUDINAL SECTION



CURB & GUTTER TRANSITION PROFILE

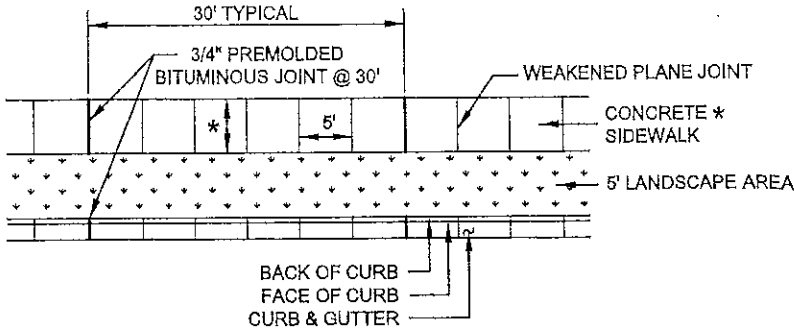


6" STAND-UP CURB

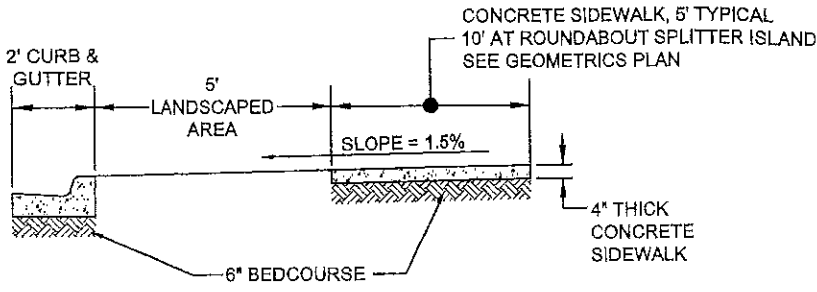


TYPICAL SAW CUT DETAIL

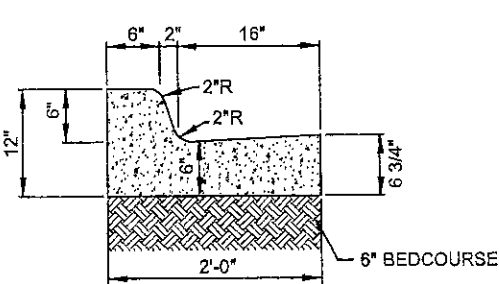
CONCRETE SIDEWALK, 5' TYPICAL
* 10' AT ROUNDABOUT SPLITTER ISLAND
SEE GEOMETRICS PLAN



TYPICAL SIDEWALK PLAN



TYPICAL SIDEWALK SECTION



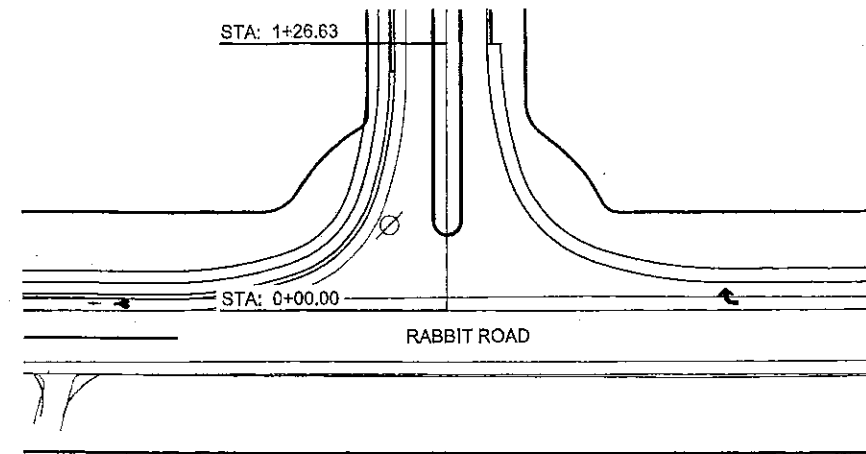
STANDARD CURB AND GUTTER

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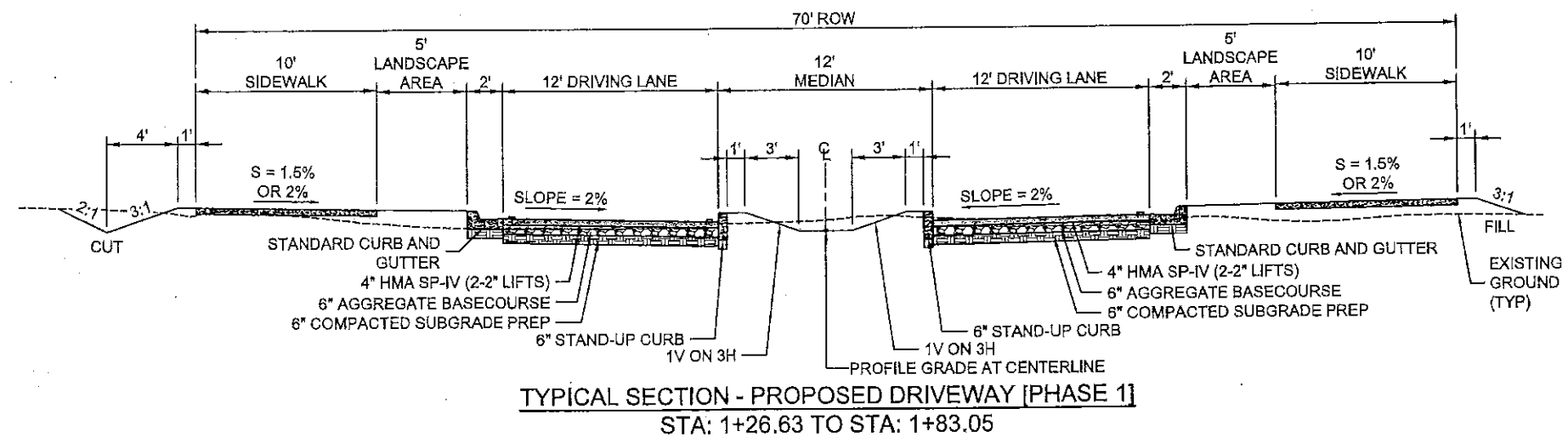
PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

GENERAL NOTES AND TYPICAL
DETAILS

DATE: FEBRUARY 2017 SCALE: N.T.S. SHEET: 5-1



NOTE:
STA: 0+00 TO 1+26.63 CONSISTS
OF VARIABLE GEOMETRY DUE
TO THE INTERSECTION WITH
RABBIT ROAD



ENGINEER'S SEAL	
DATE	
REVISIONS	

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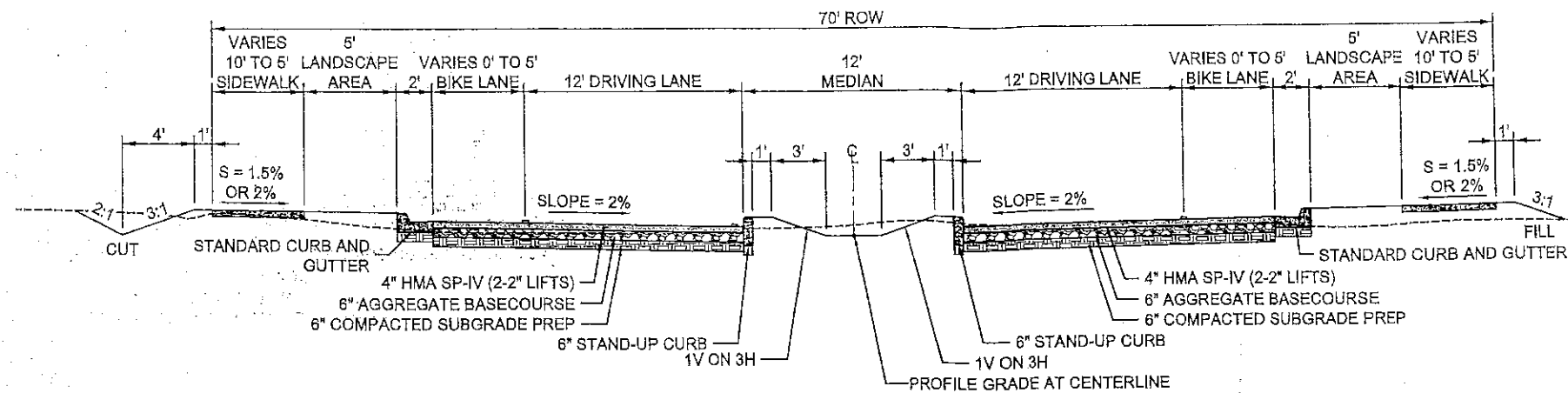
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ST. FRANCIS SOUTH PHASES 1 & 2

TYPICAL SECTIONS

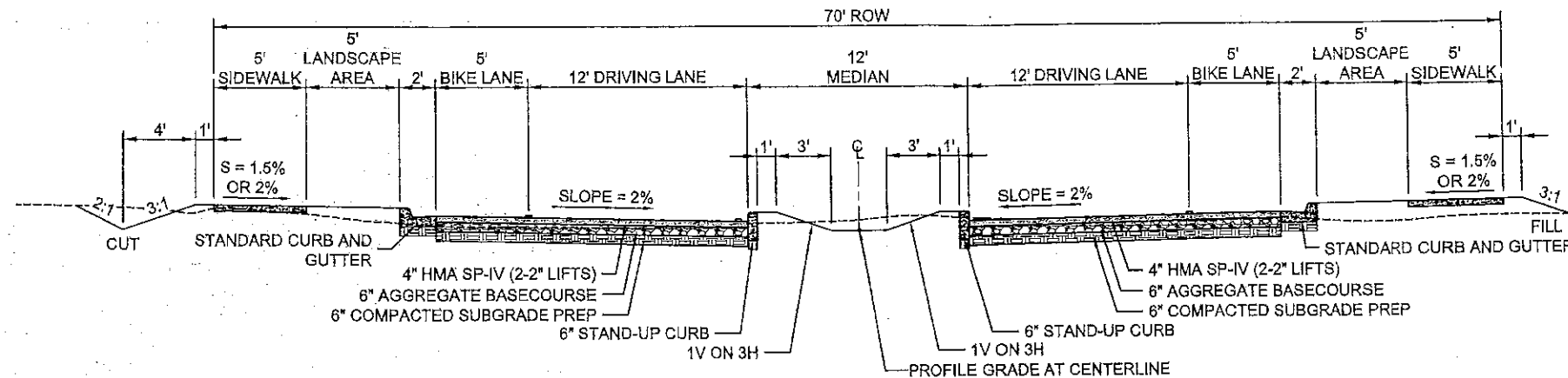
DATE:
FEBRUARY 2017

SCALE:
N.T.S.

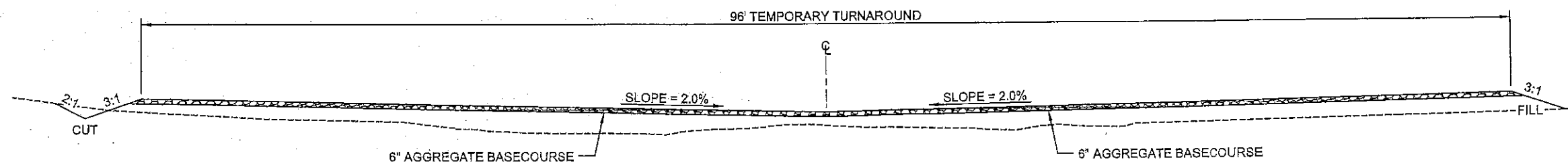
SHEET:
5-2



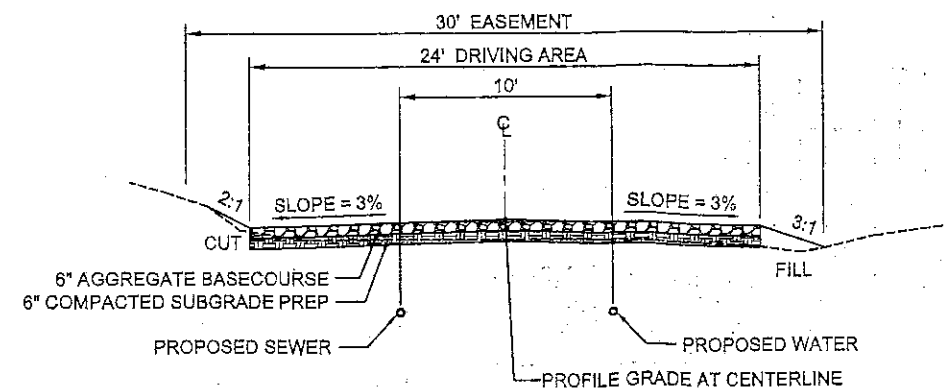
TYPICAL SECTION - PROPOSED DRIVEWAY [PHASE 1]
STA: 1+83.05 TO STA: 2+18.19



TYPICAL SECTION - PROPOSED DRIVEWAY [PHASE 1]
STA: 2+18.19 TO STA: 7+96.90



TYPICAL SECTION - PROPOSED DRIVEWAY AT TEMPORARY TURNAROUND [PHASE 1]
STA: 7+96.90 TO STA: 9+11.06



TYPICAL SECTION - PROPOSED UTILITY ACCESS ROAD

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Consultants, LLC**

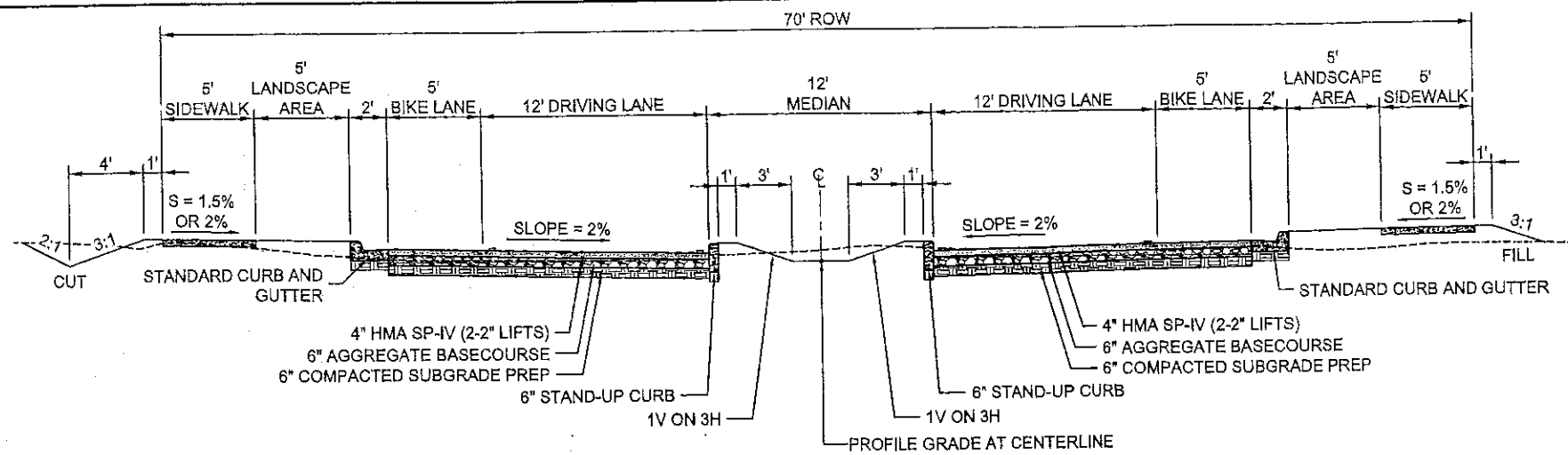
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FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

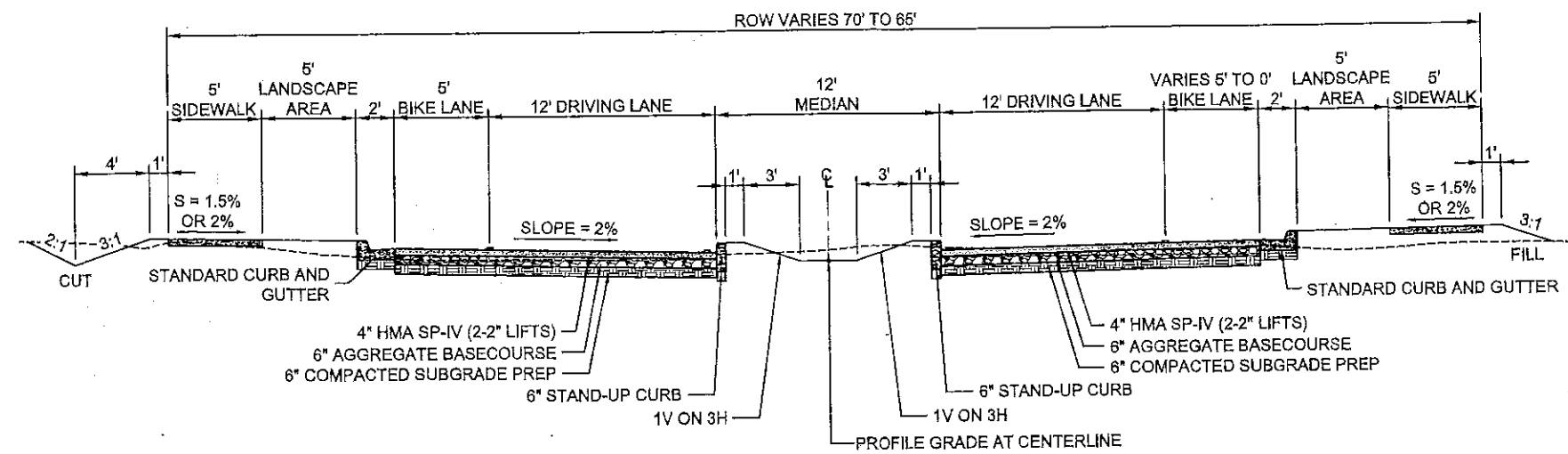
TYPICAL SECTIONS

DATE FEBRUARY 2017	SCALE N.T.S.	SHEET: 5-3
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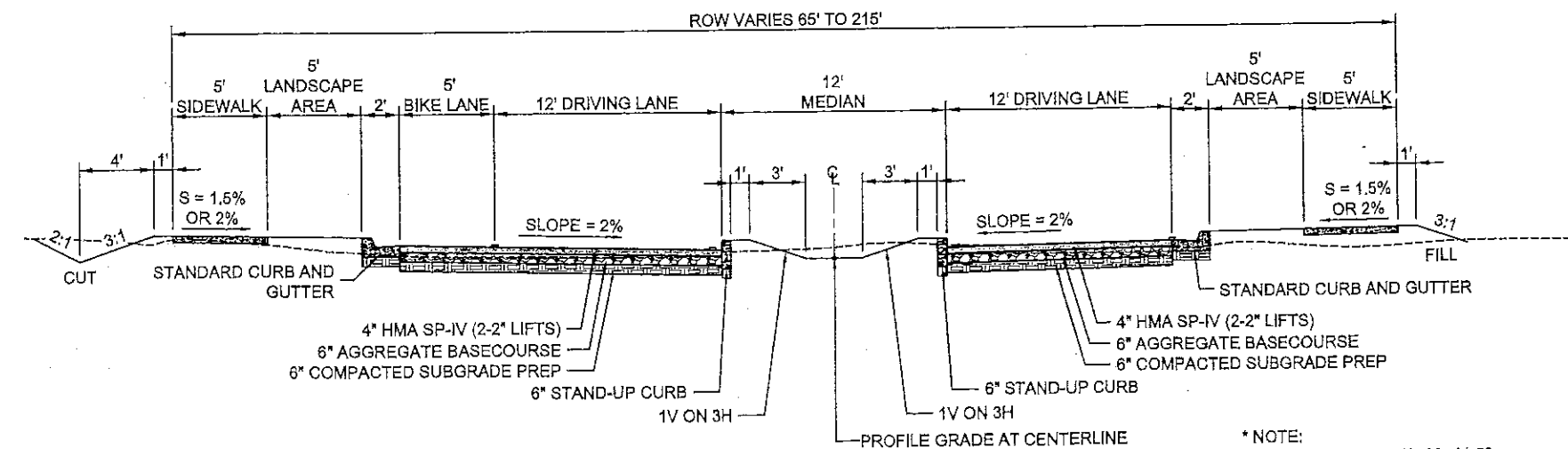
ENGINEER'S SEAL	
DATE	
REVISIONS	



TYPICAL SECTION - PROPOSED DRIVEWAY [PHASE 2]
STA: 7+96.90 TO STA: 20+76.50



TYPICAL SECTION - PROPOSED DRIVEWAY [PHASE 2]
STA: 20+76.50 TO STA: 21+11.50



TYPICAL SECTION - PROPOSED DRIVEWAY [PHASE 2]
STA: 21+11.50 TO STA: 22+11.29 *

* NOTE:
STA: 12+11.50 TO STA: 22+11.29
INTERSECTION WITH RABBIT ROAD
REFER TO GEOMETRICS PLANS

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TYPICAL SECTIONS

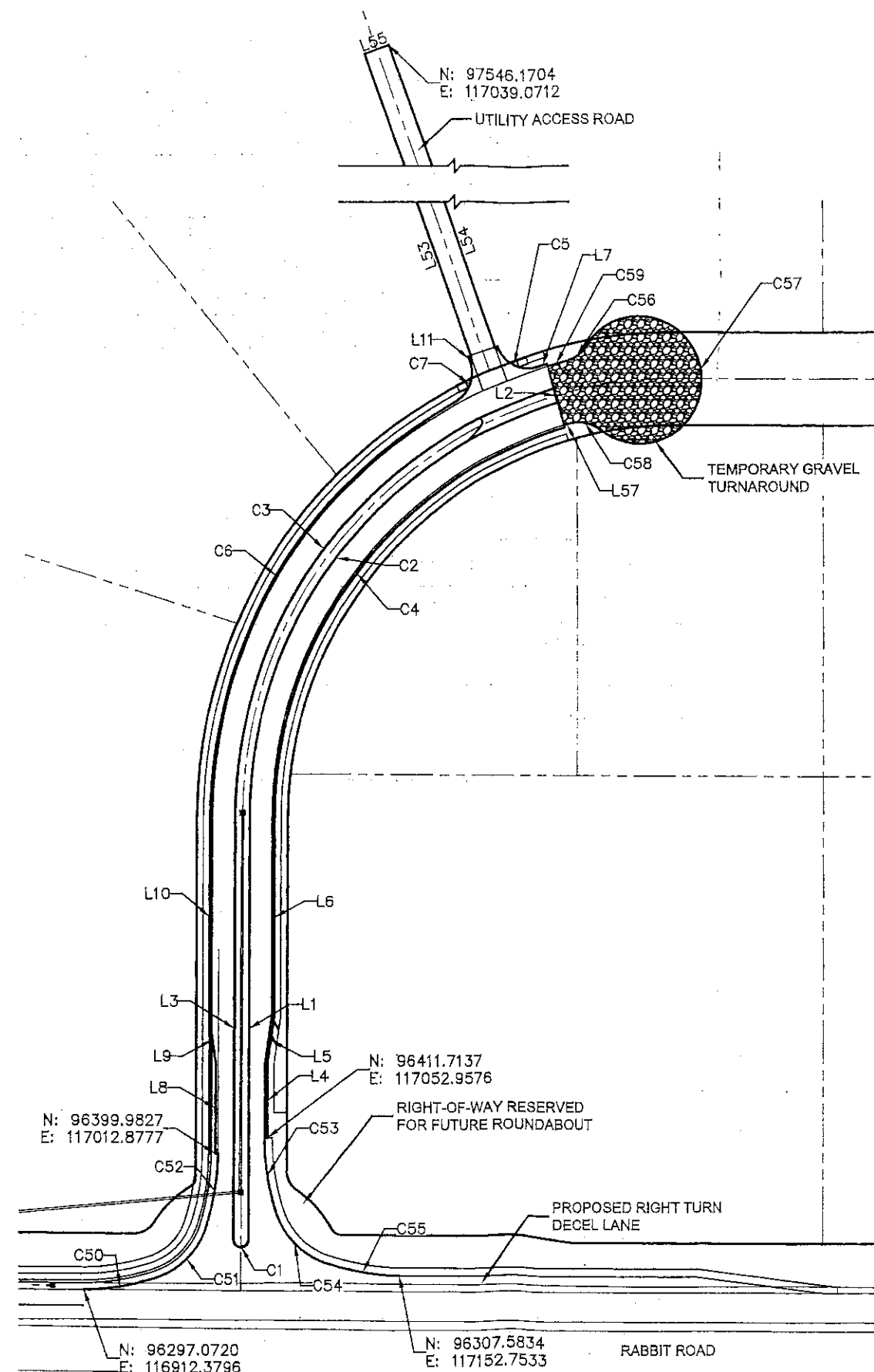
DATE: FEBRUARY 2017	SCALE: N.T.S.	SHEET: 5-4
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ENGINEER'S SEAL	
DATE	
REVISIONS	
1	
2	
3	
4	
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6	
7	
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9	
10	

GEOMETRICS DATA [BOC - PHASE 1]				
Line #/Curve #	Length	Bearing/Delta	Radius	Tangent
C1	17.28	180.00	5.50	INFINITY
C2	421.45	73.73	327.50	245.59
C3	435.61	73.73	338.50	253.84
C4	396.36	73.73	308.00	230.97
C5	38.14	87.42	25.00	23.90
C6	383.69	60.36	364.24	211.80
C7	35.55	81.48	25.00	21.53
L1	318.68	S0° 23' 08.59"W		
L2	11.00	S15° 52' 55.21"E		
L3	318.68	N0° 23' 08.59"E		
L4	56.42	S0° 23' 08.59"W		
L5	35.36	S8° 30' 56.96"W		
L6	150.32	S0° 23' 08.59"W		
L7	6.94	N73° 33' 46.89"E		
L8	68.42	S0° 23' 08.59"W		
L9	35.36	S7° 44' 39.78"E		
L10	150.32	S0° 23' 08.59"W		
L11	3.84	S19° 34' 22.09"E		

GEOMETRICS DATA [EOA - PHASE 1]				
Line #/Curve #	Length	Bearing/Delta	Radius	Tangent
C50	55.71	17.71	180.24	28.08
C51	55.95	53.43	60.00	30.19
C52	57.16	18.19	180.00	28.82
C53	57.16	18.19	180.00	28.82
C54	56.16	53.63	60.00	30.33
C55	56.16	17.87	180.11	28.31
L53	571.56	N19° 34' 22.09"W		
L54	571.56	S19° 34' 22.09"E		
L55	20.00	N70° 25' 37.91"E		

GEOMETRICS DATA [GRAVEL TURNAROUND - PHASE 1]				
Line #/Curve #	Length	Bearing/Delta	Radius	Tangent
C56	20.35	41.35	28.19	10.64
C57	232.35	277.35	48.00	42.21
C58	25.29	51.59	28.09	13.58
C59	14.55	2.34	356.00	7.28
L57	3.99	S74° 34' 43.98"W		



GEOMETRICS PLAN - PHASE 1
SCALE: 1" = 50'

0 25 50 100
SCALE: 1" = 50'

NOTES:

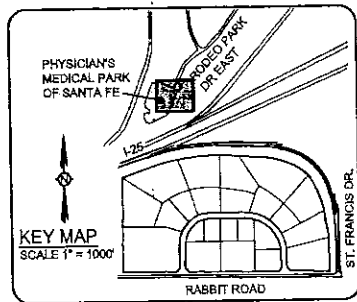
- GEOMETRICS DATA IS FROM TAKEN FROM BACK OF CURB, EDGE OF ASPHALT, OR EDGE OF TEMPORARY GRAVEL TURNAROUND AS NOTED.

FOR ROADWAY IMPROVEMENTS ON RABBIT ROAD, SEE NMDOT PLAN SET.

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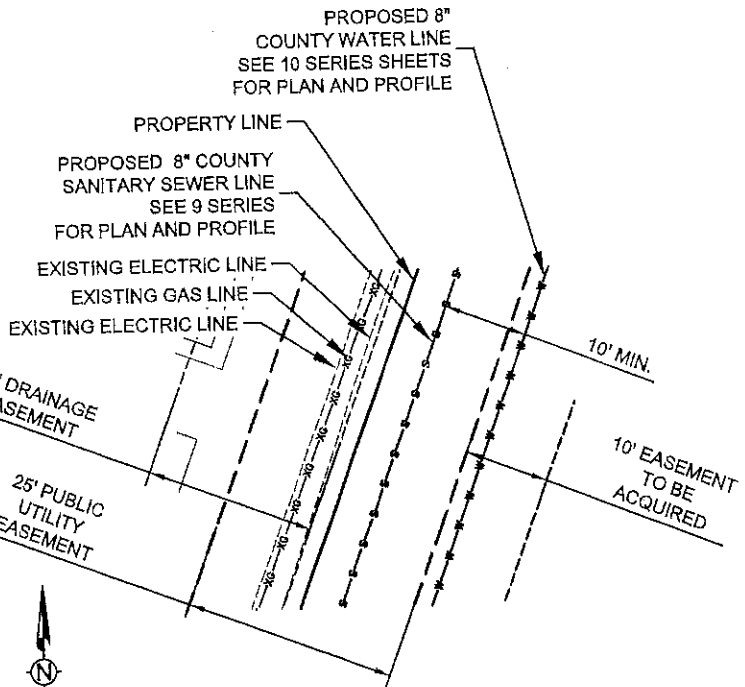
PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4				
FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2				
GEOMETRICS PLAN - PHASE 1				
DATE: FEBRUARY 2017	SCALE: 1" = 50'	SHEET: 5-5		

ENGINEER'S SEAL	
DATE	
REVISIONS	

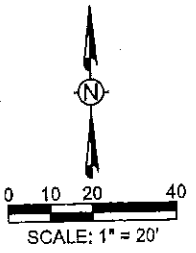
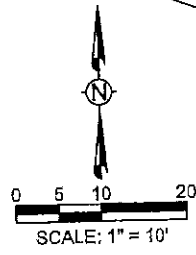


LEGEND	
	PROPOSED WATER LINE
	PROPOSED SANITARY SEWER LINE
	PROPOSED JOINT UTILITY TRENCH
	FUTURE SANITARY SEWER LINE
	FUTURE SANITARY SEWER LINE
	PROPOSED FIRE HYDRANT
	PROPOSED WATER METER
	PROPOSED WATER ELBOW
	PROPOSED WATER TEE FITTING
	PROPOSED CAP
	PROPOSED SAS MANHOLE

EXTREME CAUTION
EXTREME CAUTION IS TO BE USED WHEN WORKING NEAR EXISTING GAS LINES. CONTRACTOR TO VERIFY LOCATION IN FIELD PRIOR TO ANY EXCAVATION OR DEMOLITION. CONTRACTOR TO PROTECT LINES DURING ALL PHASES OF CONSTRUCTION.
EXTREME CAUTION



UTILITY BLOW-UP DETAIL
SCALE: 1" = 10'



MATCHLINE
SHEET 6-3

WET UTILITY PLAN - PHASE 1
SCALE: 1" = 20'

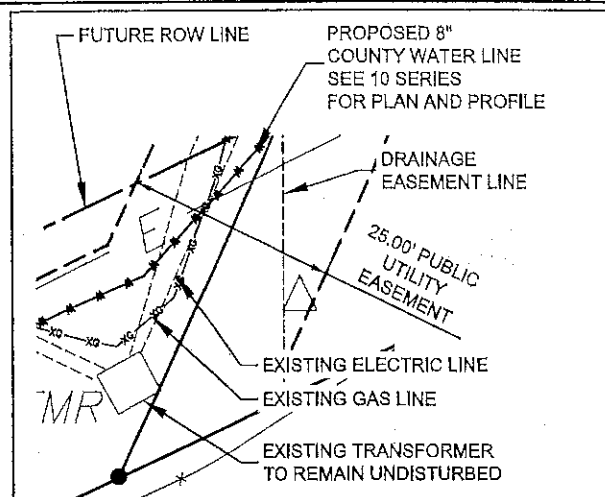
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PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
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FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

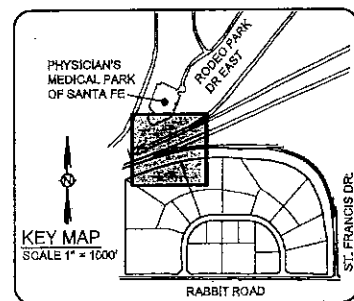
WET UTILITY PLAN - PHASE 1

DATE	SCALE	SHEET
FEBRUARY 2017	1" = 20'	6-2

ENGINEER'S SEAL	
DATE	
REVISIONS	



UTILITY BLOW-UP DETAIL
SCALE: 1" = 10'



WATER LINE CONSTRUCTION
OPTION 1 (USE EXISTING CONDUIT)

8" STEEL CARRIER PIPE
EXISTING 18" CMP

FUTURE PRIVATE 6" SANITARY SEWER LINE

PROPOSED 8" COUNTY SANITARY SEWER LINE
SEE 9 SERIES
FOR PLAN AND PROFILE

PROPOSED 8" COUNTY WATER LINE
SEE 10 SERIES SHEETS
FOR PLAN AND PROFILE

0 20 40 80
SCALE: 1" = 40'

WET UTILITY PLAN - PHASE 1
SCALE: 1" = 40'

MATCHLINE
SHEET 6-4

SHEET 6-2
MATCHLINE

SEE UTILITY BLOW-UP
DETAIL, THIS SHEET

WATER LINE CONSTRUCTION
OPTION 2 (NEW BORE)

EXISTING 36" CMP

8" STEEL CARRIER PIPE

EXISTING 24" CONCRETE CULVERT

16" STEEL CASING

BORE SANITARY SEWER LINE UNDER I-25
SEE DETAIL, THIS SHEET

RUN WATER LINE UNDER I-25 IN EXISTING 18" CMP
SEE DETAIL, THIS SHEET

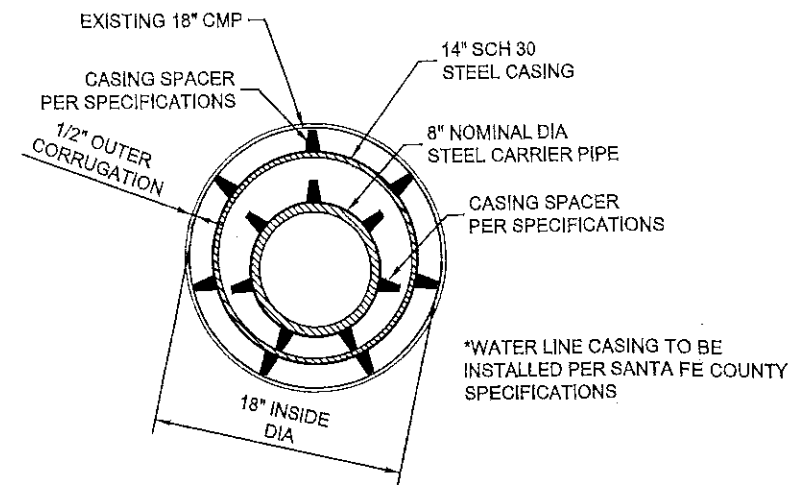
I-25 SOUTH BOUND

I-25 NORTH BOUND

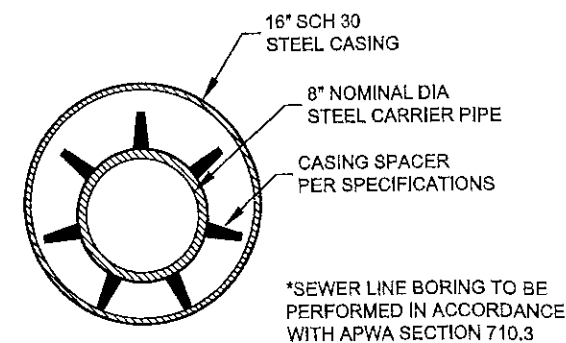
I-25 NORTH BOUND OFF RAMP

LOT LINE

PHASE 1 LOTS



CASING DETAIL FOR WATER LINE CONSTRUCTION OPTION 1



BORING DETAIL FOR SANITARY SEWER
AND WATER LINE CONSTRUCTION OPTION 2

LEGEND

- PROPOSED WATER LINE
- PROPOSED SANITARY SEWER LINE
- PROPOSED JOINT UTILITY TRENCH
- FUTURE SANITARY SEWER LINE
- FUTURE SANITARY SEWER LINE
- PROPOSED FIRE HYDRANT
- PROPOSED WATER METER
- PROPOSED WATER ELBOW
- PROPOSED WATER TEE FITTING
- PROPOSED CAP
- PROPOSED SAS MANHOLE

REVISIONS	DATE	ENGINEER'S SEAL

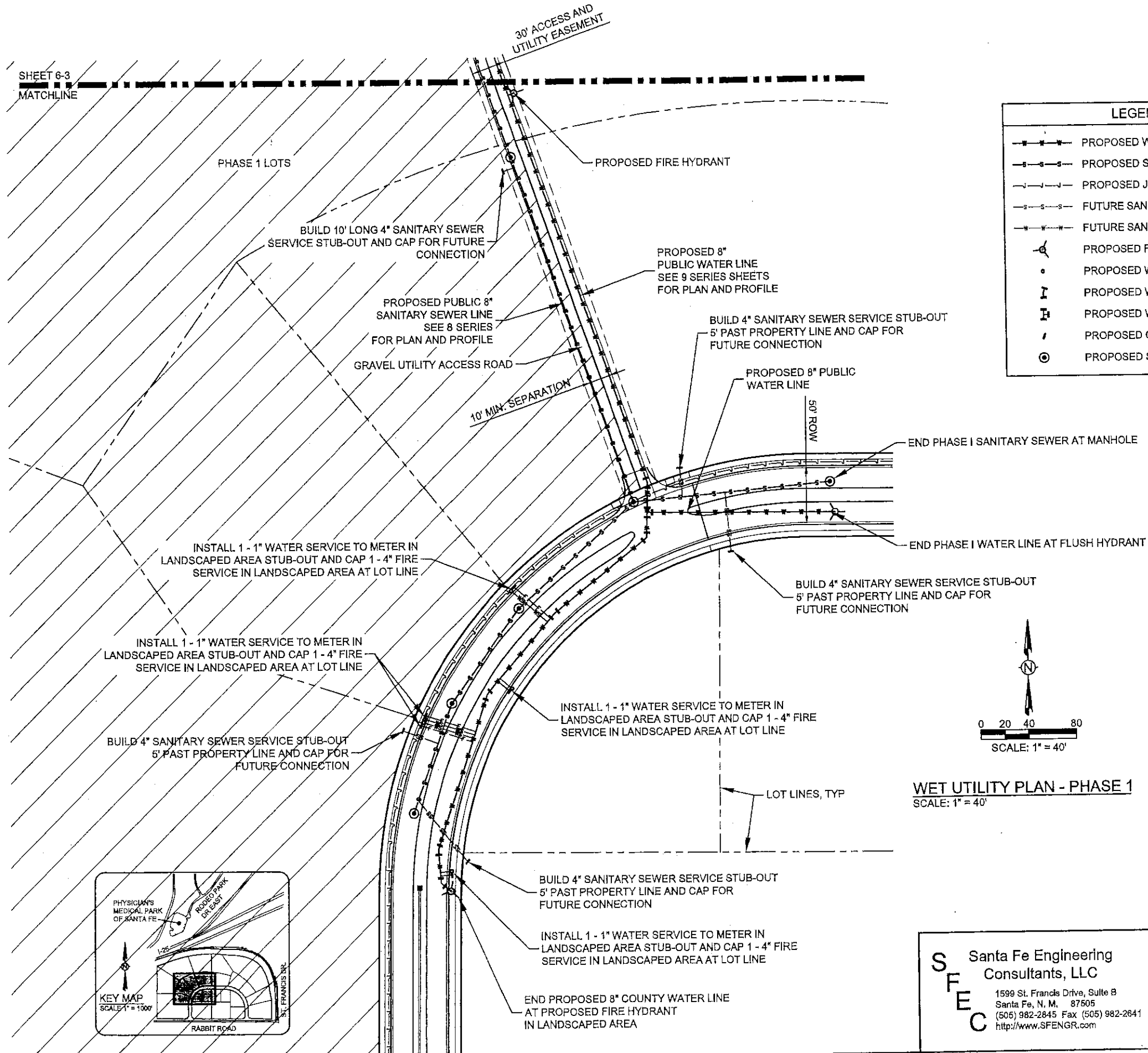
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PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

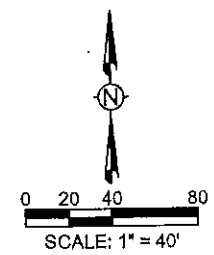
WET UTILITY PLAN - PHASE 1
(CONT'D)

DATE: FEBRUARY 2017 SCALE: 1" = 40' SHEET: 8-3

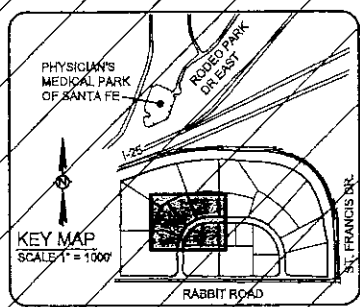
SHEET 6-3
MATCHLINE



LEGEND	
	PROPOSED WATER LINE
	PROPOSED SANITARY SEWER LINE
	PROPOSED JOINT UTILITY TRENCH
	FUTURE SANITARY SEWER LINE
	FUTURE SANITARY SEWER LINE
	PROPOSED FIRE HYDRANT
	PROPOSED WATER METER
	PROPOSED WATER ELBOW
	PROPOSED WATER TEE FITTING
	PROPOSED CAP
	PROPOSED SAS MANHOLE



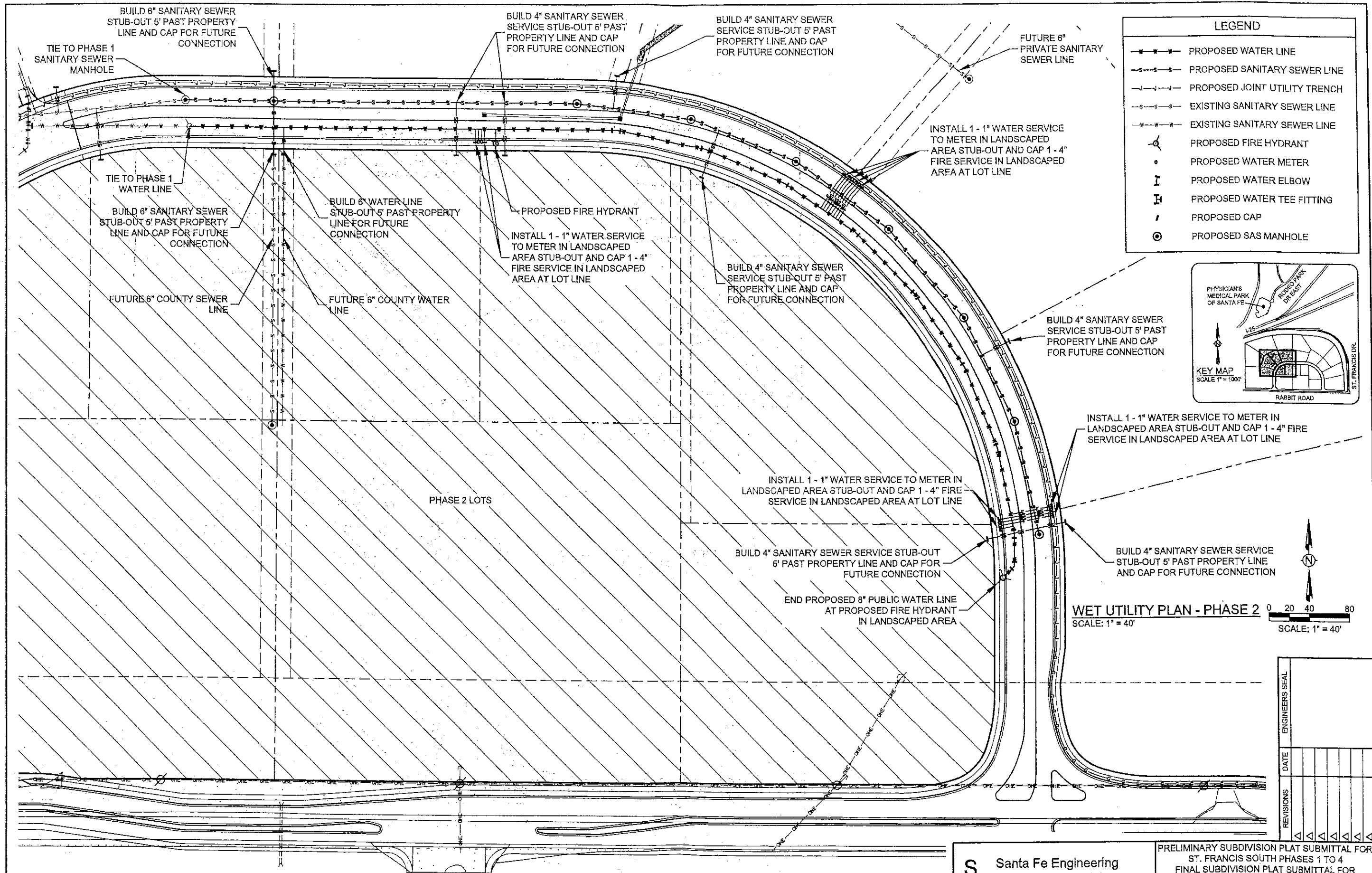
WET UTILITY PLAN - PHASE 1
SCALE: 1" = 40'



ENGINEER'S SEAL	
DATE	
REVISIONS	

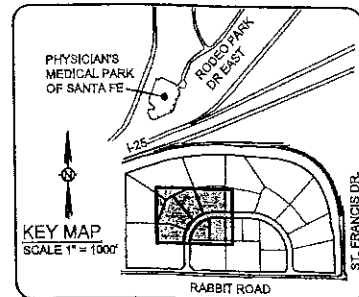
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FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2		
WET UTILITY PLAN - PHASE 1 (CONT'D)		
DATE: FEBRUARY 2017	SCALE: 1" = 40'	SHEET: 6-4



LEGEND

- +—+— PROPOSED WATER LINE
- s—s— PROPOSED SANITARY SEWER LINE
- j—j— PROPOSED JOINT UTILITY TRENCH
- s—s— EXISTING SANITARY SEWER LINE
- w—w— EXISTING SANITARY SEWER LINE
- ⊕ PROPOSED FIRE HYDRANT
- PROPOSED WATER METER
- ⌋ PROPOSED WATER ELBOW
- ⌋ PROPOSED WATER TEE FITTING
- / PROPOSED CAP
- ⊙ PROPOSED SAS MANHOLE



WET UTILITY PLAN - PHASE 2
 SCALE: 1" = 40'

ENGINEER'S SEAL	DATE	
	REVISIONS	

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 ST. FRANCIS SOUTH PHASES 1 & 2

WET UTILITY PLAN - PHASE 2

DATE: FEBRUARY 2017

SCALE: 1" = 40'

SHEET: 6-5

LEGEND:

PROPOSED JOINT TRENCH

PROPOSED GAS LINE

EXISTING GAS LINE

EXISTING OVERHEAD ELECTRIC

EXISTING TELECOMMUNICATIONS LINE

UTILITY CONNECTION OPTIONS:

ELECTRIC:

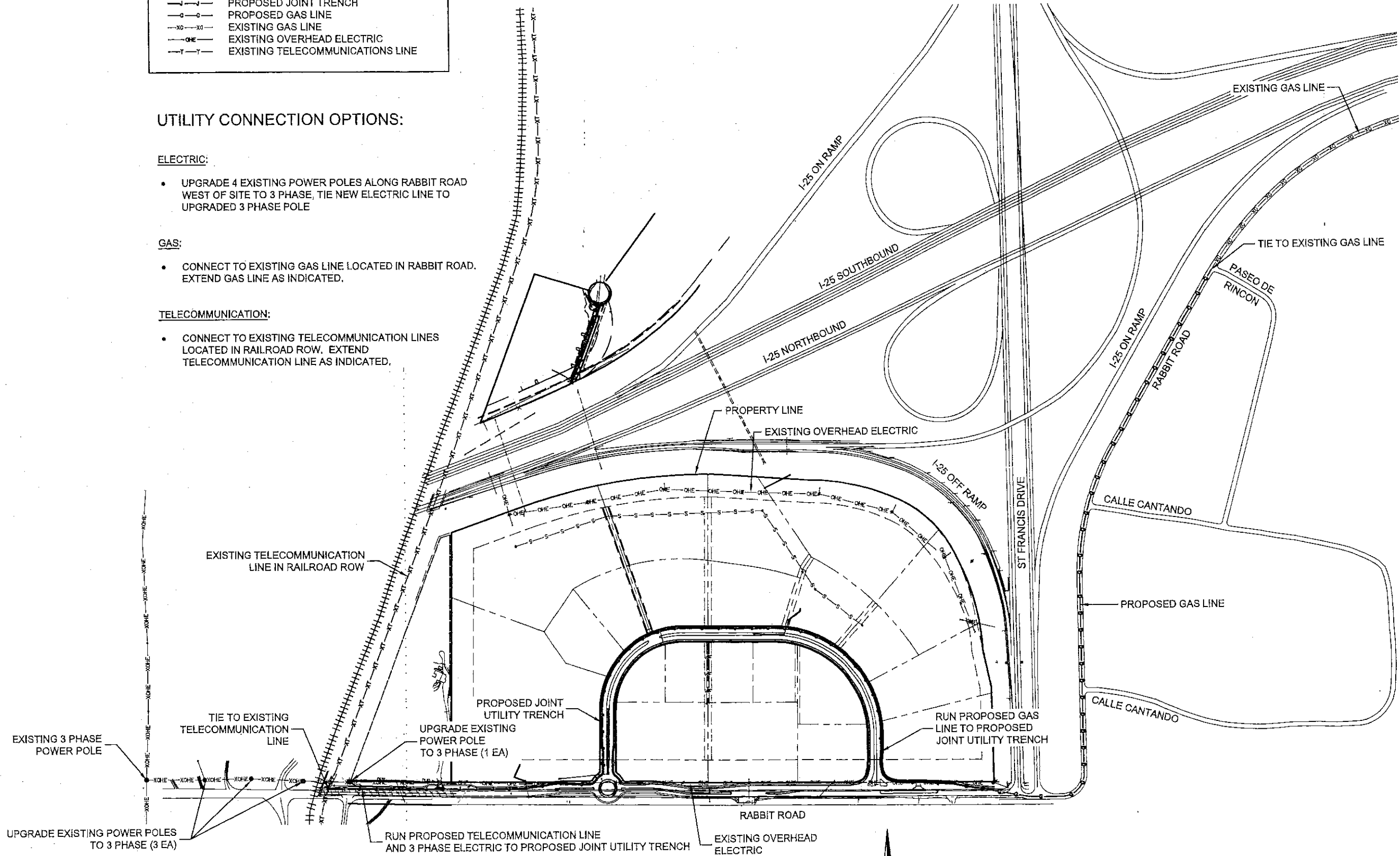
- UPGRADE 4 EXISTING POWER POLES ALONG RABBIT ROAD WEST OF SITE TO 3 PHASE, TIE NEW ELECTRIC LINE TO UPGRADED 3 PHASE POLE

GAS:

- CONNECT TO EXISTING GAS LINE LOCATED IN RABBIT ROAD. EXTEND GAS LINE AS INDICATED.

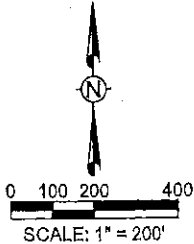
TELECOMMUNICATION:

- CONNECT TO EXISTING TELECOMMUNICATION LINES LOCATED IN RAILROAD ROW. EXTEND TELECOMMUNICATION LINE AS INDICATED.



UTILITY CONTACT INFORMATION		
COMPANY	CONTACT	PHONE NUMBER
SANTA FE COUNTY WATER AND WASTEWATER	MICHAEL K. KELLY	(505) 992-3023
PNM	DOUG BEJOS	(505) 473-3237
NEW MEXICO GAS COMPANY	FRANK ARAGON	(505) 470-0668
CENTURY LINK	DOUG DALE	(505) 473-2194
COMCAST	DAVID AIKIN	(505) 474-7886

UTILITY PLAN - DRY UTILITIES
SCALE: 1" = 200'



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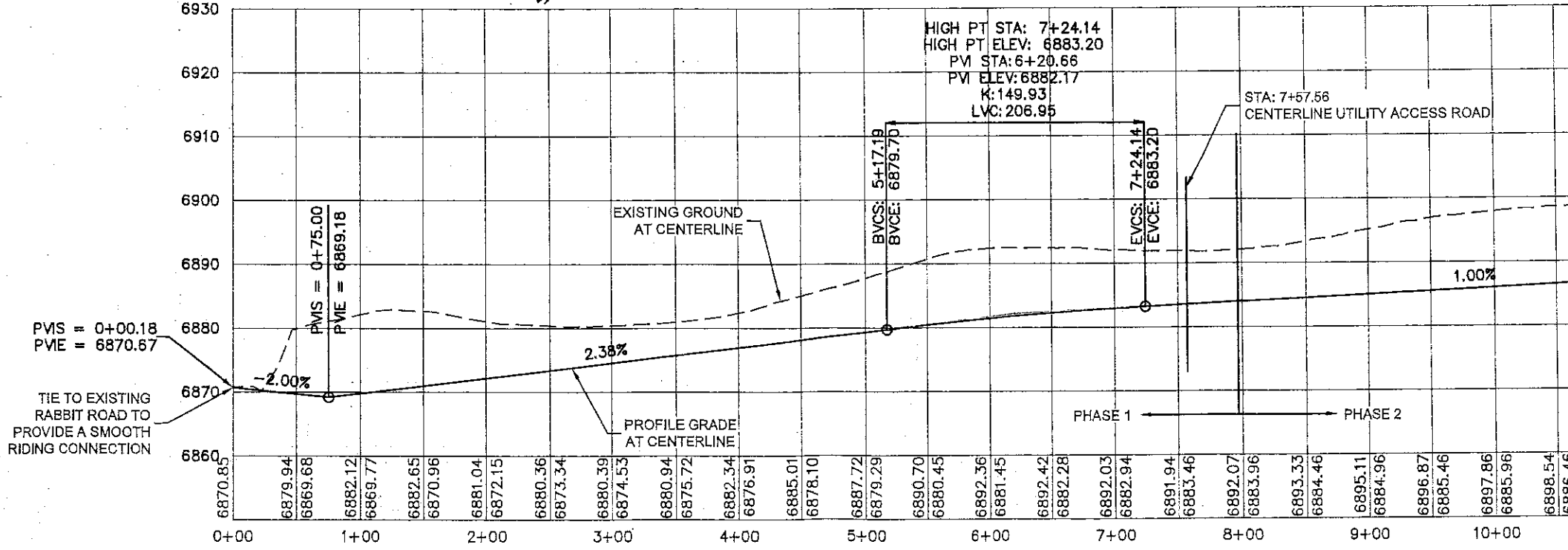
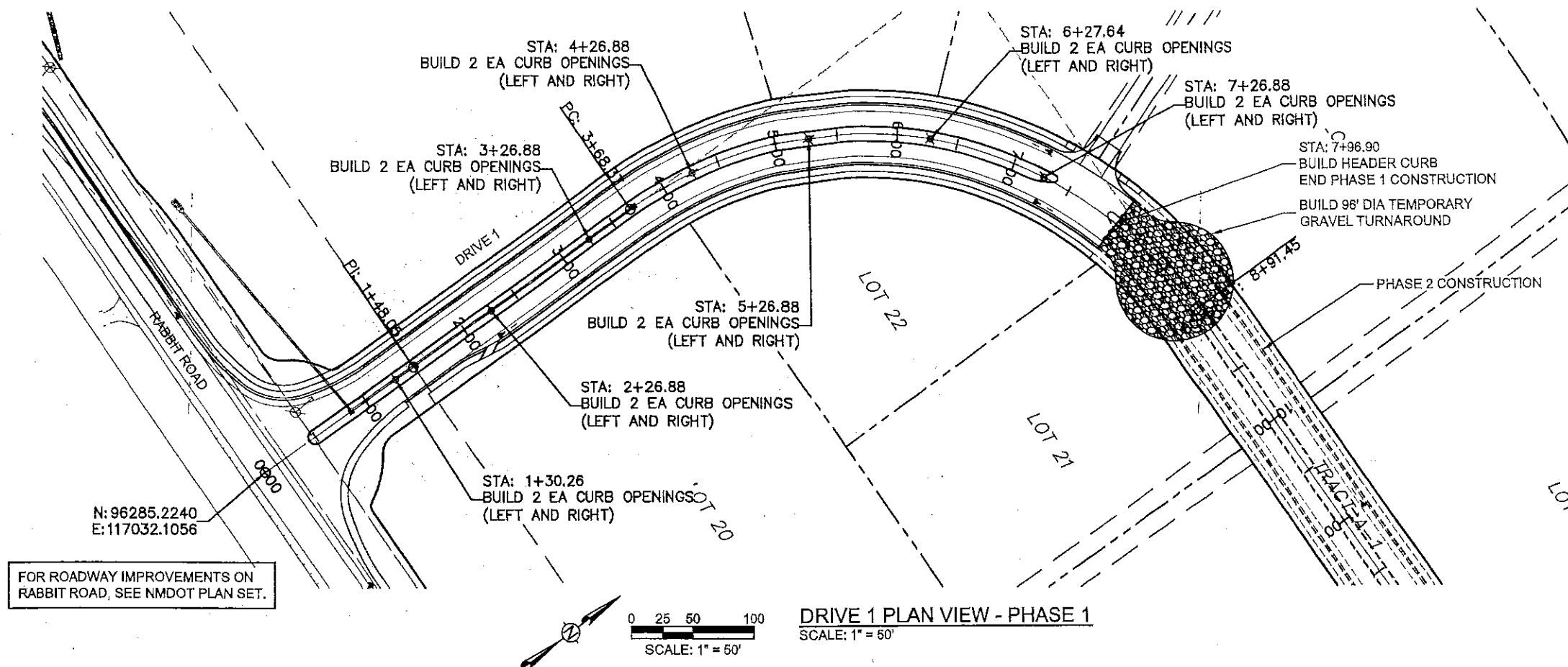
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ST. FRANCIS SOUTH PHASES 1 & 2

DRY UTILITY PLAN

DATE: FEBRUARY 2017

SCALE: 1"=200'

SHEET: 6-6



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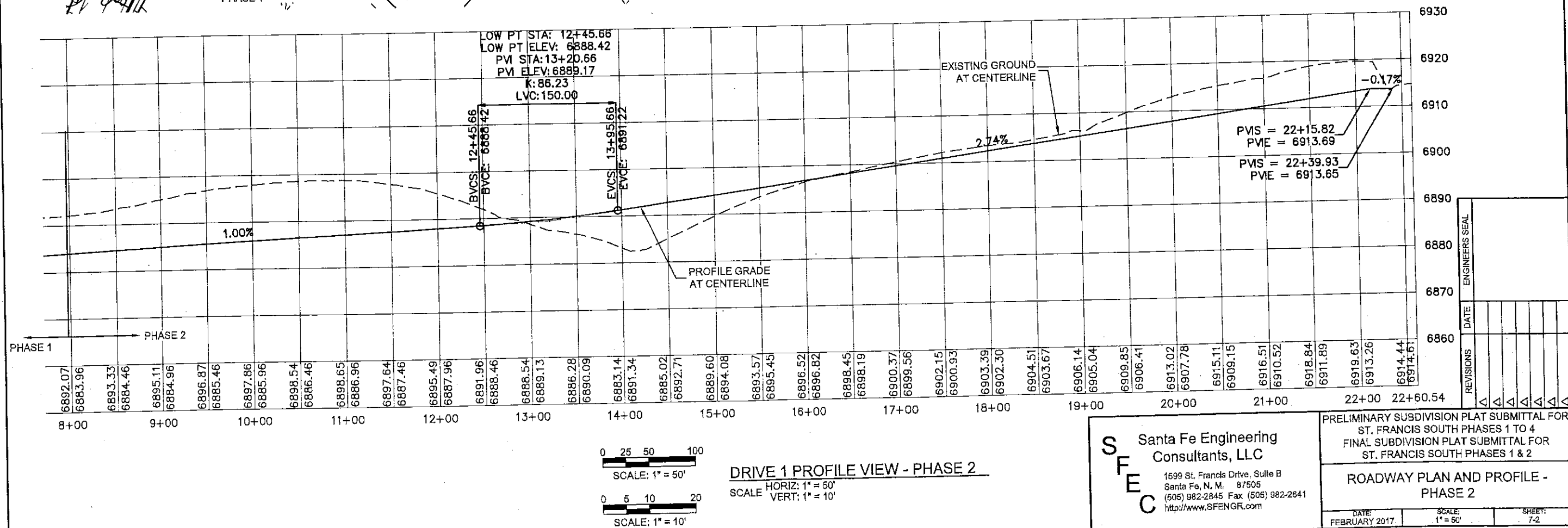
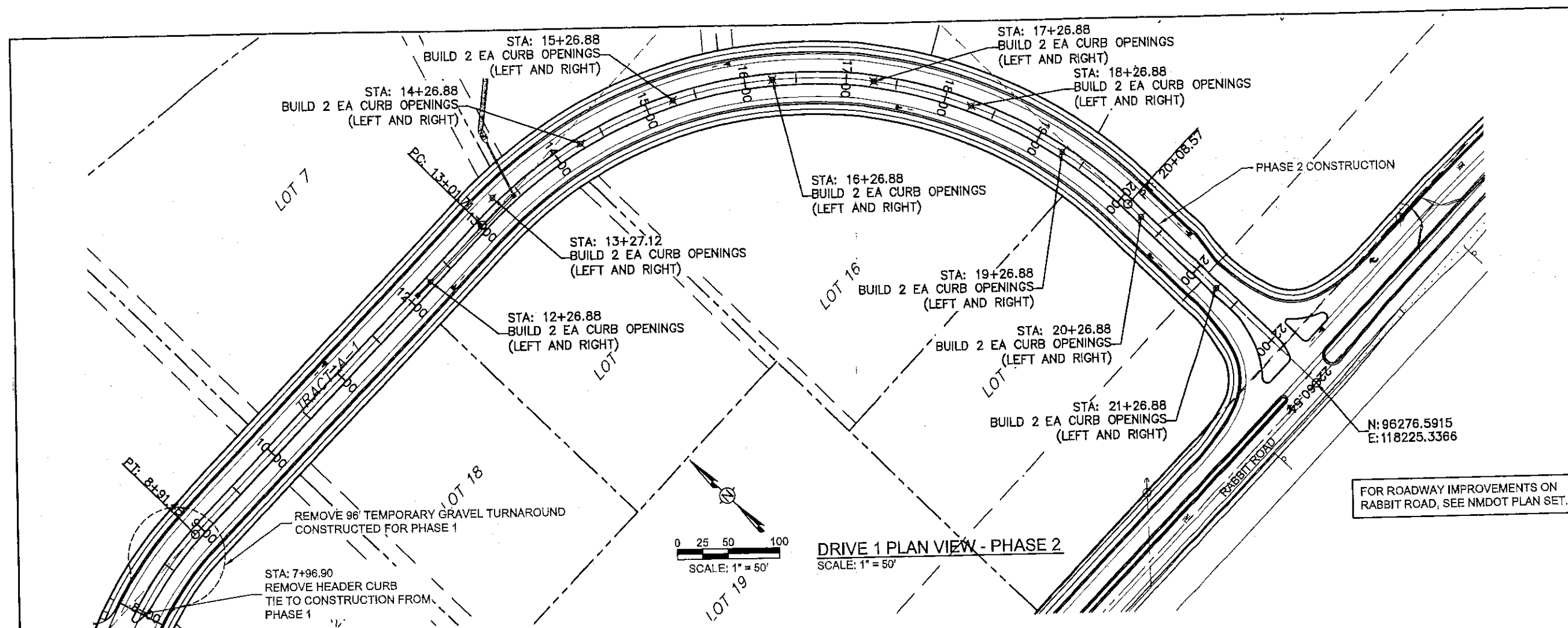
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**ROADWAY PLAN AND PROFILE -
PHASE 1**

DATE: FEBRUARY 2017

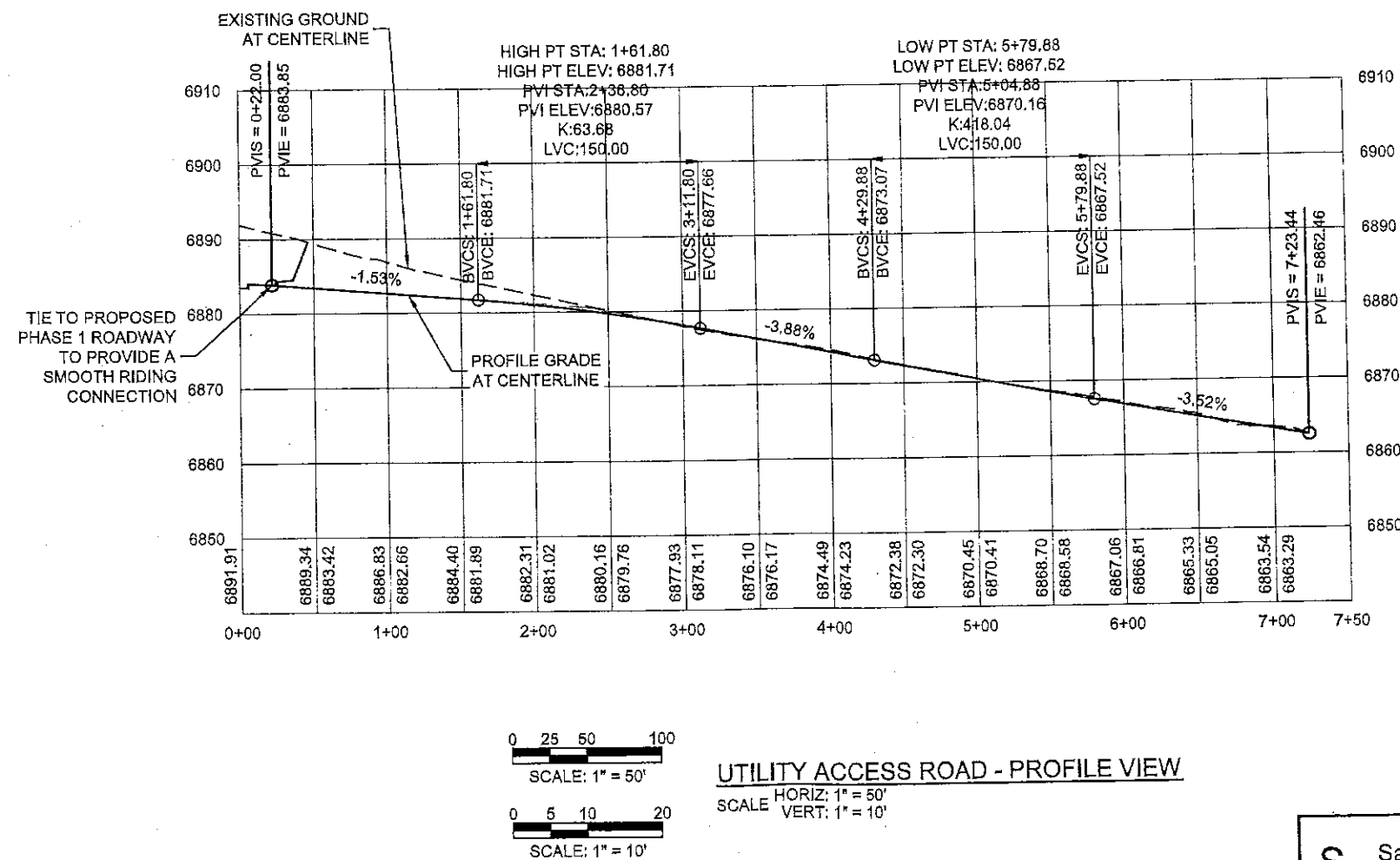
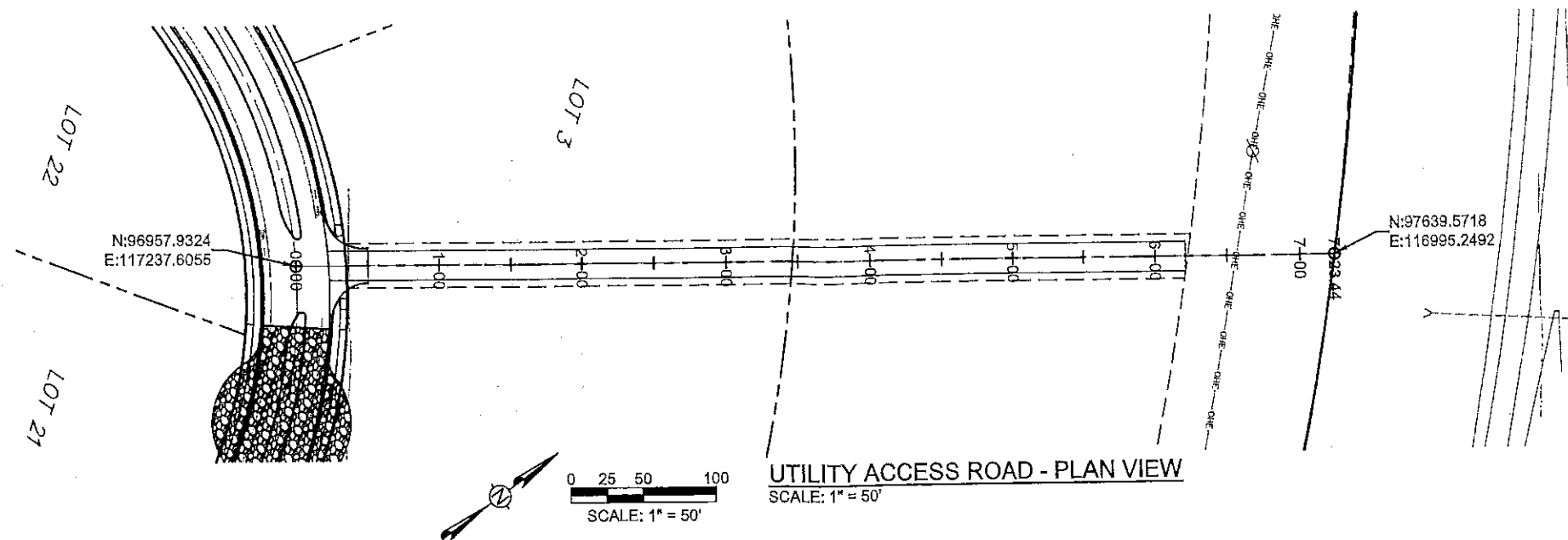
SCALE: 1" = 50'

SHEET: 7-1



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PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4		
FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2		
ROADWAY PLAN AND PROFILE - PHASE 2		
DATE: FEBRUARY 2017	SCALE: 1" = 50'	SHEET: 7-2



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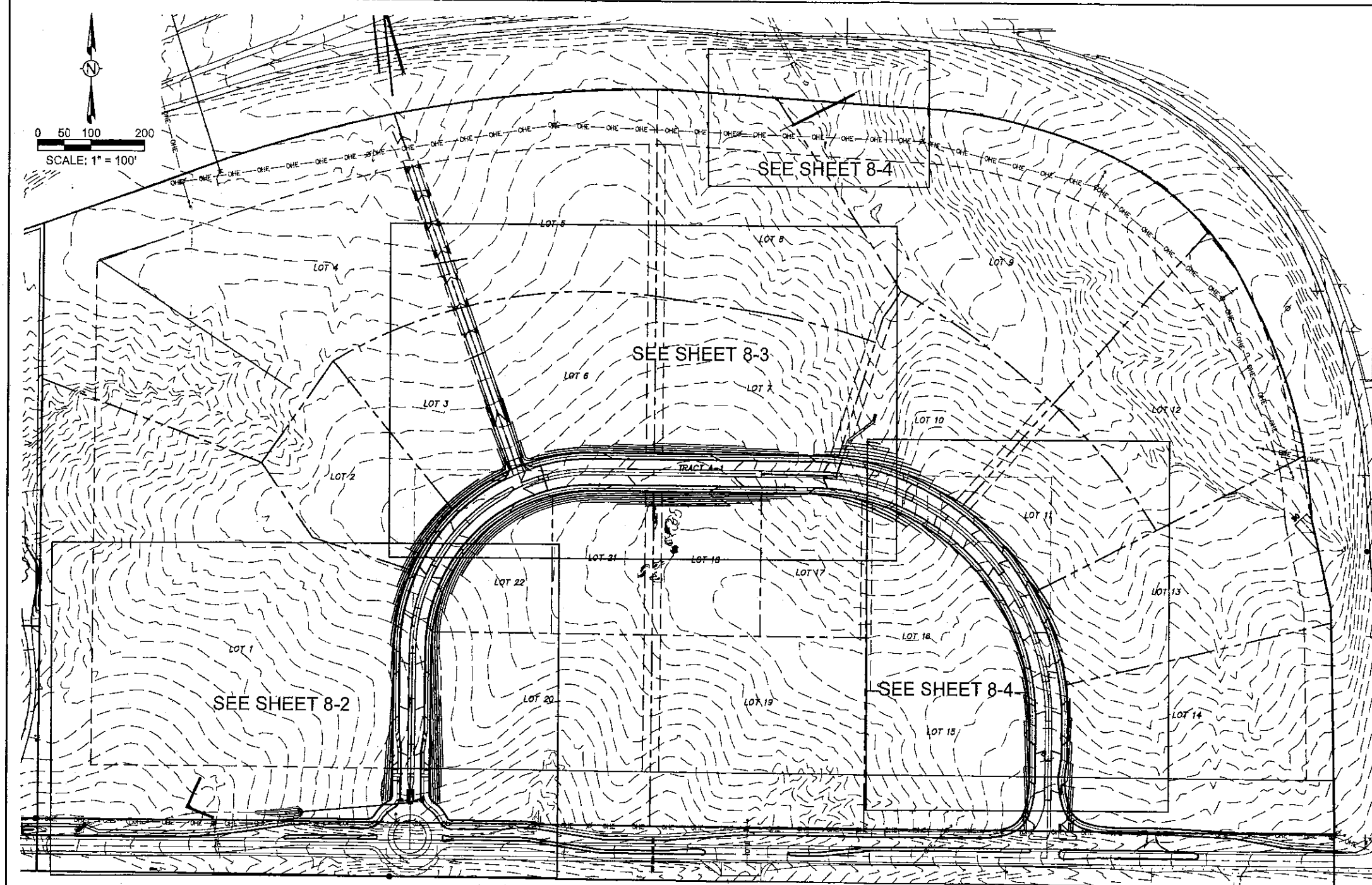
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UTILITY ACCESS ROAD PLAN AND PROFILE

DATE: FEBRUARY 2017

SCALE: 1" = 50'

SHEET: 7-3



PONDING CALCULATIONS FOR ROADWAYS PHASE 1

THE PROPOSED DEVELOPMENT AT THE SITE CONSISTS OF THE FOLLOWING:

PROPOSED IMPERVIOUS AREAS (ROOFS, SIDEWALKS, DRIVEWAYS, ETC.) TYPE "C" = 36,704 SQ. FT.

PROPOSED GRAVEL DRIVE TYPE "C" = 19,601 SQ. FT.

THE CITY OF SANTA FE TERRAIN MANAGEMENT REGULATIONS REQUIRE THAT PEAK DISCHARGE AFTER DEVELOPMENT MUST NOT EXCEED PEAK DISCHARGE BEFORE DEVELOPMENT. EXCESS RUNOFF FROM NEW IMPERVIOUS AREAS (ROOFS, SIDEWALKS, PATIOS, ETC.) WILL BE MITIGATED BY TEMPORARY DETENTION IN LANDSCAPED PONDING AREAS WITH CONTROLLED RELEASE. CALCULATIONS TO DETERMINE VOLUMES ARE AS FOLLOWS:

ACCORDING TO THE USDA & NRCS WEB SOIL SURVEY, THE AVERAGE SOILS ON THE SITE ARE IN HYDROLOGIC GROUP C.

PRIOR TO THE DEVELOPMENT AT THE SITE, THE UNDEVELOPED AREA COULD BE CONSIDERED "HERBACEOUS", IN FAIR CONDITION. ACCORDING TO TR 55, THE DIRECT RUNOFF FOR THE 100 YEAR PRECIPITATION IS AS FOLLOWS:

	CN	DIRECT RUNOFF (INCHES)
HERBACEOUS HYDROLOGIC SOIL GROUP C COVER FAIR	81	1.46

THE DIRECT RUNOFF FOR THE PROPOSED IMPERVIOUS AREAS FOR THE 100 YEAR PRECIPITATION IS AS FOLLOWS:

	CN	DIRECT RUNOFF (INCHES)
IMPERVIOUS AREAS (ROADWAYS) HYDROLOGIC SOIL GROUP C	98	2.96

THE DIRECT RUNOFF FOR THE PROPOSED GRAVEL AREAS FOR THE 100 YEAR PRECIPITATION IS AS FOLLOWS:

	CN	DIRECT RUNOFF (INCHES)
GRAVEL AREAS (ROADWAYS) HYDROLOGIC SOIL GROUP C	89	2.07

TO MITIGATE THE PROPOSED IMPERVIOUS AREAS (ROADWAYS), THE DIFFERENCE IN DIRECT RUNOFF FROM THE PRE-DEVELOPMENT CONDITIONS AND POST DEVELOPMENT CONDITIONS IS CALCULATED. THE PONDING VOLUME IS CALCULATED AS FOLLOWS:

2.96" DIRECT RUNOFF IMPERVIOUS AREAS
1.46" DIRECT RUNOFF PRE-DEVELOPMENT
STORE 1.50" OF RAINFALL FOR IMPERVIOUS AREAS

IMPERVIOUS AREAS = 36,704 SQ. FT.
POND 1.50" FROM ALL IMPERVIOUS AREAS

THE TOTAL VOLUME OF WATER TO BE STORED FOR IMPERVIOUS AREA:

$$\text{REQUIRED VOLUME} = \frac{(1.50" \times 36,704 \text{ SQ. FT.})}{12}$$

$$= 4,588 \text{ CU. FT.}$$

TO MITIGATE THE PROPOSED GRAVEL AREAS (ROADWAYS), THE DIFFERENCE IN DIRECT RUNOFF FROM THE PRE-DEVELOPMENT CONDITIONS AND POST DEVELOPMENT CONDITIONS IS CALCULATED. THE PONDING VOLUME IS CALCULATED AS FOLLOWS:

2.07" DIRECT RUNOFF GRAVEL AREAS
1.46" DIRECT RUNOFF PRE-DEVELOPMENT
STORE 0.61" OF RAINFALL FOR IMPERVIOUS AREAS

IMPERVIOUS AREAS = 19,601 SQ. FT.
POND 0.61" FROM ALL IMPERVIOUS AREAS

THE TOTAL VOLUME OF WATER TO BE STORED FOR IMPERVIOUS AREA:

$$\text{REQUIRED VOLUME} = \frac{(0.61" \times 19,601 \text{ SQ. FT.})}{12}$$

$$= 996 \text{ CU. FT.}$$

TOTAL PONDING VOLUME REQUIRED = 5,584 CU. FT.
TOTAL VOLUME OF PONDING PROVIDED = 5,600 CU. FT.

GRADING AND DRAINAGE PLAN

SCALE: 1" = 100'

PONDING CALCULATIONS FOR ROADWAYS PHASE 2

THE PROPOSED DEVELOPMENT AT THE SITE CONSISTS OF THE FOLLOWING:

PROPOSED GRAVEL TRAIL = 74,968 SQ. FT.

THE CITY OF SANTA FE TERRAIN MANAGEMENT REGULATIONS REQUIRE THAT PEAK DISCHARGE AFTER DEVELOPMENT MUST NOT EXCEED PEAK DISCHARGE BEFORE DEVELOPMENT. EXCESS RUNOFF FROM NEW GRAVEL TRAIL WILL BE MITIGATED BY TEMPORARY DETENTION IN LANDSCAPED PONDING AREAS WITH CONTROLLED RELEASE. CALCULATIONS TO DETERMINE VOLUMES ARE AS FOLLOWS:

ACCORDING TO THE USDA & NRCS WEB SOIL SURVEY, THE AVERAGE SOILS ON THE SITE ARE IN HYDROLOGIC GROUP C.

PRIOR TO THE DEVELOPMENT AT THE SITE, THE UNDEVELOPED AREA COULD BE CONSIDERED "HERBACEOUS", IN FAIR CONDITION. ACCORDING TO TR 55, THE DIRECT RUNOFF FOR THE 100 YEAR PRECIPITATION IS AS FOLLOWS:

	CN	DIRECT RUNOFF (INCHES)
HERBACEOUS HYDROLOGIC SOIL GROUP C COVER FAIR	81	1.46

THE DIRECT RUNOFF FOR THE IMPERVIOUS AREAS FOR THE 100 YEAR PRECIPITATION IS AS FOLLOWS:

	CN	DIRECT RUNOFF (INCHES)
IMPERVIOUS AREAS (ROADWAYS) HYDROLOGIC SOIL GROUP C	98	2.96

TO MITIGATE THE PROPOSED IMPERVIOUS AREAS, THE DIFFERENCE IN DIRECT RUNOFF FROM THE PRE-DEVELOPMENT CONDITIONS AND POST DEVELOPMENT CONDITIONS IS CALCULATED. THE PONDING VOLUME IS CALCULATED AS FOLLOWS:

2.96" DIRECT RUNOFF IMPERVIOUS AREAS
1.46" DIRECT RUNOFF PRE-DEVELOPMENT
STORE 1.50" OF RAINFALL FOR IMPERVIOUS AREAS

IMPERVIOUS AREAS = 74,968 SQ. FT.
POND 1.50" FROM ALL IMPERVIOUS AREAS

THE TOTAL VOLUME OF WATER TO BE STORED FOR IMPERVIOUS AREA:

$$\text{REQUIRED VOLUME} = \frac{(1.50" \times 74,968 \text{ SQ. FT.})}{12}$$

$$= 9,371 \text{ CU. FT.}$$

TOTAL PONDING VOLUME REQUIRED = 9,371 CU. FT.
TOTAL VOLUME OF PONDING PROVIDED = 29,350 CU. FT.

GRADING NOTES

- THE LIMITS OF CONSTRUCTION AND LOCATIONS OF THE CONTRACTORS STAGING AREAS SHALL BE IDENTIFIED BY THE CONTRACTOR AND APPROVED BY THE OWNER. THE LIMITS OF CONSTRUCTION SHALL BE CLEARLY DELINEATED AND SHALL BE THE MINIMUM REQUIRED TO MAINTAIN ALL WORKERS IN A SAFE CONDITION, TO PROVIDE ACCESS, AND TO MEET O.S.H.A. REGULATIONS.
- CONTRACTOR IS REQUIRED TO PROVIDE DUST AND EROSION CONTROL PROTECTION.
- GRADING SETBACKS TO EXTERIOR PROPERTY LINES ARE AS FOLLOWS:
TOP OF VERTICAL SLOPE = ONE FIFTH THE VERTICAL HEIGHT OF CUT WITH A MINIMUM OF 2 FEET AND A MAXIMUM OF 15 FEET.
TOE OF FILL SLOPE = ONE HALF THE HEIGHT OF THE SLOPE WITH A MINIMUM OF 2 FEET AND A MAXIMUM OF 15 FEET.
- THE SURVEYOR SHALL VERIFY SETBACKS, TIE IN ELEVATIONS, AND TOPOGRAPHY WHEN STAKING PROJECT.
- UNSATURABLE MATERIAL FROM SITE GRADING AND REMOVAL OPERATIONS, SHALL BE DISPOSED OF AT AN APPROVED LANDFILL.
- ALL DISTURBED AREAS SHALL BE REVEGETATED AND COORDINATED WITH THE LANDSCAPING PLAN.
- ALL FILL MATERIAL SHALL MEET THE REQUIREMENTS OF THE N.M.D.O.T. OR SITE SPECIFIC GEOTECHNICAL SPECIFICATIONS. THE FILL SHALL BE COMPACTED AS PER N.M.D.O.T. SPECIFICATIONS.
- THE MAXIMUM CROSS SLOPE OF WALKING SURFACES SHALL NOT EXCEED 2%.
- INSTALL SILT FENCE AROUND THE SITE.

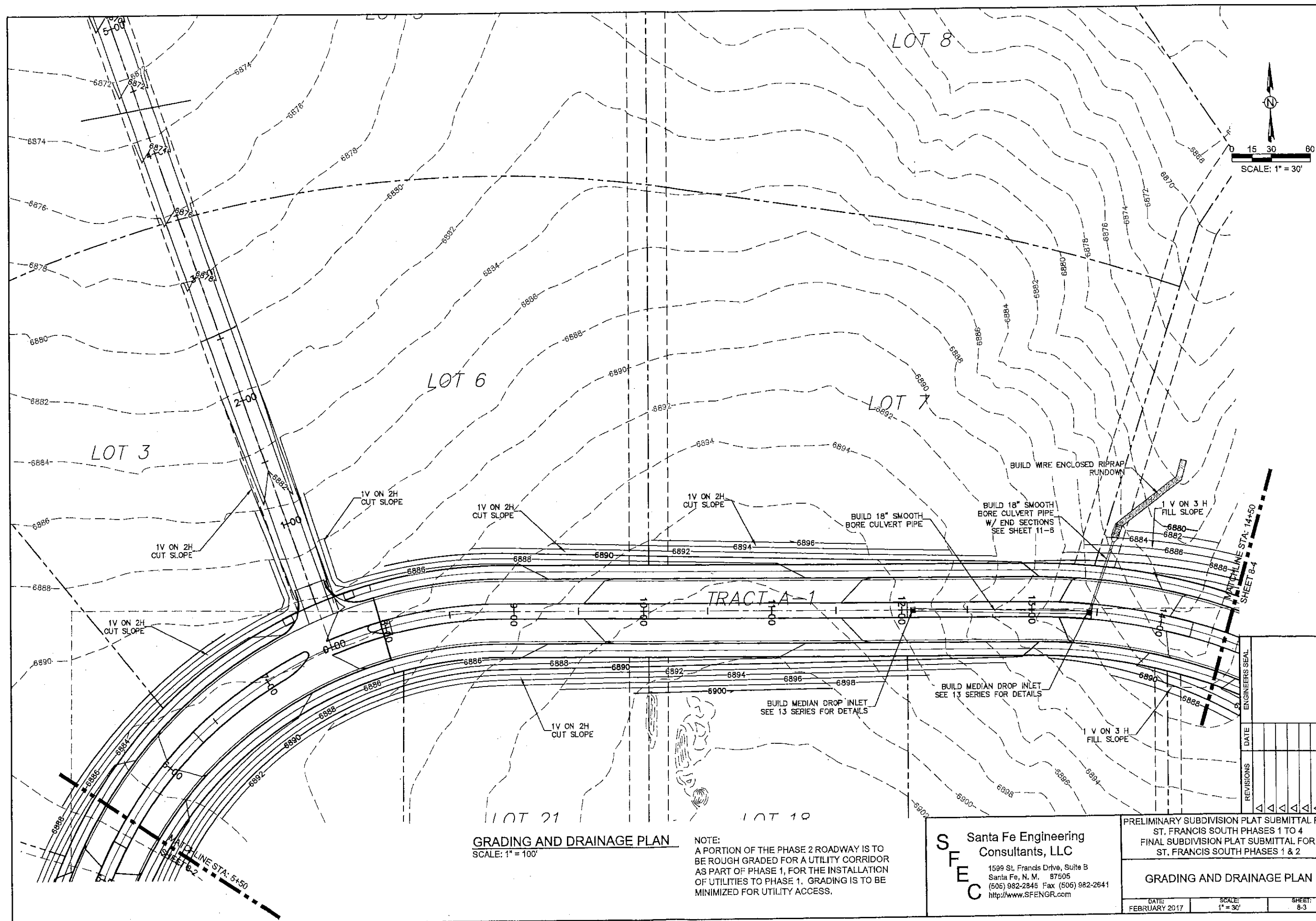
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OVERALL GRADING AND DRAINAGE PLAN

DATE: FEBRUARY 2017 SCALE: 1" = 30' SHEET: 8-1



GRADING AND DRAINAGE PLAN
SCALE: 1" = 100'

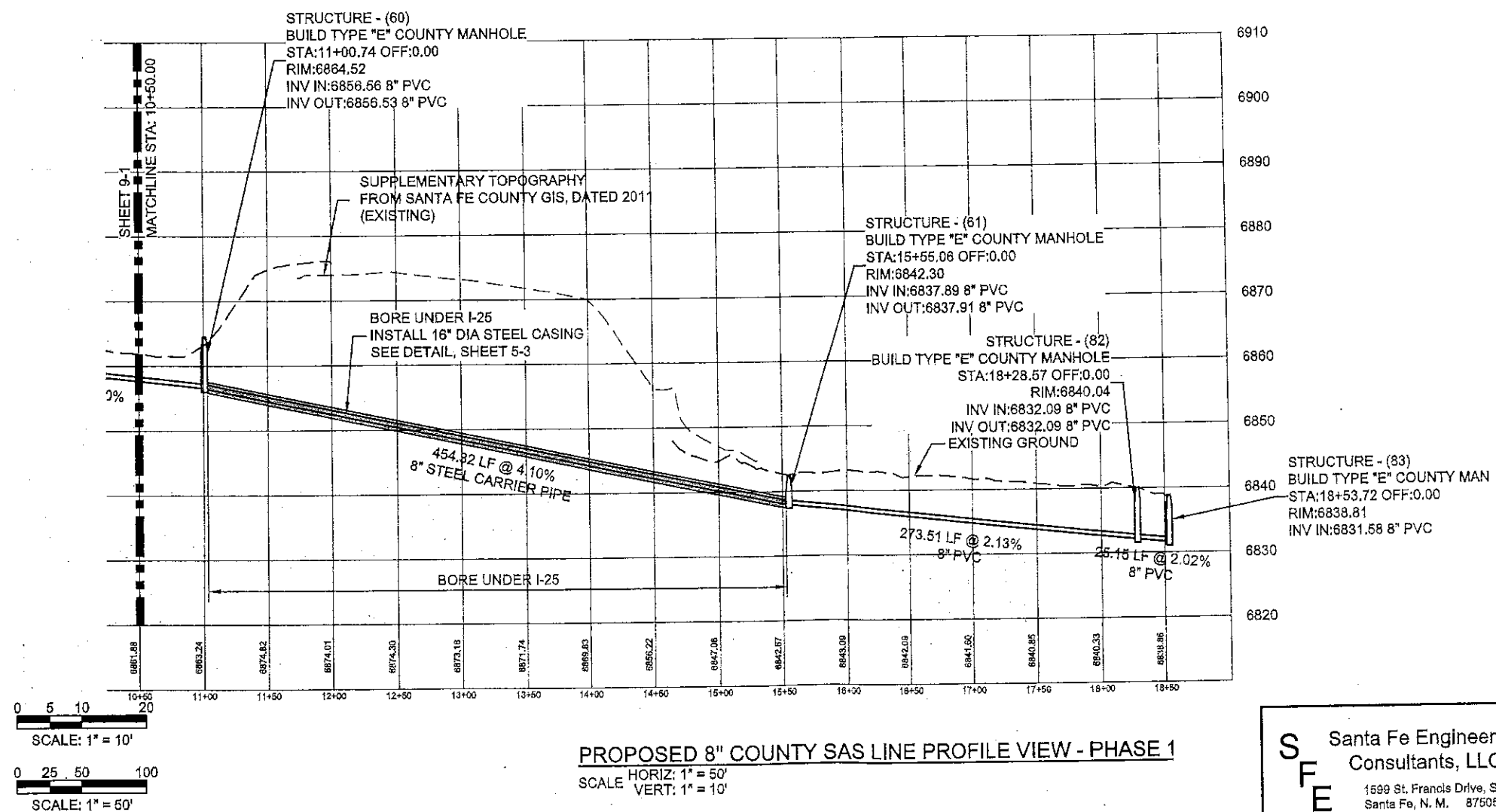
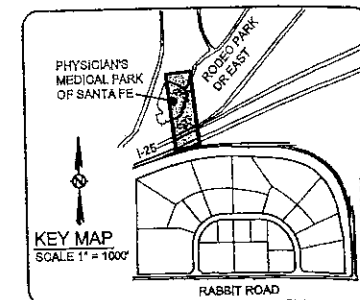
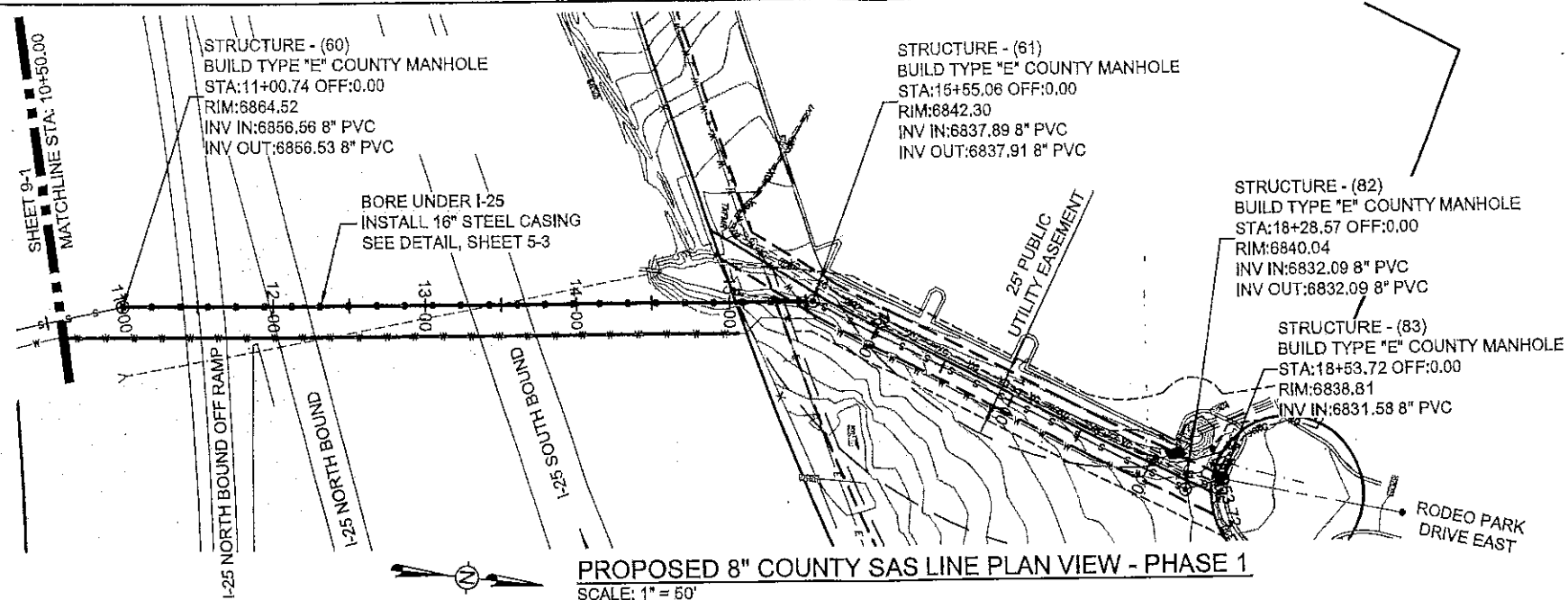
NOTE:
A PORTION OF THE PHASE 2 ROADWAY IS TO
BE ROUGH GRADED FOR A UTILITY CORRIDOR
AS PART OF PHASE 1, FOR THE INSTALLATION
OF UTILITIES TO PHASE 1. GRADING IS TO BE
MINIMIZED FOR UTILITY ACCESS.

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GRADING AND DRAINAGE PLAN

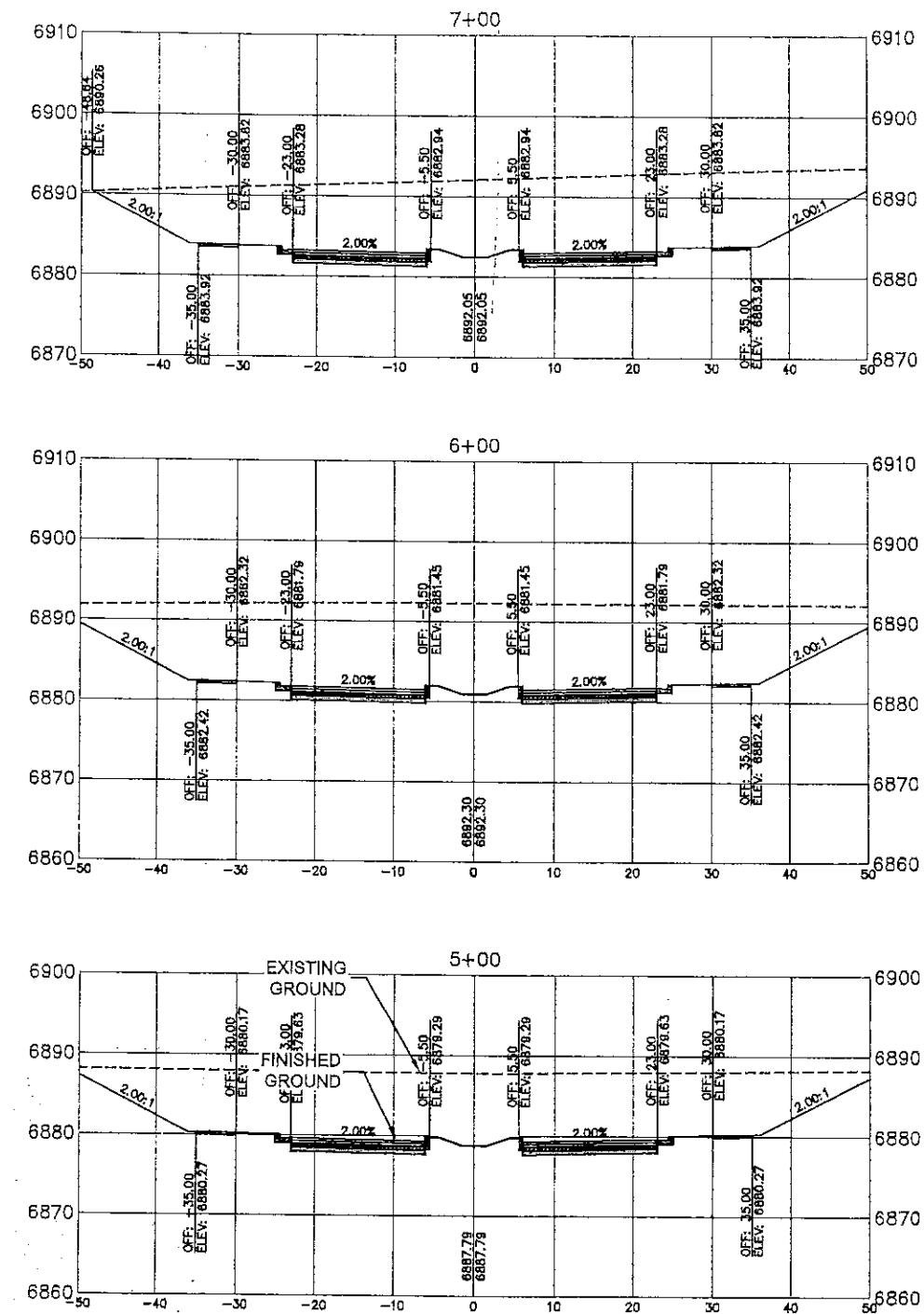
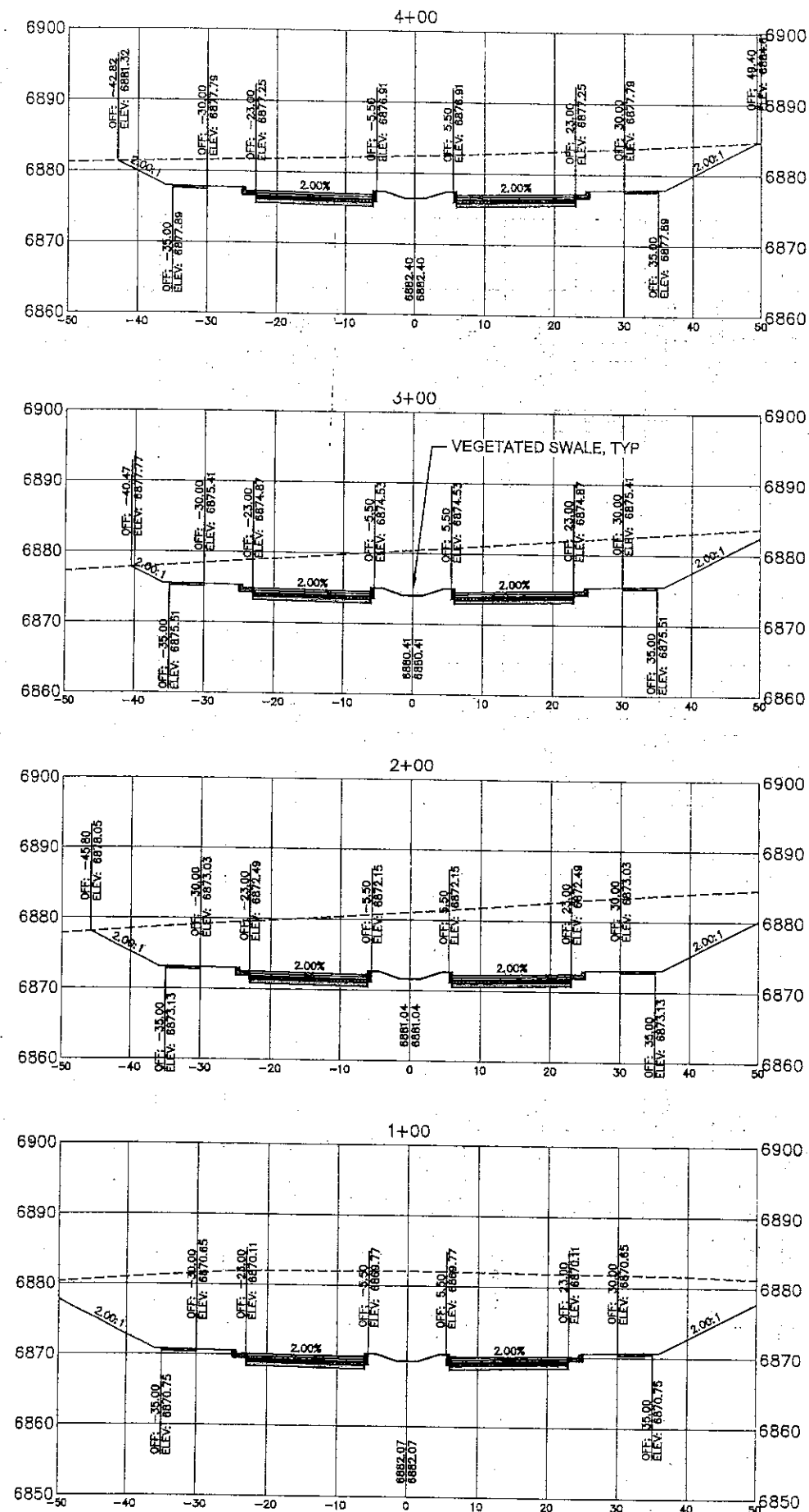
DATE	SCALE	SHEET
FEBRUARY 2017	1" = 30'	8-3



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FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2	
SANITARY SEWER PLAN AND PROFILE - PHASE 1 (CONT'D)	
DATE: FEBRUARY 2017	SHEET: 9-2

ENGINEER'S SEAL	
DATE	
REVISIONS	



0 5 10 20
SCALE: 1" = 10'

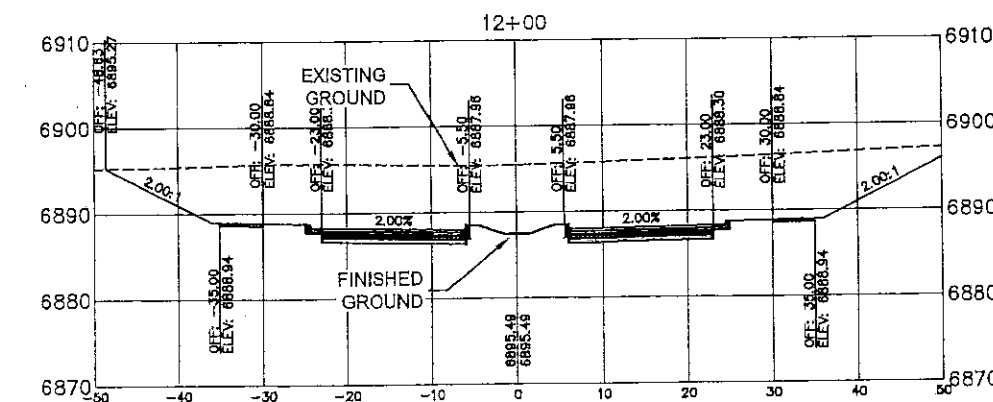
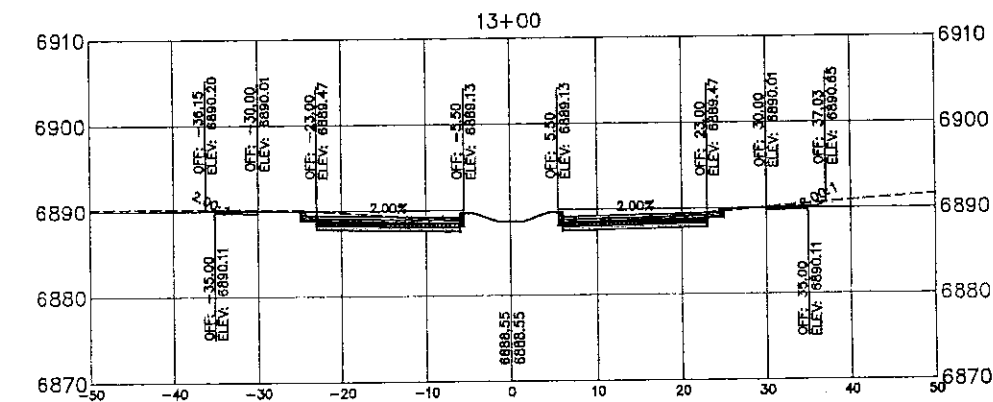
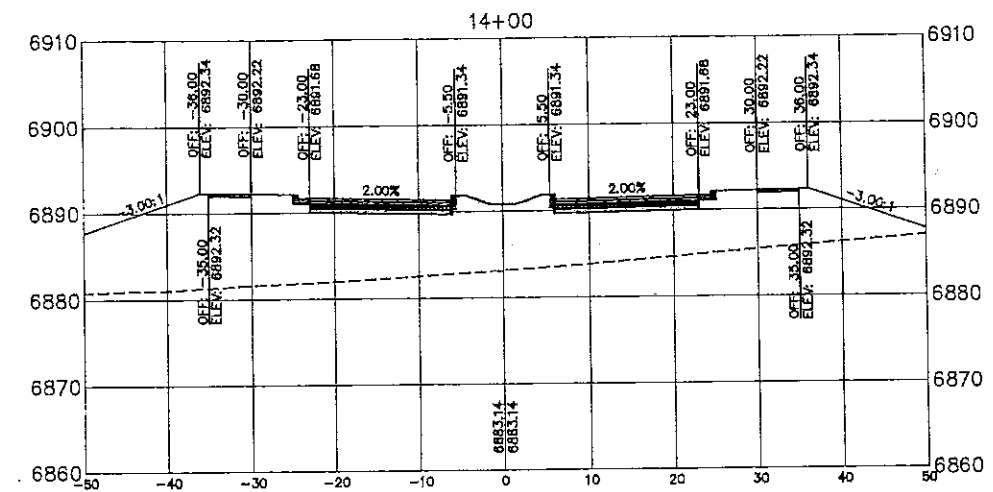
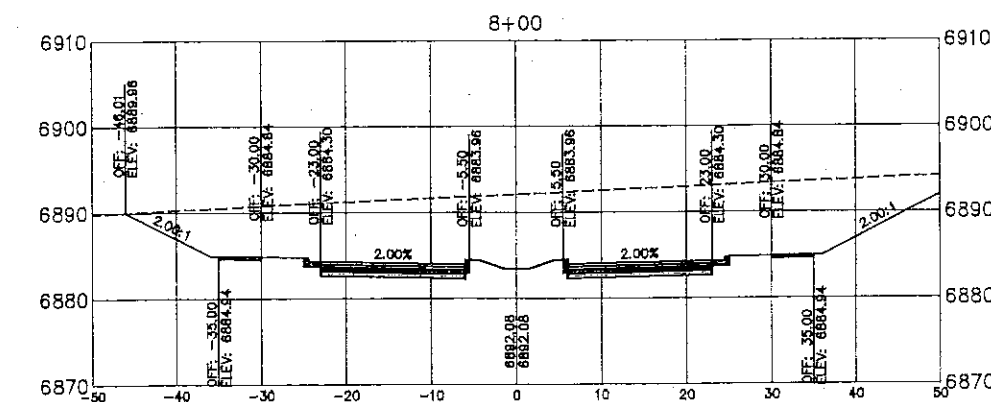
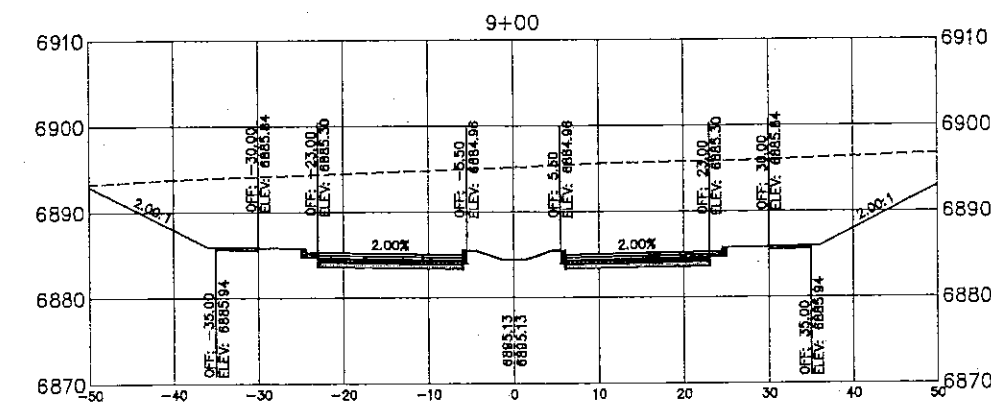
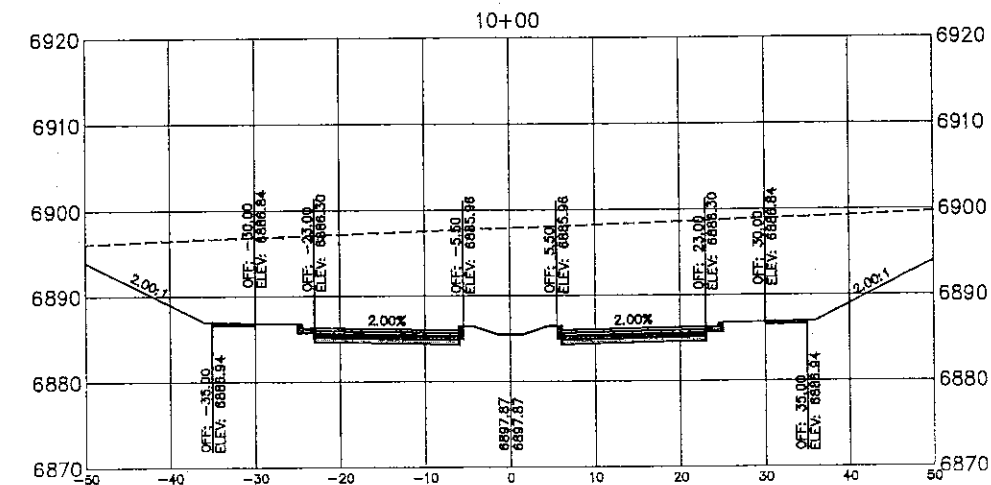
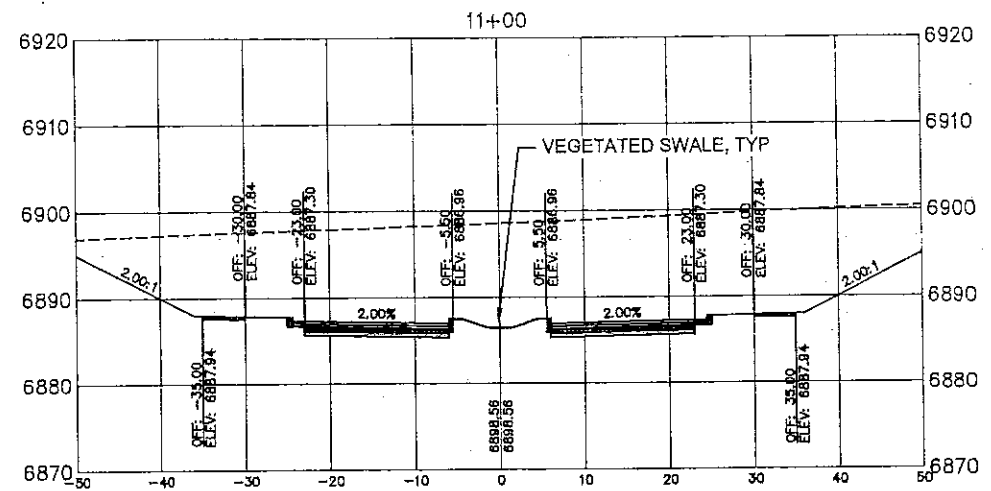
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ROADWAY CROSS SECTIONS

DATE: FEBRUARY 2017 SCALE: 1" = 10' SHEET: 11-1

REVISIONS	DATE	ENGINEERS SEAL

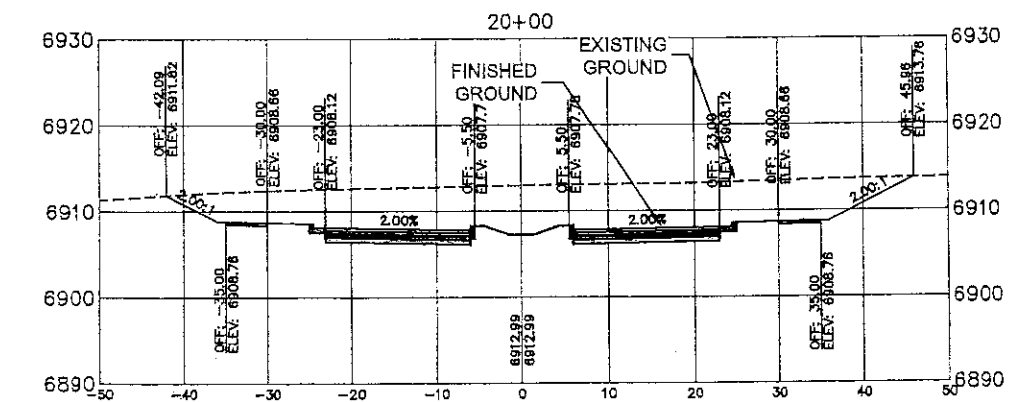
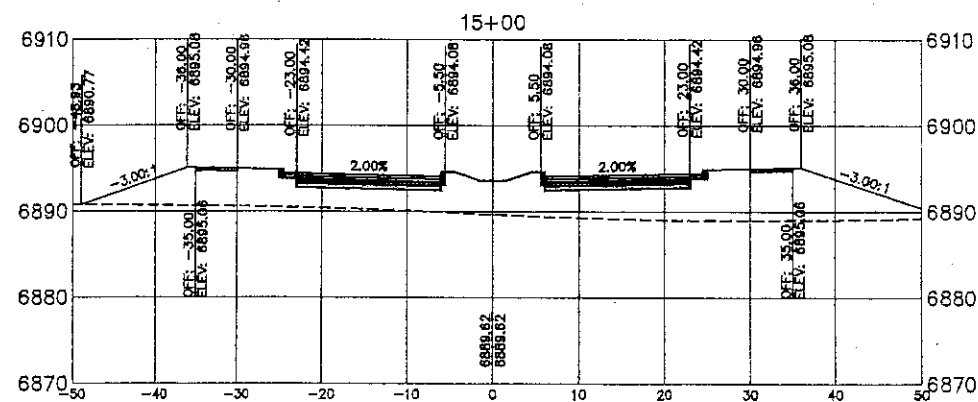


0 5 10 20
SCALE: 1" = 10'

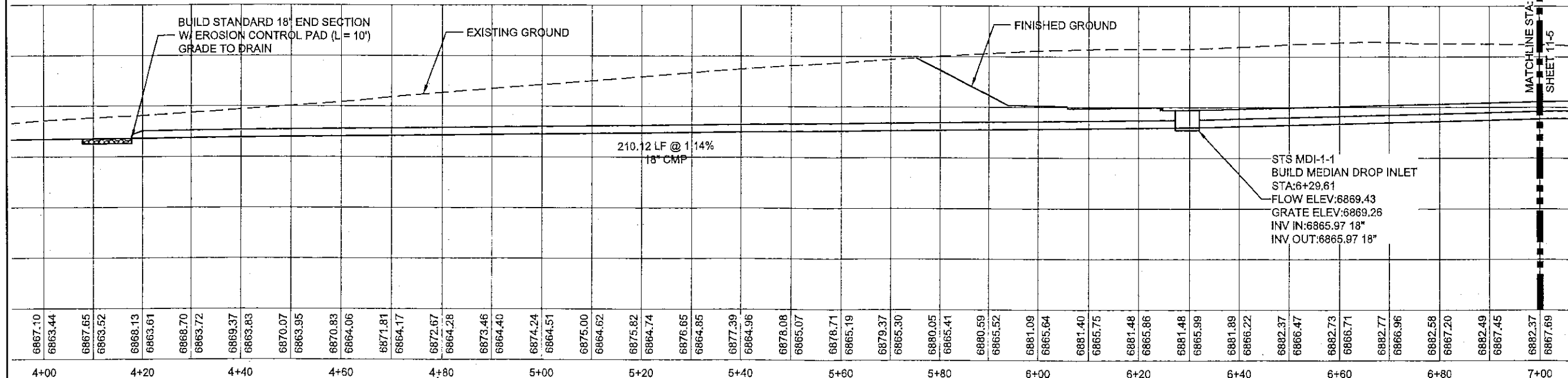
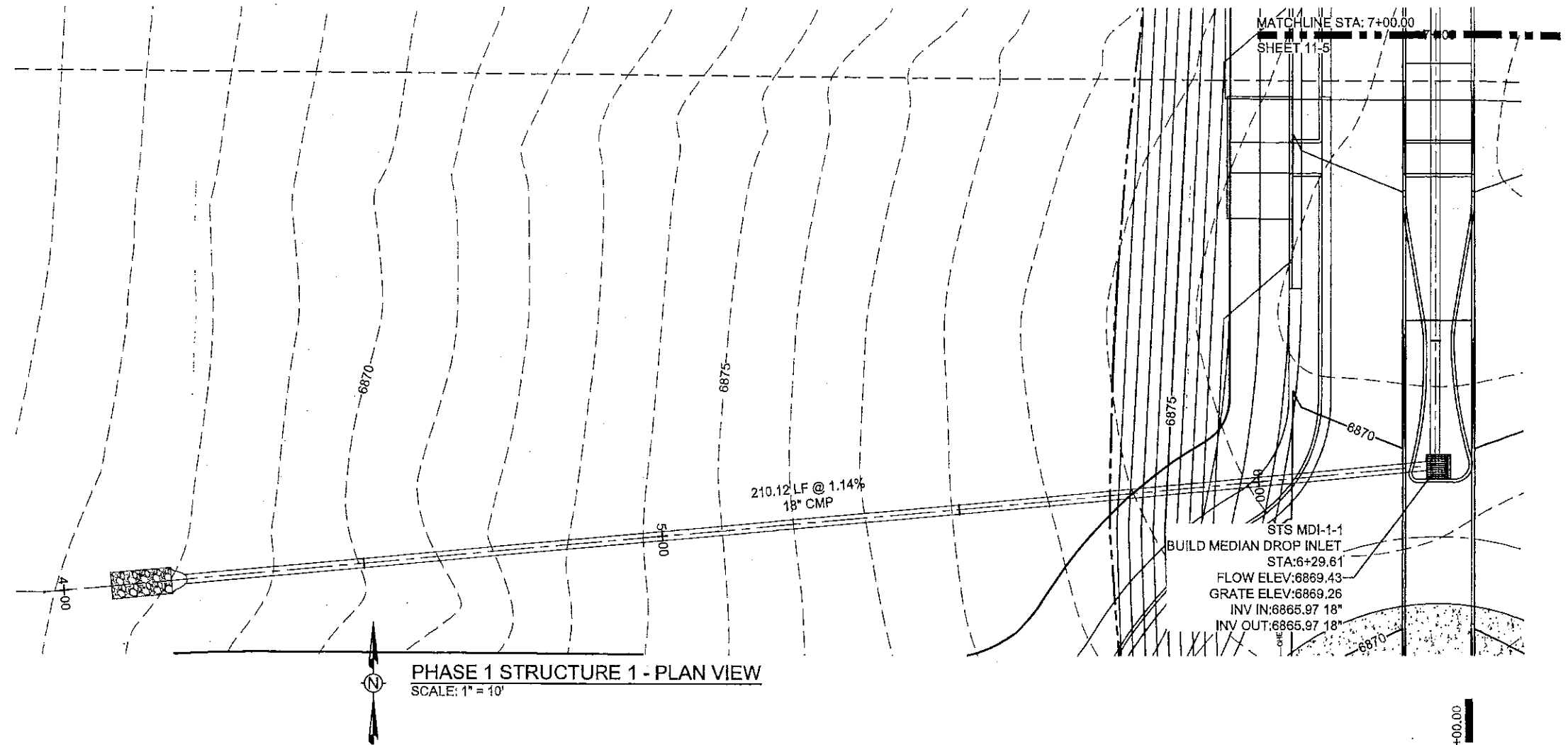
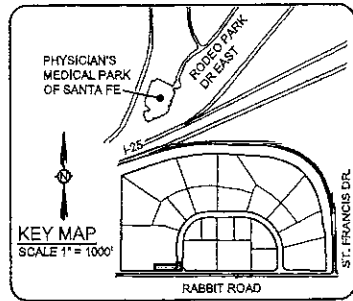
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FINAL SUBDIVISION PLAT SUBMITTAL FOR: ST. FRANCIS SOUTH PHASES 1 & 2		
ROADWAY CROSS SECTIONS (CONT'D)		
DATE: FEBRUARY 2017	SCALE: 1" = 10'	SHEET: 11-2

ENGINEER'S SEAL	
DATE	
REVISIONS	



PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4 FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2		
ROADWAY CROSS SECTIONS (CONT'D)		
DATE: FEBRUARY 2017	SCALE: 1" = 10'	SHEET: 11-3



REVISIONS	DATE	ENGINEER'S SEAL

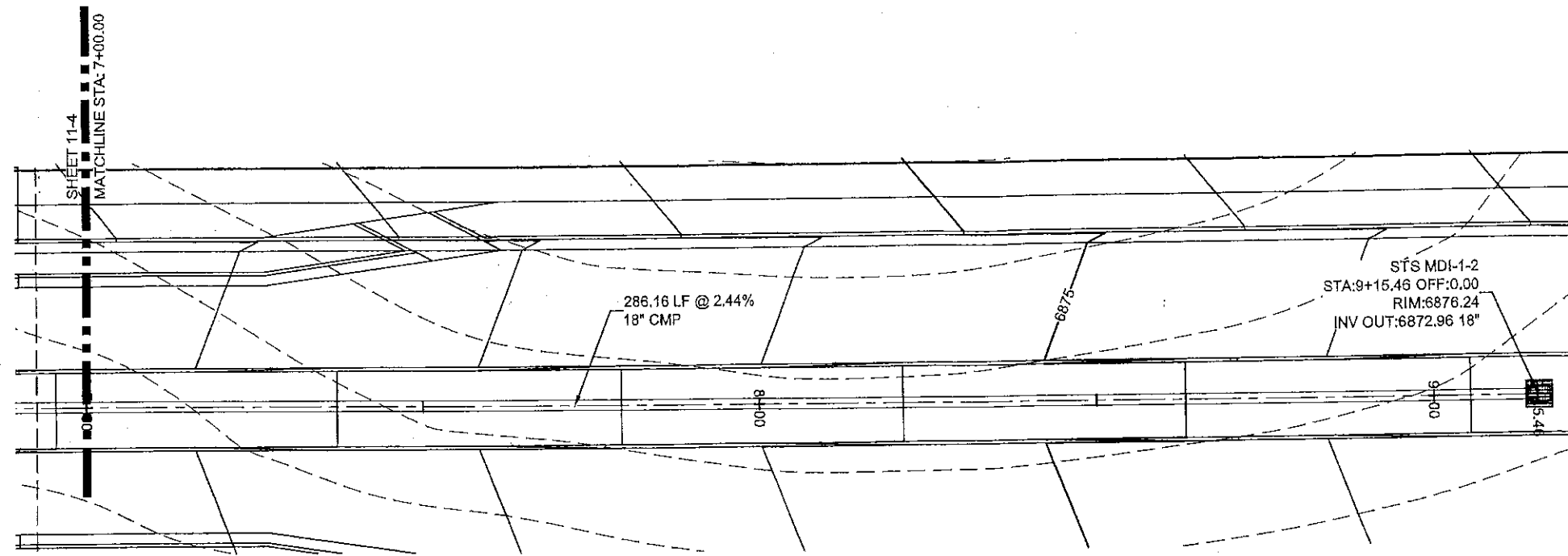
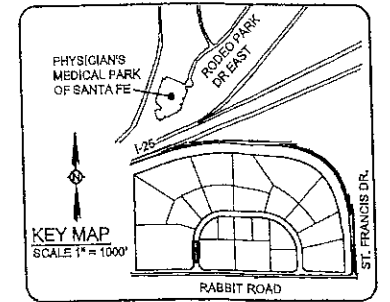
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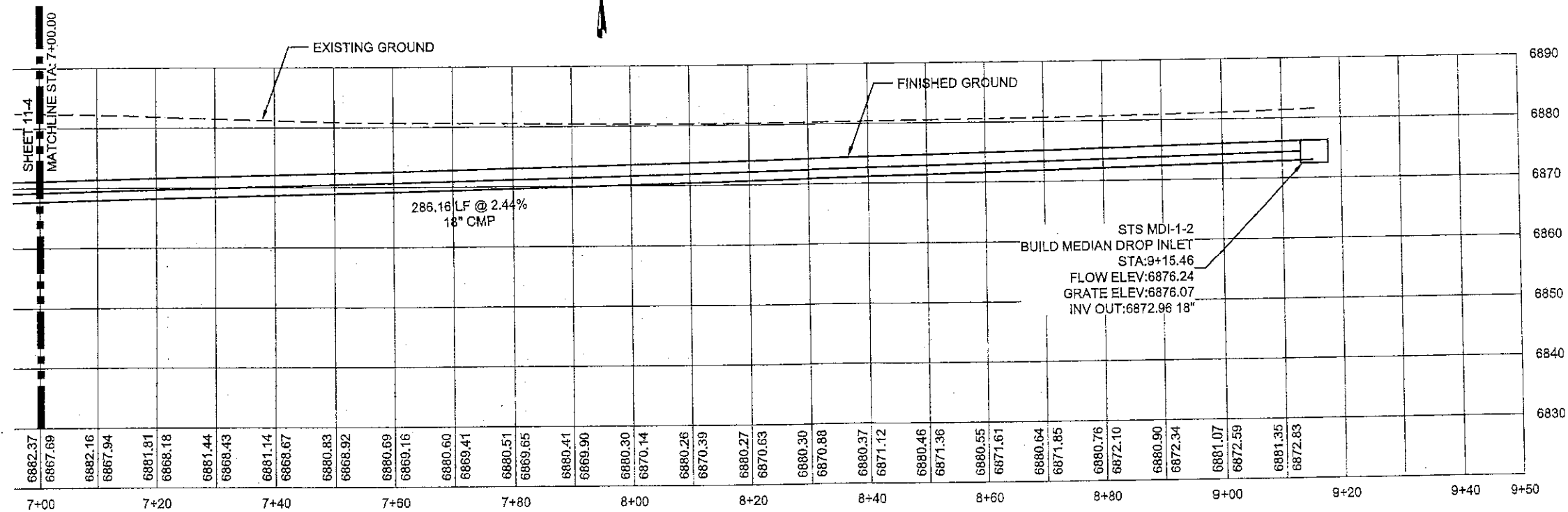
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ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

STRUCTURE SECTIONS

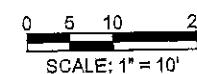
DATE: FEBRUARY 2017
SCALE: 1" = 10'
SHEET: 11-4



PHASE 1 STRUCTURE 1 - PLAN VIEW
SCALE: 1" = 10'



PHASE 1 STRUCTURE 1 - PROFILE VIEW
SCALE: HORIZ: 1" = 10'
VERT: 1" = 10'



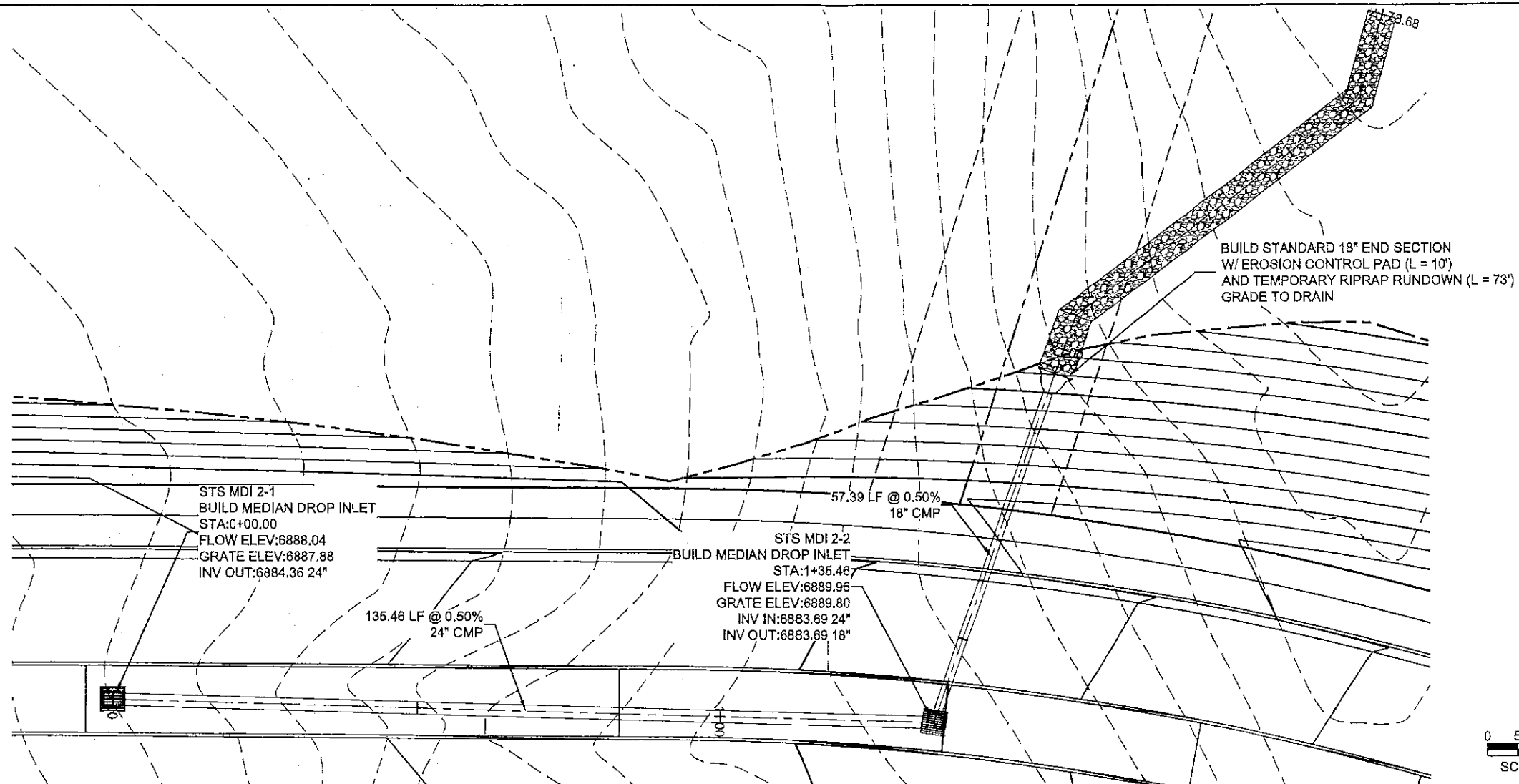
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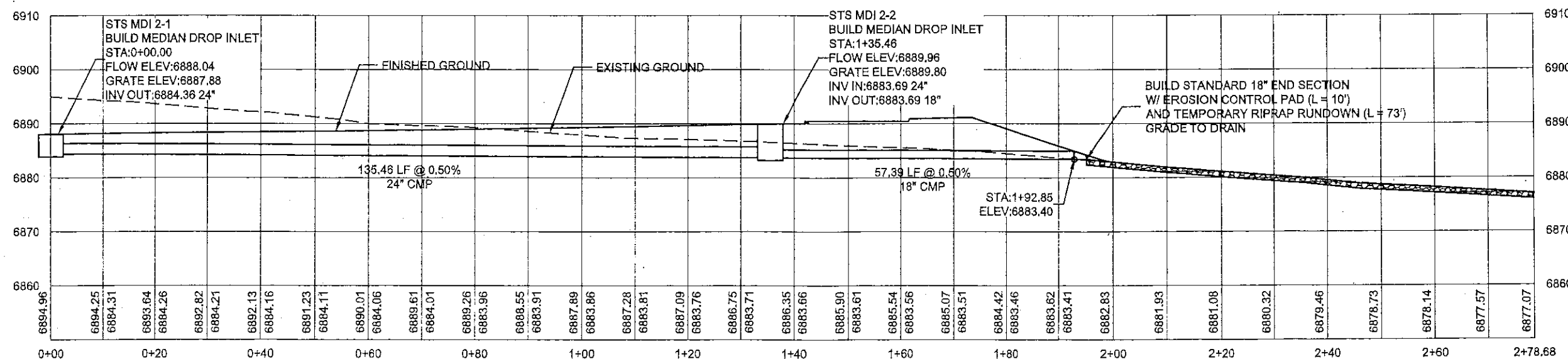
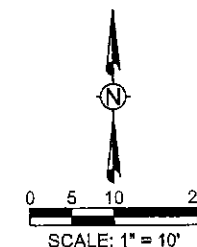
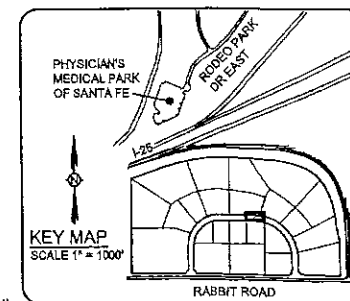
STRUCTURE SECTIONS

DATE: FEBRUARY 2017 SCALE: 1" = 10' SHEET: 11-5

REVISIONS	DATE	ENGINEER'S SEAL



PHASE 2 STRUCTURE 1 - PLAN VIEW
SCALE: 1" = 10'



PHASE 2 STRUCTURE 1 - PROFILE VIEW
SCALE: HORIZ: 1" = 10'
VERT: 1" = 10'

REVISIONS	DATE	ENGINEER'S SEAL

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PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
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FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

STRUCTURE SECTIONS

DATE: FEBRUARY 2017

SCALE: 1" = 10'

SHEET: 11-6

GABION NOTES

ASSEMBLING AND PLACING

- Each gabion shall be assembled by tying all untied edges with binding wire. The binding wire shall be tightly looped around every other mesh opening along the seams, in such a manner that single and double loops are alternated.
- A line of empty gabions shall be placed into position according to the contract drawings and binding wire shall be used to securely tie each unit to the adjoining one along the vertical reinforced edges and the top selvages. The base of the empty gabions placed on top of a filled line of gabions shall be tightly wired to the latter at front and back.
- To achieve better alignment and finish in retaining walls, gabion stretching is recommended.
- Connecting wires shall be inserted during the filling operation in the following manner:

ASSEMBLING AND PLACING (CONT'D)

36" Gabions

- Gabions shall be filled to a depth of twelve (12) inches.
- One connecting wire in each direction shall be tightly tied to opposite faces of each gabion cell at a height of twelve (12) inches above the base.
- Gabions shall be filled with a further depth of twelve (12) inches, and two connecting wires shall similarly be tied at this level.
- Gabions shall be filled to the top.

All connecting wires shall be looped around two-mesh openings and the ends of the wire shall be securely twisted to prevent their loosening.

18" and 12" Gabions

Connecting wires are not necessary unless the eighteen (18) inch size is used to build vertical structures, in this case, two wires, one in each direction, at nine (9) inches from the base, must be placed as above.

- The gabions in any row shall be filled in stages so that local deformations may be avoided. That is, at no time shall any gabion be filled to a depth exceeding one foot more than the adjoining gabion.
- When a gabion has been filled the lid shall be bent over by hand until it meets the front and ends. Then the lid shall be tightly bound to the rest of the basket with the lacing wire along all edges and internal cell diaphragms in the same manner described above for assembly.

MATERIALS

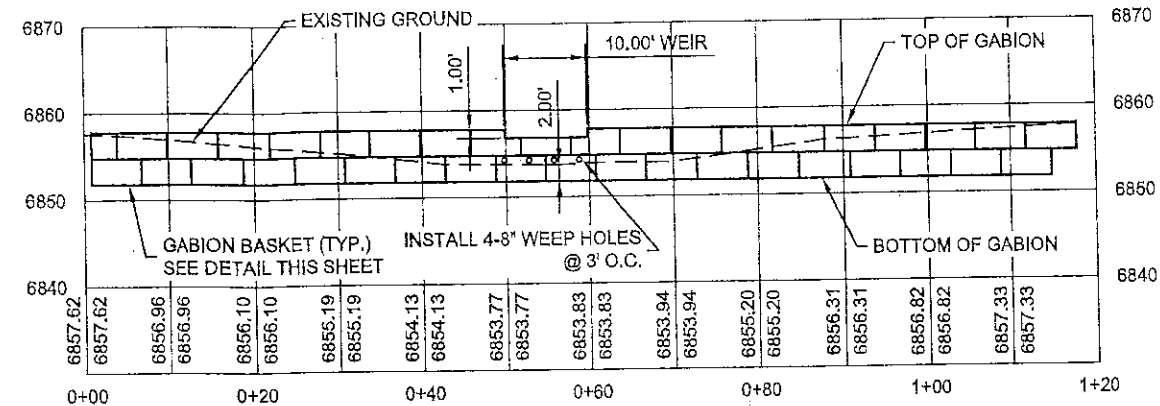
Galvanized Steel Wire Mesh Gabions. Gabion basket units shall be of non-raveling construction and fabricated from a double twisted hexagonal mesh of hot dipped galvanized steel wire having a diameter of 0.118 inches (approximately US gauge 11) after galvanization. The steel wire used shall be galvanized prior to weaving into mesh. All gabion diaphragm and frame wire shall equal or exceed Federal Specification 00-W-461H, possess soft tensile strength, and a finish 5 Class 8 zinc coating of not less than 0.80 oz/sq. ft. of uncoated wire surface. The weight of zinc coating shall be as determined by ASTM test designation on A-90. Mesh openings shall be hexagonal in shape and uniform in size measuring not more than 3 1/4 inches by 4 1/2 inches approximately 1-9 sq. in. area opening. Selvage or perimeter basket frame wire shall be of a heavier gauge than the mesh wire with a diameter of 0.150 inches (approximately US gauge 9) after galvanization. Lacing and connecting wire shall meet the same specifications as the wire used in the gabion body except that its diameter shall be of 0.0866 inches (approximately US gauge 13 1/2) after galvanization. All of the above wire diameters are subject to tolerance limit of 0.004 inches in accordance with ASTM A-641.

GABION FILL

The material used for gabion fill shall be clean, hard stone with pieces ranging from four (4) to eight (8) inches in greatest dimension.

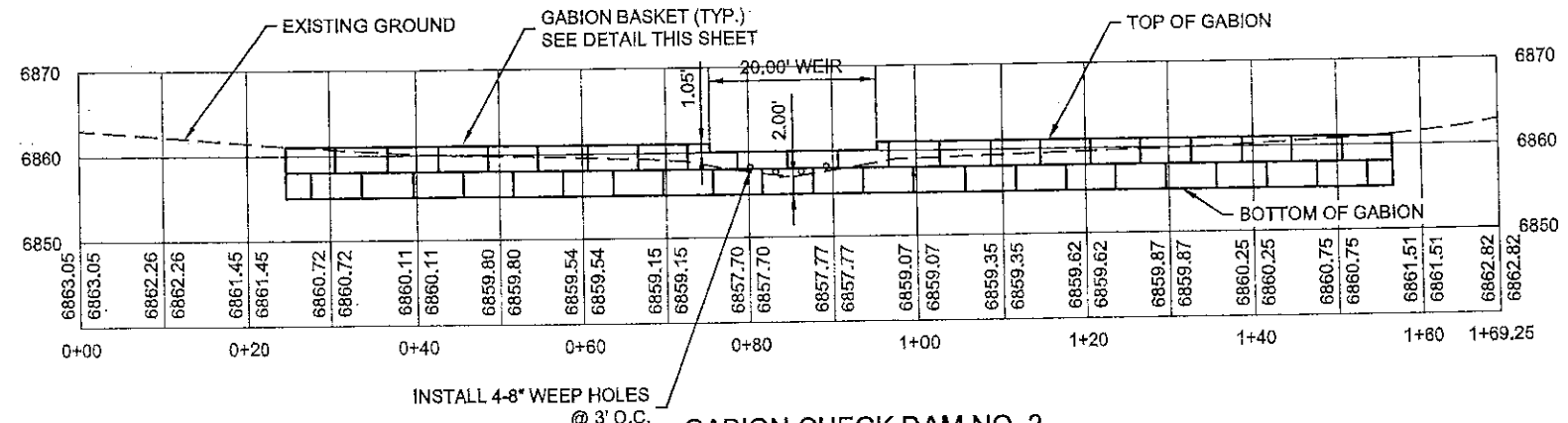
FILLING

Gabions may be filled by hand or by mechanical means. Every effort shall be made to keep voids and bulges, in the gabions to a minimum in order to ensure proper alignment and a neat, compact, square appearance.



GABION CHECK DAM NO. 1

SCALE HORIZ: 1" = 10'
VERT: 1" = 10'



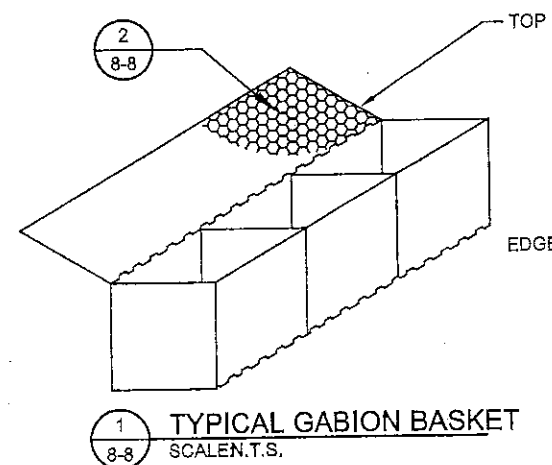
GABION CHECK DAM NO. 2

SCALE HORIZ: 1" = 10'
VERT: 1" = 10'

GABION STANDARD SIZES			
LENGTH CODE	DIMENSIONS	NUMBER OF CELLS	CAPACITY (CUBIC YARDS)
A	6'x3'x3'	2	2
B	9'x3'x3'	3	3
C	12'x3'x3'	4	4
D	6'x3'x1'-6"	2	1
E	9'x3'x1'-6"	3	1.500
F	12'x3'x1'-6"	4	2
G	6'x3'x1'	2	0.666
H	9'x3'x1'	3	1
I	12'x3'x1'	4	1.333

TOLERANCES

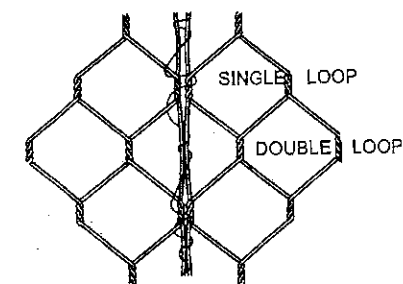
ALL GABION DIMENSIONS SHALL BE WITHIN A TOLERANCE LIMIT OF + 5% OF THE MANUFACTURER STATED SIZES.



TYPICAL GABION BASKET

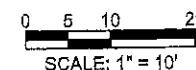
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.0866 DIA. LACING WIRE
(NOTE: CONTRACTOR MAY USE APPROVED SNAP RING CONNECTORS IN LIEU OF WIRE TIE)



LACING DETAIL

SCALE: T.S.



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PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 TO 4
FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

GABION CHECK DAMS

DATE: FEBRUARY 2017 SCALE: 1" = 10' SHEET: 11-7

STORM WATER POLLUTION PREVENTION PLAN
PRELIMINARY PLAT AND PRELIMINARY DEVELOPMENT
PLAN SUBMITTAL
ST. FRANCIS SOUTH
SANTA FE, NEW MEXICO

1. SITE AND PROJECT DESCRIPTION

Grading and drainage improvements, construction of
roadways, utilities and miscellaneous improvements.

1.1 Owner Operator:

VEGAS VERDES, LLC.
P.O. BOX 22865
SANTA FE, NM 87502

Construction Operator (Separate NPDES Permit needed),

Telephone: _____

Fax: _____

1.2 Location of the site:

The site is located in Township 16 North, Range 9 East,
Section 11 in Santa Fe, New Mexico.

The latitude and longitude are as follows:

Latitude: 35°37'47" N
Longitude: 105°57'40" W

1.3 The total area of the site:

68.93 ± acres

1.4 The area of the site that is expected to be disturbed:

5.47 ± acres

1.5 Endangered Species:

Based on the instruction provided by the EPA Compliance
Assurance and Enforcement Division Water Enforcement
Branch, Region 6 "Storm Water Region 6 NPDES General
Permit for Storm Water Discharge from Construction Activity,"
there are no endangered or threaten species, or designated
critical habitat, which are likely to be adversely affected by the
construction activity's storm water discharge or storm water
discharge related activities.

1.6 Intended sequence of major construction activities:

1. Install Best Management Practices (BMP's). The
contractor shall minimize soil disturbance and insure the
proper stockpiling of materials.

2. Site Grading. The contractors shall use site-specific
controls as shown on the
Temporary Erosion Control Plans and Grading and Drainage
Plans. Soil disturbance shall be minimized.

3. Stabilization. The contractors shall use site-specific
controls as shown on the
Permanent Erosion Control Plans.

1.7 The following maps have been prepared:

1.7.1 Vicinity Map

See Sheet 1-1.

1.7.2 Drainage patterns:

See Grading and Drainage Plan, Sheet 8-1.

1.7.3 Approximate slopes after major grading.

See Permanent Erosion Control, Sheet 12-3.

1.7.4 Outline area of disturbance:

See Permanent Erosion Control, Sheet 12-3.

1.7.5 Outline of areas which won't be disturbed:

See Permanent Erosion Control, Sheet 12-3.

1.7.6 Location of major structural and non-structural concepts:

See Temporary Erosion Control Plan, Sheet 12-2.

1.7.7 Areas where stabilization practices are expected to
occur.

See Permanent Erosion Control Plan, Sheet 12-3.

2. DESCRIPTION OF CONTROLS

2.1 Posting Requirements:

The Contractor will post conspicuously near the entrance to
the site the following items:

Copies of NOI's
Name and telephone number of contact persons.

2.2 "Good Housekeeping":

The contractors shall practice "Good Housekeeping." This
includes the proper disposal of construction and demolition
debris on a daily basis, proper wash down methods of
construction vehicles, the proper stockpiling of materials. The
construction site shall be kept in a neat and orderly manner
and exposure of material to storm water will be minimized to
the extent practicable.

2.3 Concrete Washout

Concrete washout is a process wastewater and must be
controlled in a designated area (e.g. bermed pit) and disposed
of properly and noted on the Temporary Erosion Control
Plans.

2.4 Minimize Disturbance and Preserve Natural Vegetation:

The Contractor shall minimize disturbance. The limits of
construction shall be clearly delineated and enforced. Special
attention will be given to protecting established vegetation.
The contractor will be responsible for replacing vegetation
that is unnecessarily disturbed.

2.5 Inspections:

Disturbed areas, stabilization and structural control measures
shall be inspected as required and at least once every two
weeks, and within 24 hours or the next working day of the end
of a storm event. If a portion of the site has been finally or
temporarily stabilized, and runoff is unlikely due to winter
conditions, or during seasonal arid periods, inspections shall
be conducted on a monthly basis.

Inspections shall be documented on the inspection and
maintenance form. Inspections shall be continued by the
Owner and final stabilization of an area is achieved and/or the
Notice of Termination is submitted.

2.6 Record Keeping:

A copy of the SWPP Plan will be maintained onsite for the use
of all Operators and those identified in the SWPP Plan as
having on site responsibilities. Items that will be maintained
and attached to the SWPP include:

Inspection Reports: The inspection reports shall be signed by
a qualified inspector assigned by the Contractor. The SWPP
package and reports shall be available to EPA representatives
at all times during construction.

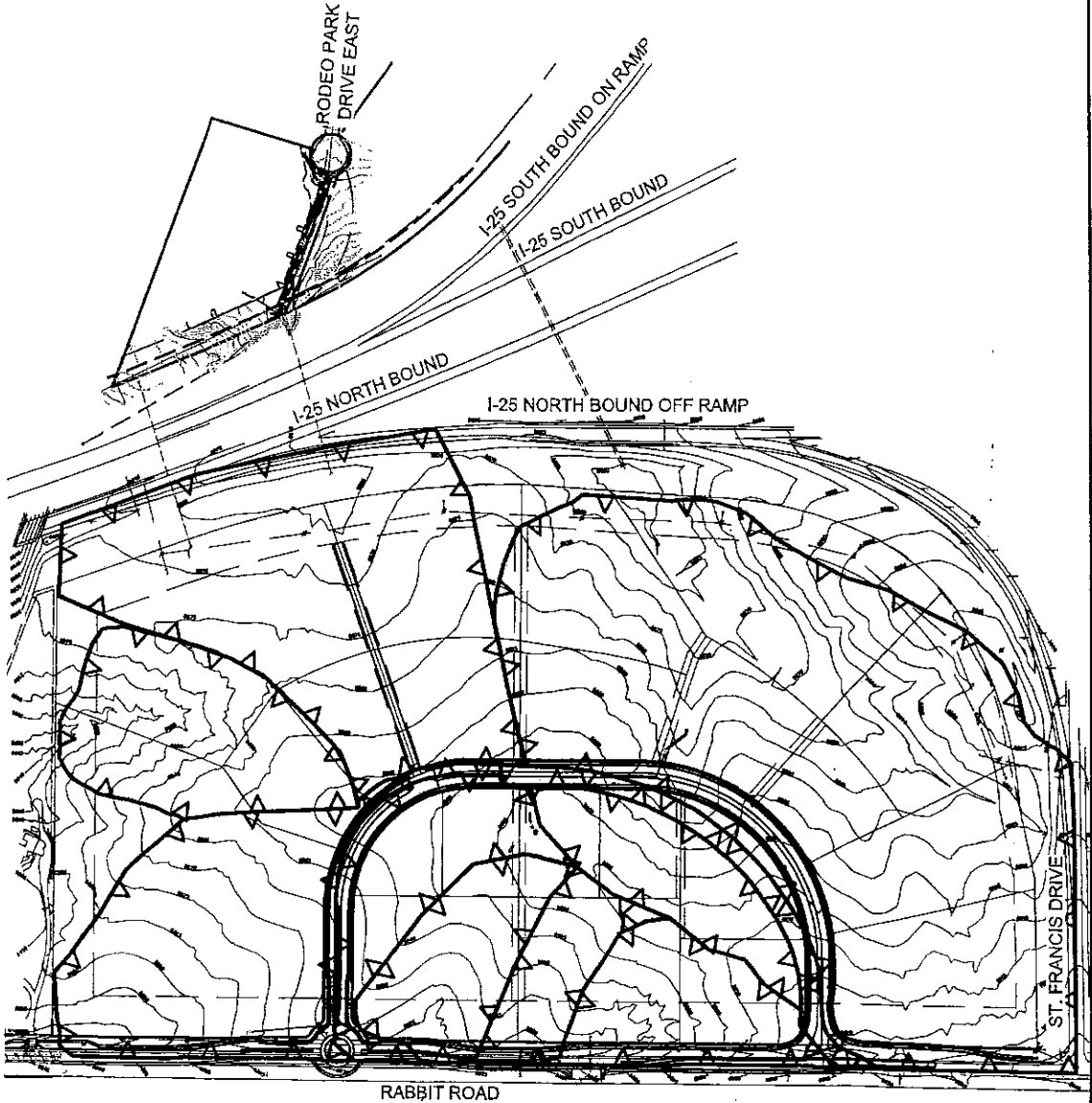
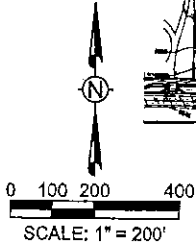
Land Disturbance Log: Containing dates when major soil
disturbing activities occur, dates when construction activities
temporarily or permanently cease on a portion of the site, and
dates when stabilization measures are initiated.

Spill Tracking: Copies of SWPP Plans, inspection records,
spill reports, all reports required by NPDES Permit coverage,
and data used to complete the NOI shall be retained by the
permittees.

2.7 Plan Amendment:

This SWPP Plan will be amended when: There is a change in
design, construction, operation, or maintenance.

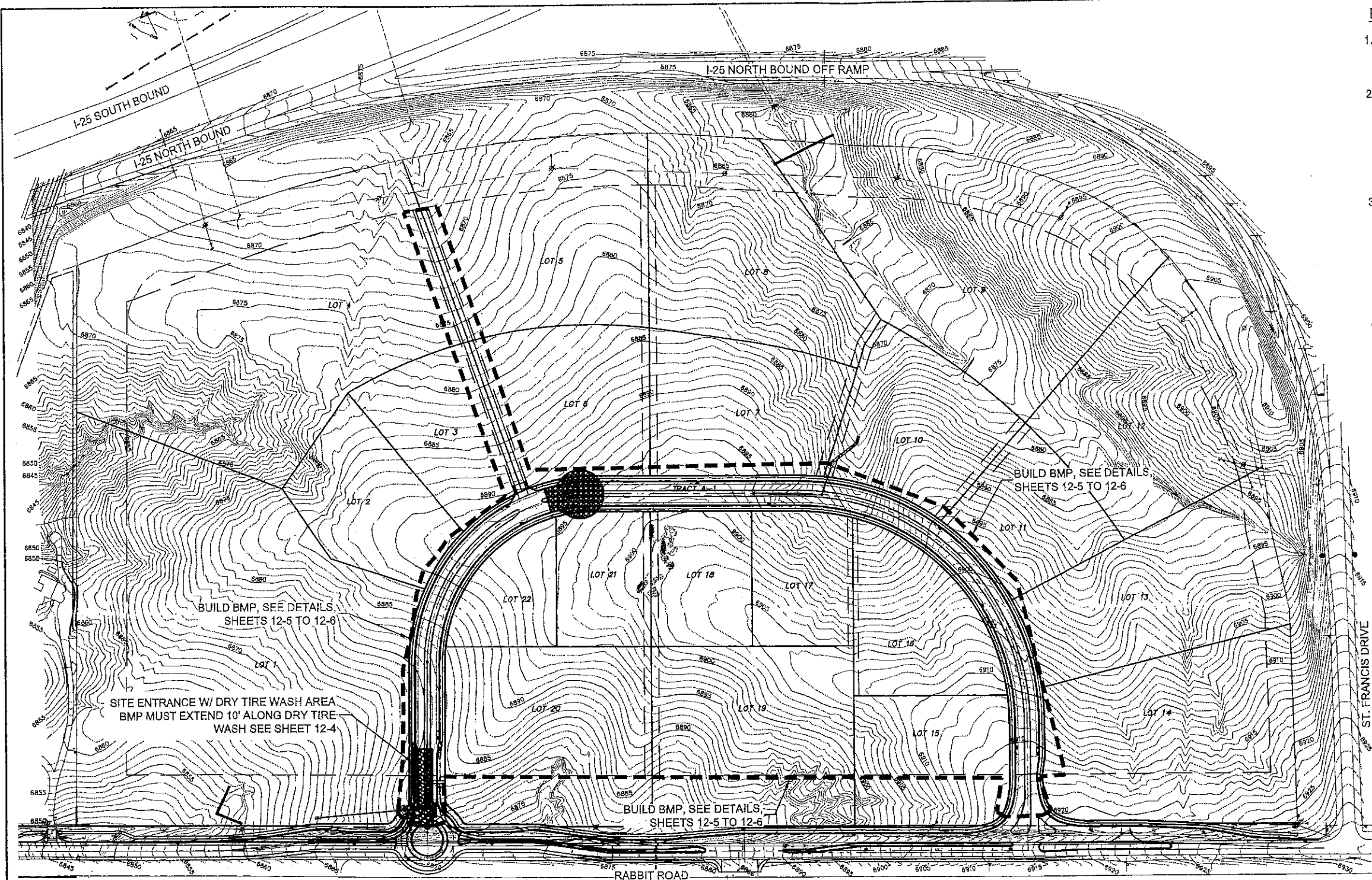
If the inspection report identifies problems or inadequacies
with the current BMPs, the SWPP Plan shall be modified as
necessary to include additional or modified BMP's designed to
correct the problems.



DEVELOPED CONDITIONS DRAINAGE MAP
SCALE: 1" = 200'

REVISIONS	DATE	ENGINEER'S SEAL

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	STORM WATER POLLUTION PREVENTION PLAN		
	DATE: FEBRUARY 2017	SCALE: 1" = 200'	SHEET: 12-1

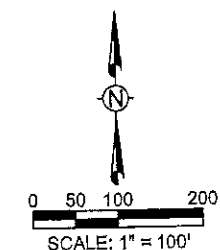


BEST MANAGEMENT PRACTICES - NOTES

1. DISTURBED AREAS SHALL BE PROTECTED FROM EROSION DURING CONSTRUCTION BY MEANS ADEQUATE TO RETAIN SOIL ON SITE.
2. EXCEPT AS NECESSARY TO INSTALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES, LAND SHALL NOT BE GRADED OR CLEARED UNTIL ALL SUCH TEMPORARY DEVICES HAVE BEEN PROPERLY INSTALLED AND INSPECTED. CONTROL DEVICES SHALL BE KEPT IN PLACE AND USED UNTIL THE DISTURBED AREA IS PERMANENTLY STABILIZED.
3. SIGNIFICANT TREES, AREAS WITH SUBSTANTIAL GRASS COVERAGE, AND DRAINAGE WAYS THAT ARE TO REMAIN UNDISTURBED SHALL BE FENCED OFF PRIOR TO THE USE OF ANY HEAVY MACHINERY ON-SITE AND DURING THE ENTIRE CONSTRUCTION PROCESS. FENCING SHALL BE PLACED FIVE FEET TO THE OUTSIDE OF THE DRIP LINE OF SIGNIFICANT TREES. THERE ARE NO SIGNIFICANT TREES.
4. SOIL STOCKPILES SHALL BE PROTECTED FROM EROSION THROUGHOUT CONSTRUCTION BY USING APPROPRIATE EROSION CONTROL TECHNIQUES. STAGING AND SOIL STOCKPILE AREAS SHALL BE CLEARLY DESIGNATED ON THE SITE. ALL TOPSOIL SHALL BE KEPT ON SITE, WITHIN THE DISTURBANCE ZONE OF CONSTRUCTION, AND REINTRODUCED IN TO PLANTING AREAS TO THE EXTENT POSSIBLE. THERE ARE NO SOIL STOCKPILES ASSOCIATED WITH THIS PROJECT.
5. WATERING DOWN EXPOSED AREAS IS REQUIRED TO PREVENT THE BLOWING OF DUST OR SEDIMENT.
6. PROTECTION FOR STORM DRAIN INLETS SHALL BE PROVIDED TO PREVENT THE ENTRY OF SEDIMENT WHILE STILL ALLOWING THE ENTRY OF STORM WATER.
7. EROSION AND SEDIMENT CONTROL SHALL BE ACCOMPLISHED BY WATERING AND/OR FIBER MULCH WITH TACKIFIER

LEGEND

--- BUILD BMP, SEE DETAILS, SHEETS 12-5 TO 12-6



TEMPORARY EROSION CONTROL PLAN SCALE: 1" = 100'

MAINTENANCE RESPONSIBILITIES AND INSPECTIONS

1. DISTURBED AREAS, STABILIZATION AND STRUCTURAL CONTROL MEASURES SHALL BE INSPECTED AS REQUIRED AND AT LEAST ONCE EVERY TWO WEEKS, AND WITHIN 24 HOURS OR THE NEXT WORKING DAY OF THE END OF A STORM EVENT. IF A PORTION OF THE SITE HAS BEEN FINALLY OR TEMPORARILY STABILIZED, AND RUNOFF IS UNLIKELY DUE TO WINTER CONDITIONS, OR DURING SEASONAL ARID PERIODS, INSPECTIONS SHALL BE CONDUCTED ON A MONTHLY BASIS.
2. ALL STORMWATER FACILITIES SHALL BE MAINTAINED BY THE CONTRACTOR.
3. THE STORMWATER MANAGEMENT SYSTEM SHALL BE MAINTAINED IN GOOD CONDITION AND PROMPTLY REPAIRED.
4. THE COUNTY OR ITS AUTHORIZED AGENT MAY ENTER UPON THE PROPERTY, AT REASONABLE TIME TO ENSURE THAT THE SYSTEM IS MAINTAINED IN PROPER WORKING CONDITION.
5. IF AFTER NOTICE BY THE COUNTY TO CORRECT A VIOLATION REQUIRING MAINTENANCE WORK, SATISFACTORY CORRECTIONS ARE NOT MADE WITHIN A REASONABLE PERIOD OF TIME, THE COUNTY MAY PERFORM ALL NECESSARY WORK TO PLACE THE FACILITY IN PROPER WORKING CONDITION. THE OWNER(S) OR RESPONSIBLE PARTY OF THE FACILITY SHALL BE ASSESSED THE ASSOCIATED COSTS OF THE WORK.

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Consultants, LLC

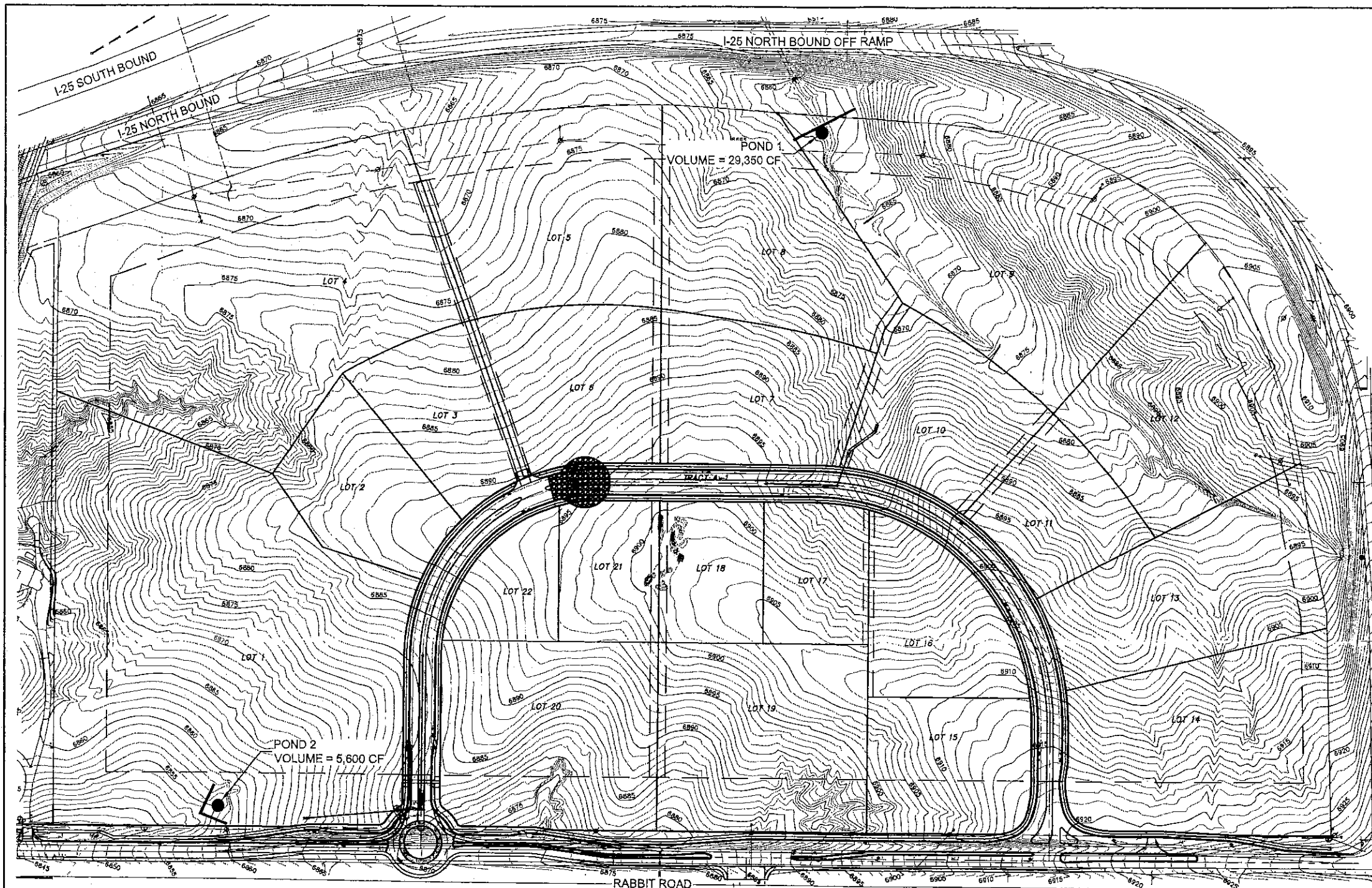
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FINAL SUBDIVISION PLAT SUBMITTAL FOR
ST. FRANCIS SOUTH PHASES 1 & 2

TEMPORARY EROSION CONTROL PLAN

DATE: FEBRUARY 2017	SCALE: 1" = 100'	SHEET: 12-2
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ENGINEER'S SEAL	
DATE	
REVISIONS	



LONG TERM MAINTENANCE RESPONSIBILITIES AND INSPECTIONS

- ALL STORM WATER FACILITIES SHALL BE MAINTAINED BY
- FOR DEVELOPMENTS OF FIFTEEN OR MORE DWELLING UNITS, THE DEVELOPER SHALL PROVIDE LIABILITY AND PROPERTY DAMAGE INSURANCE, IN A FORM APPROVED BY THE COUNTY ATTORNEY, IN ORDER TO PROTECT ADJACENT PROPERTY OWNERS FROM FAILURE OF DRAINAGE OR EROSION CONTROL STRUCTURES WHICH WERE REQUIRED FOR THE DEVELOPMENT.
- FOR NEW DEVELOPMENTS OF FIFTEEN OR MORE DWELLING UNITS, PERFORMANCE BONDS OR EQUIVALENT SHALL BE POSTED BY THE DEVELOPER FOR 10 YEARS AND THEREAFTER SHALL BE RENEWED BY THE NEIGHBORHOOD OR RESPONSIBLE ASSOCIATION FOR MAINTAINING ALL COMMON DRAINAGE STRUCTURES. FAILURE TO RENEW THE BOND IN A TIMELY MANNER SHALL BE GROUNDS FOR THE COUNTY ATTORNEY'S OFFICE TO CALL THE BOND. THE BOND SHALL BE OF AN AMOUNT SUFFICIENT TO DEFRAY MAINTENANCE COSTS FOR 10 YEARS.
- THE STORM WATER MANAGEMENT SYSTEM SHALL BE MAINTAINED IN GOOD CONDITION AND PROMPTLY REPAIRED.
- THE COUNTY OR ITS AUTHORIZED AGENT MAY ENTER UPON THE PROPERTY, AT REASONABLE TIMES TO ENSURE THAT THE SYSTEM IS MAINTAINED IN PROPER WORKING CONDITION.
- IF AFTER NOTICE BY THE COUNTY TO CORRECT A VIOLATION REQUIRING MAINTENANCE WORK, SATISFACTORY CORRECTIONS ARE NOT MADE WITHIN A REASONABLE PERIOD OF TIME, THE COUNTY MAY PERFORM ALL NECESSARY WORK TO PLACE THE FACILITY IN PROPER WORKING CONDITION. THE OWNER(S) OR RESPONSIBLE PARTY OF THE FACILITY SHALL BE ASSESSED THE ASSOCIATED COSTS OF THE WORK.

ST. FRANCIS DRIVE

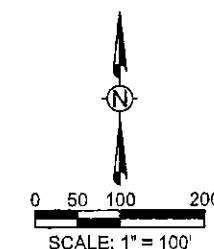
STORMWATER FACILITIES MAINTENANCE SCHEDULE

SPRING MAINTENANCE:	MARCH 21st	CLEARING AND MAINTENANCE FOR ALL STORMWATER MANAGEMENT FACILITIES SHALL OCCUR WITHIN 30 DAYS OF DATE. REMOVE SEDIMENT BUILD UP IN IN ALL PONDS AND RESEED.
FALL MAINTENANCE:	SEPTEMBER 18th	CLEARING AND MAINTENANCE FOR ALL STORMWATER MANAGEMENT FACILITIES SHALL OCCUR WITHIN 30 DAYS OF DATE.
PERIODIC MAINTENANCE:	AFTER SIGNIFICANT STORMS	CLEARING AND MAINTENANCE FOR ALL STORMWATER MANAGEMENT FACILITIES SHALL OCCUR WITHIN 30 DAYS OF STORM.

SFCC 13-2 PROHIBITS THE DISCHARGE OF POLLUTANTS INCLUDING SEDIMENT, VEGETATION, SLURRIES, MUD, PLASTERS, CONCRETE RESINATES AND ANY CONSTRUCTION MATERIALS, WASTES AND GARBAGE, ETC. TO THE STORM DRAIN SYSTEM. THE STORM DRAIN SYSTEM INCLUDES ROADS, STREETS, CURBS, GUTTERS, DROP INLETS, PIPED STORM DRAINS, CULVERTS, RETENTION AND DETENTION BASINS, NATURAL AND MAN-MADE DRAINAGE CHANNELS, ARROYOS, RIVERS, AND ANY FACILITY AND APPURTENANCE BY WHICH STORM WATER IS COLLECTED AND/OR CONVEYED. CULVERT AND DRAIN FLUSHING SHALL BE CAREFULLY DONE. POLLUTANTS AND DEBRIS SHALL BE CAREFULLY REMOVED AND DISPOSED OF AT THE LANDFILL.

INSPECTIONS AND VIOLATIONS DURING CONSTRUCTION PROCESS

- INSPECTIONS BY COUNTY ARE DUE WHEN STORM WATER MANAGEMENT MEASURES ARE COMPLETED AND WHEN THE FINAL SITE RESTORATION MEASURES ARE COMPLETED. HOWEVER, IF FINAL SITE RESTORATION MEASURES ARE BEING DELAYED DUE TO THE SEASON, THE COUNTY SHALL BE NOTIFIED WHEN TEMPORARY EROSION CONTROL MEASURES FOR USE UNTIL RESTORATION IS COMPLETE, IN PLACE AND READY FOR INSPECTION.
- FURTHER CONSTRUCTION OR ISSUANCE OF ANY PERMITS SHALL NOT OCCUR UNTIL WRITTEN APPROVAL HAS BEEN GRANTED BY THE COUNTY.



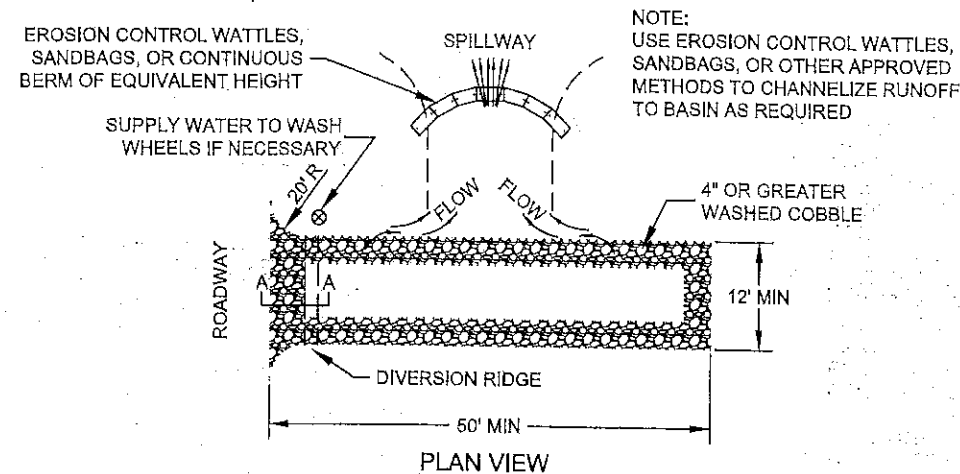
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PERMANENT EROSION
CONTROL PLAN

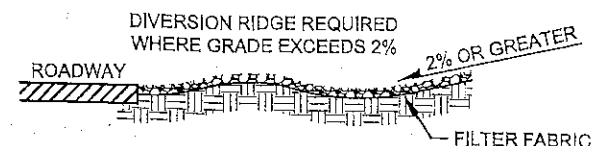
DATE: FEBRUARY 2017 SCALE: 1" = 100' SHEET: 12-3

ENGINEER'S SEAL	
DATE	
REVISIONS	



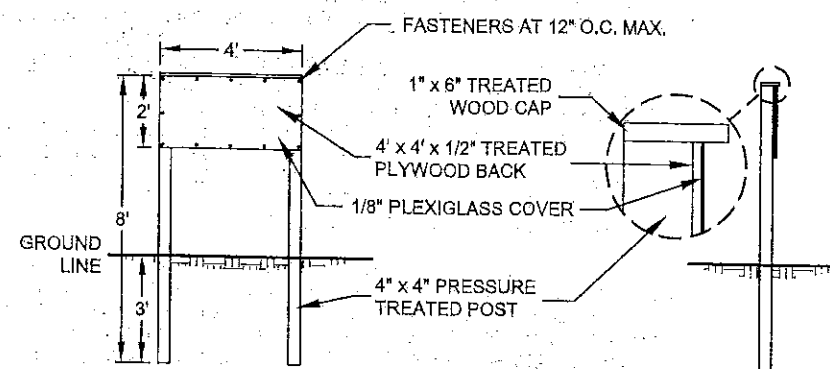
NOTES:

1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL MINIMIZE SEDIMENT TRACKING OR TRANSPORT ONTO PUBLIC ROADWAYS. THIS MAY REQUIRE ADDING ADDITIONAL LAYERS OF GRAVEL, REPAIR AND/OR CLEANOUT OF MEASURE USED TO TRAP SEDIMENT.
2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC ROADWAYS.
3. VEHICLE WASHING SHALL BE CONDUCTED IN A STABILIZED AREA WITH APPROPRIATE STORM WATER CONTROLS IN PLACE.



SECTION A-A

TEMPORARY GRAVEL CONSTRUCTION ENTRANCE / EXIT



N.P.D.E.S. PERMIT POSTING BOARD (LOCATED AT MAIN ENTRANCE)

COUNTY OF SANTA FE NPDES STORMWATER MANAGEMENT NOTES

1. DEVELOPMENT IS SUBJECT TO THE REQUIREMENTS OF THE SFCC CHAPTER XIII STORMWATER UTILITY INCLUDING THE STORMWATER UTILITY SERVICE CHARGE AND STORMWATER ILLICIT DISCHARGE CONTROL REQUIREMENTS.
2. DEVELOPMENT IS SUBJECT TO THE REQUIREMENTS OF NPDES GENERAL MS4 DISCHARGE PERMIT NO. NMR040000 AND NPDES CONSTRUCTION GENERAL PERMIT NO. NMR 100000.
3. CONSTRUCTION GENERAL DISCHARGE PERMIT NO. NMR100000- A STORMWATER POLLUTION PREVENTION PLAN MUST BE PREPARED BY A QUALIFIED PROFESSIONAL AND A NOTICE OF INTENT (NOI) FOR COVERAGE MUST BE FILED WITH THE EPA. CONSTRUCTION STORMWATER DISCHARGES **ARE NOT PERMITTED** UNTIL A MINIMUM OF SEVEN (7) CALENDAR DAYS AFTER EPA ACKNOWLEDGEMENT OF A COMPLETE NOI.
4. COUNTY OF SANTA FE STORMWATER ILLICIT DISCHARGE CONTROL- SFCC 13-2 PROHIBITS THE DISCHARGE OF POLLUTANTS INCLUDING SEDIMENT, SLURRIES, MUD, PLASTERS, CONCRETE RINSATES AND ANY CONSTRUCTION MATERIALS, WASTES AND GARBAGE, ETC. TO THE STORM DRAIN SYSTEM. THE STORM DRAIN SYSTEM INCLUDES ROADS, STREETS, CURBS, GUTTERS, DROP INLETS, PIPED STORM DRAINS, CULVERTS, RETENTION AND DETENTION BASINS, NATURAL AND MAN-MADE DRAINAGE CHANNELS, ARROYOS, RIVERS AND ANY FACILITY AND APPURTENANCE BY WHICH STORMWATER IS COLLECTED AND/OR CONVEYED.
5. COUNTY OF SANTA FE TERRAIN AND STORMWATER REGULATIONS- SFCC 14-8.2 REQUIRES THAT CONSTRUCTION DISTURBED AREA SHALL BE PROTECTED AGAINST EROSION. SEDIMENT MUST BE CONTAINED ON THE DISTURBED AREA BY THE USE OF TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES SUCH AS SILT FENCING, SWALES, BERMS, GEOTEXTILES, SEDIMENT BASINS AND TRAPS. PROTECTION FOR STORM DRAIN INLETS SHALL BE PROVIDED TO PREVENT THE ENTRY OF SEDIMENT FROM THE SITE WHILE STILL ALLOWING THE ENTRY OF STORMWATER. CONTROL DEVICES SHALL BE KEPT IN PLACE AND USED UNTIL THE DISTURBED AREA IS PERMANENTLY STABILIZED.
6. THE CONTRACTOR SHALL NOT REMOVE SILT FENCE AND MULCH SOCKS OR OTHER TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES UNTIL DISTURBED AREAS ARE STABILIZED. SOIL STABILIZATION AND EROSION CONTROL MEASURES SHALL BE COMPLETED WITHIN 21 CALENDAR DAYS AFTER COMPLETION OF CONSTRUCTION OR OTHER SOIL DISTURBANCE ACTIVITIES ON THE SITE. IF THE TIME OF YEAR IS NOT CONDUCIVE TO PLANTING, THEN PLANTING MAY BE DELAYED UNTIL THE NEXT APPROPRIATE PLANTING SEASON PROVIDED THAT ALL TEMPORARY EROSION CONTROL MEASURES ARE MAINTAINED UNTIL PERMANENT EROSION CONTROL MEASURES ARE IMPLEMENTED. TEMPORARY EROSION CONTROL MEASURES SHALL BE SELECTED, DESIGNED AND INSTALLED WITH AN APPROPRIATE SEED BASE TO PROVIDE EROSION CONTROL FOR AT LEAST THREE YEARS WITHOUT ACTIVE MAINTENANCE. TEMPORARY EROSION CONTROL MEASURES SHALL BE SELECTED, DESIGNED AND INSTALLED TO ACHIEVE 70 PERCENT VEGETATIVE COVER WITHIN THREE YEARS.

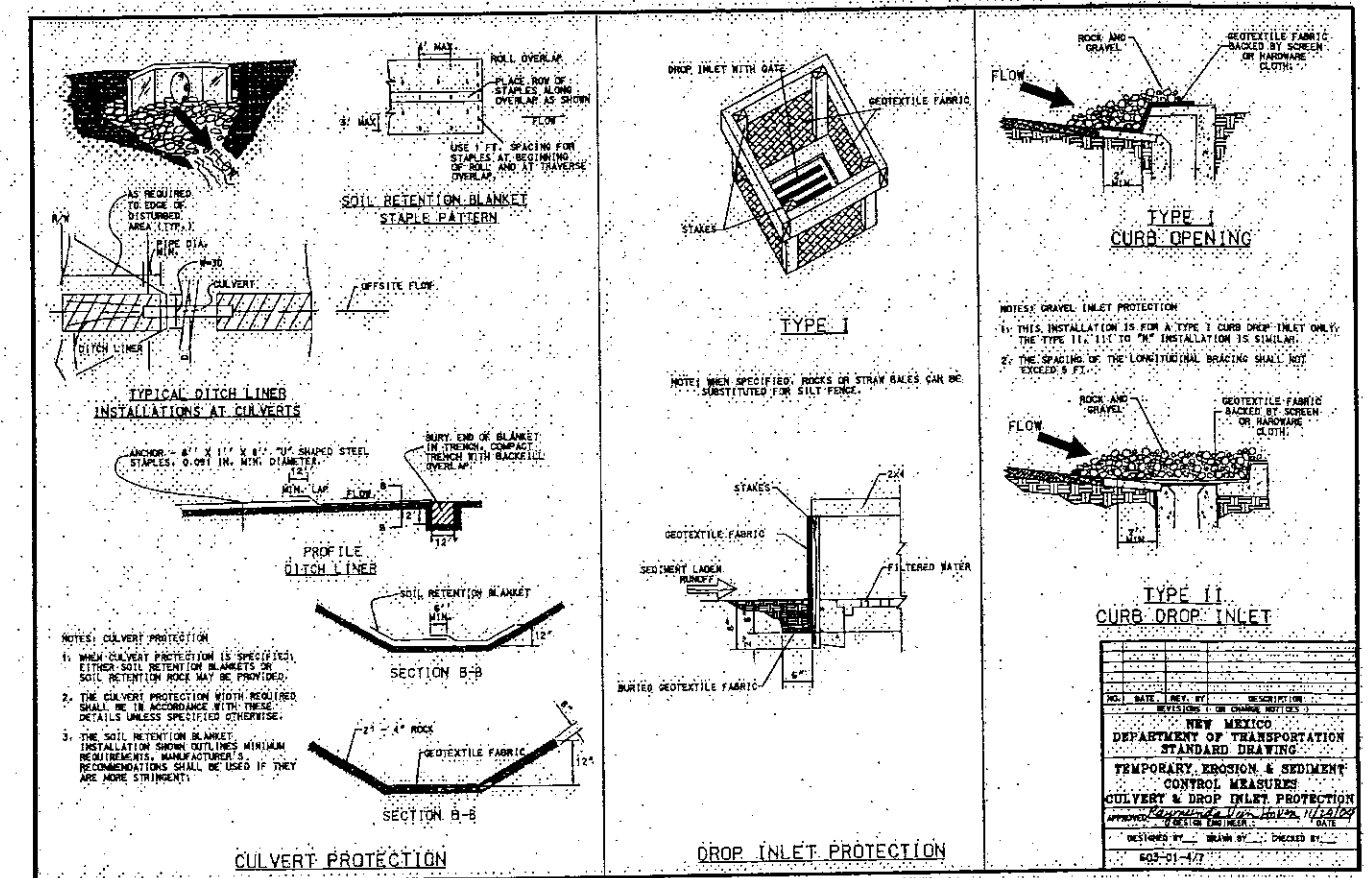
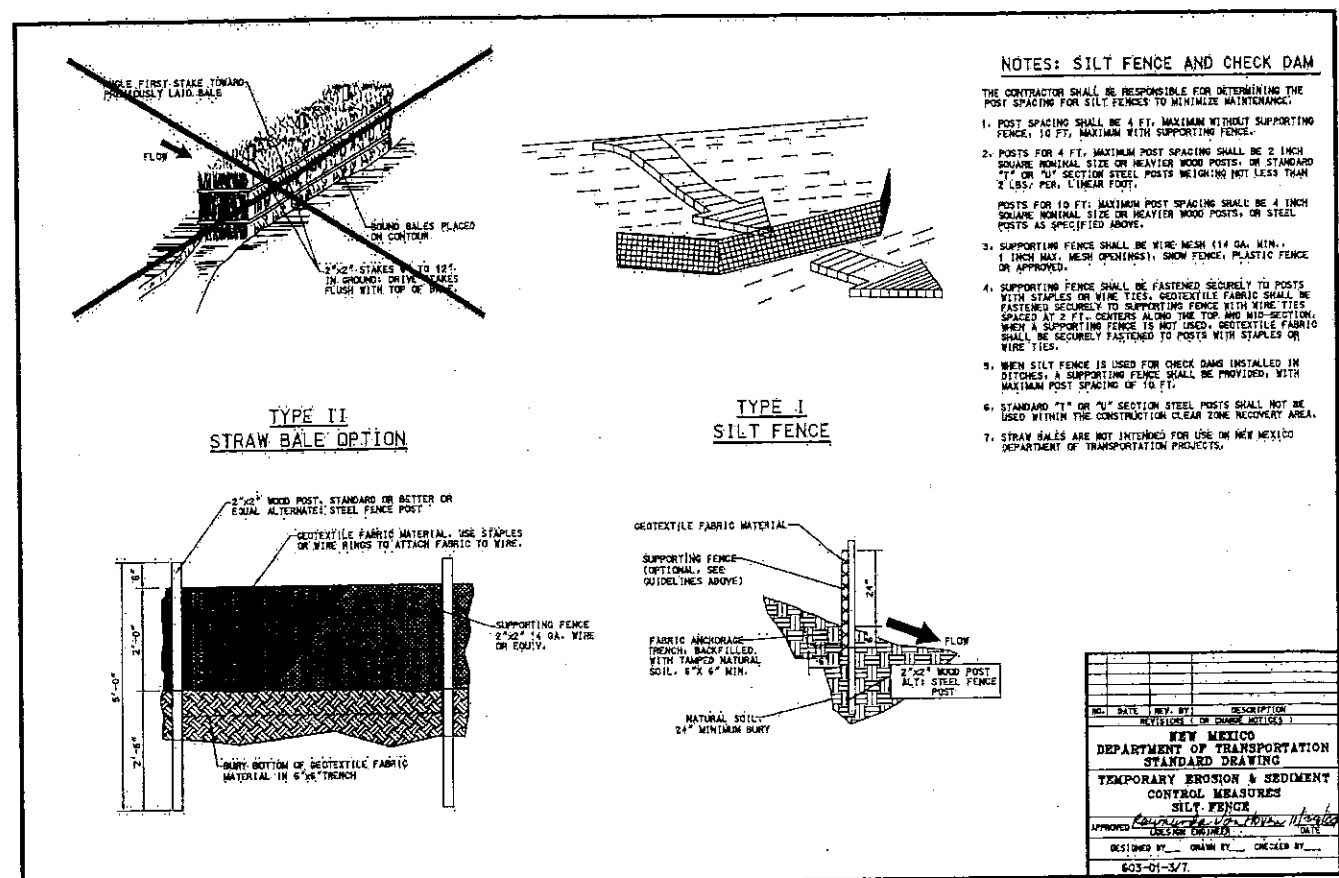
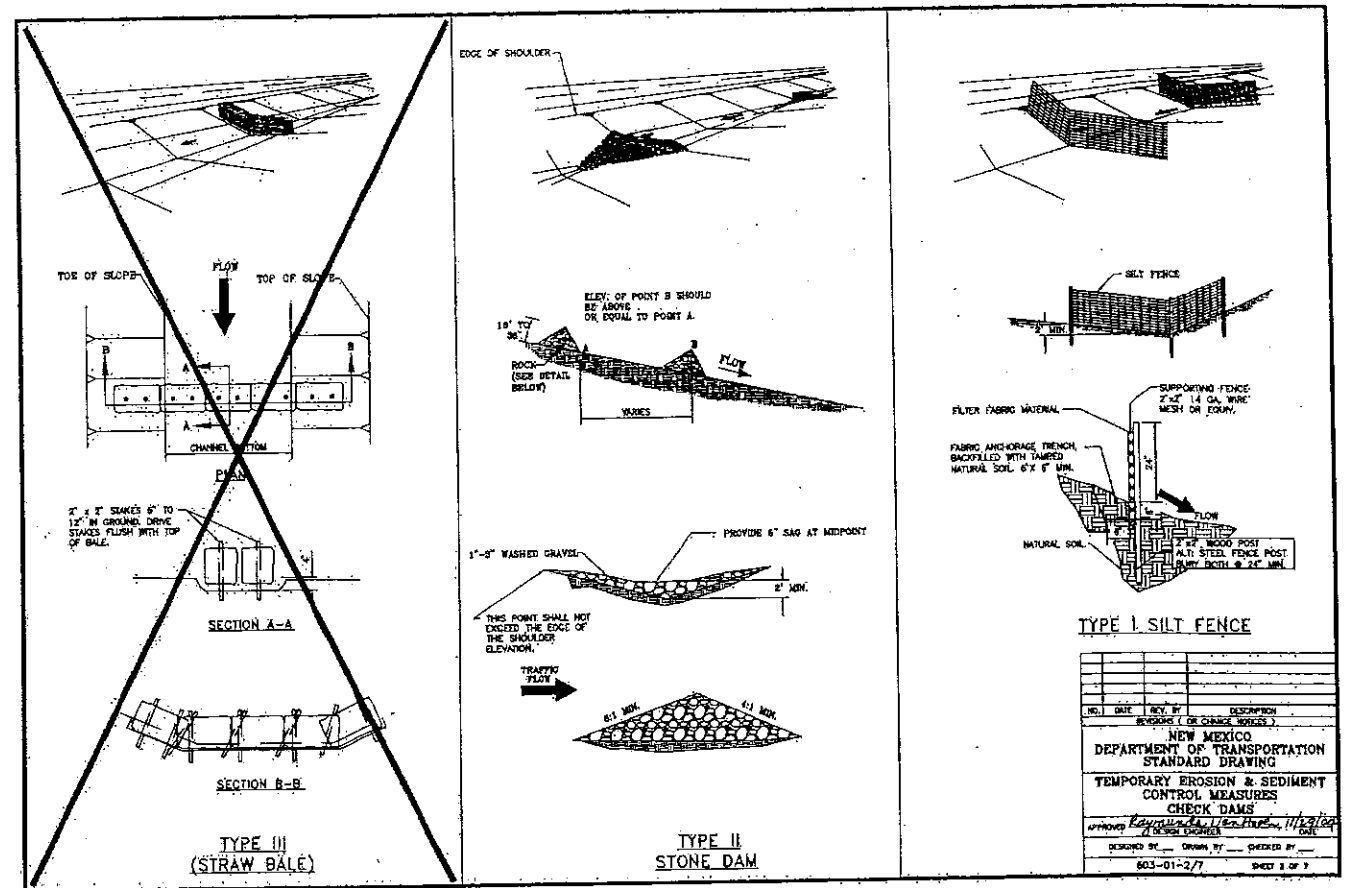
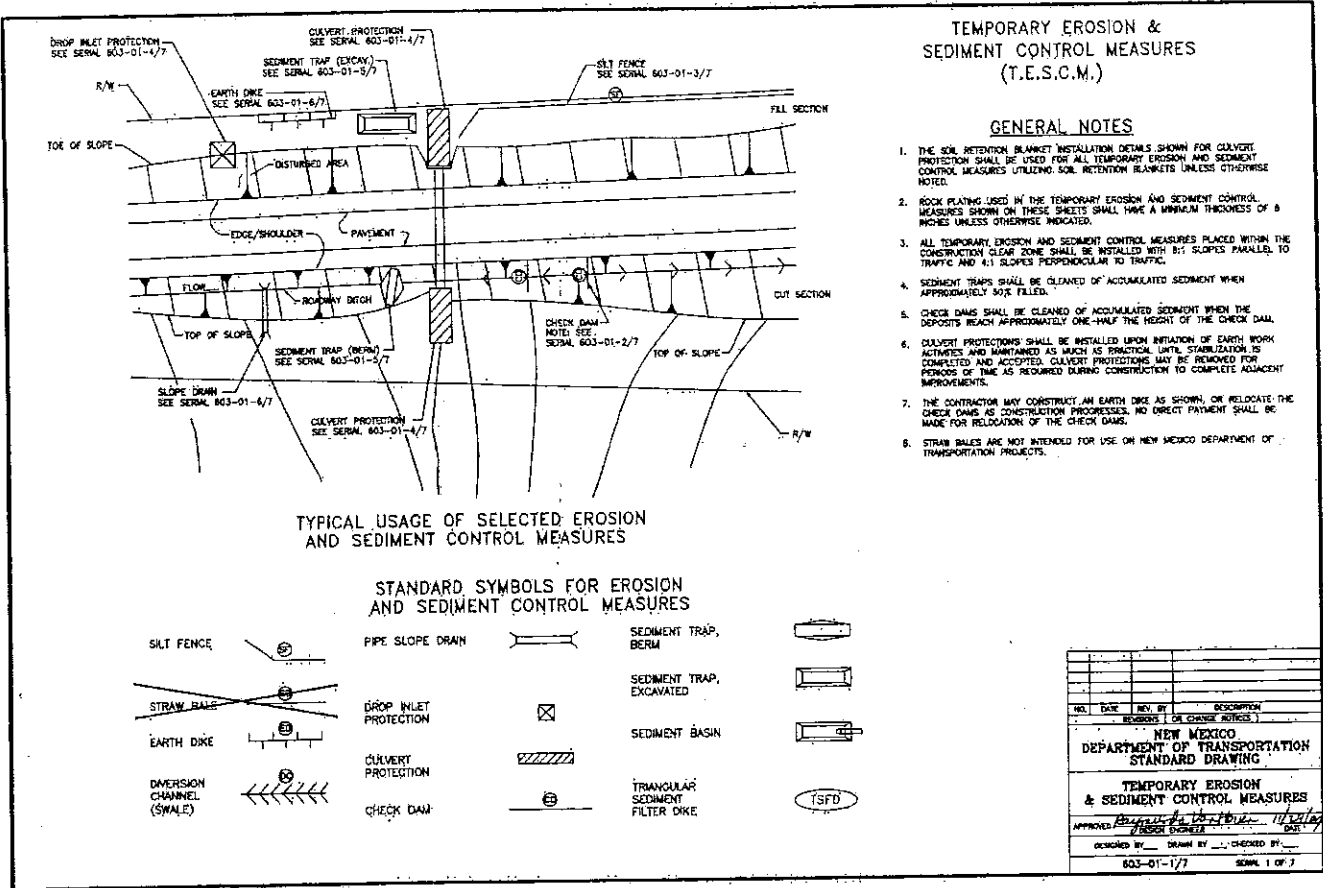
SWPP Plan Inspection Report	
Project Name:	_____
Purpose of Inspection:	_____ Date: _____
Inspector:	_____
Weather Information: Type, Time, Amount, and Duration of Each Storm Event since Last Inspection. List Dates and Location for Storm Events:	

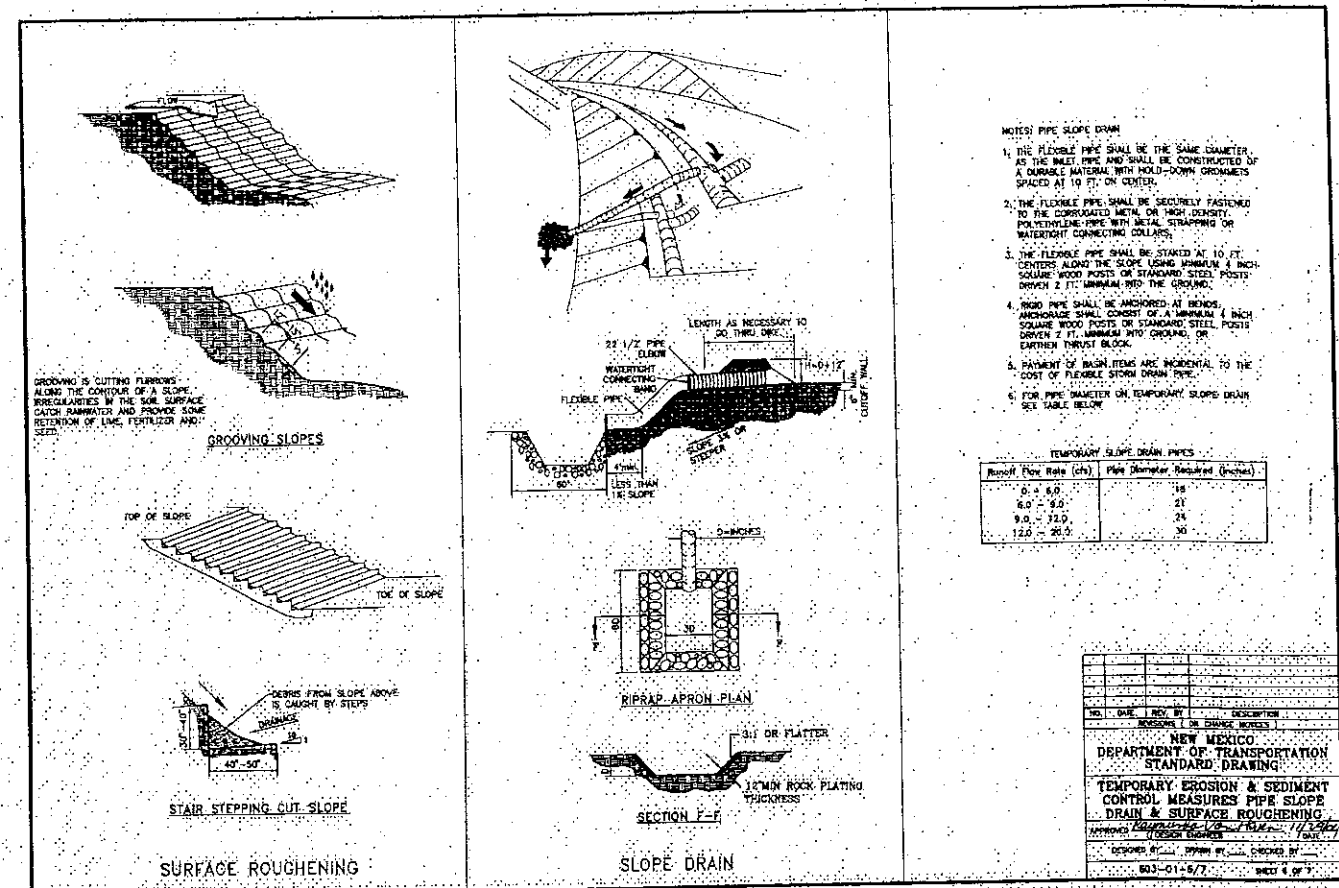
Location of NPDES Notice of Permit Coverage Posting: _____	
Has Land Disturbance Log been maintained? <input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Does the site map reflect current BMPs and Site Characteristics? <input type="checkbox"/> Yes <input type="checkbox"/> No	
Does the site map reflect current BMPs and Site Characteristics? <input type="checkbox"/> Yes <input type="checkbox"/> No	
Comments: _____	
Evidence of Discharges of Sediment or other Pollutants from the Site (Describe Below): <input type="checkbox"/> Yes <input type="checkbox"/> No	
Evidence of Spills or Leaks (Describe Below): <input type="checkbox"/> Yes <input type="checkbox"/> No	
Evidence of Offsite Sediment Tracking (Describe Below): <input type="checkbox"/> Yes <input type="checkbox"/> No	
Comments: _____	
Is Facility in Compliance with SWPP Plan and Permit? <input type="checkbox"/> Yes <input type="checkbox"/> No	
Incidents of Non-Compliance with SWPP Plan: _____	
Land Disturbance Log, Including Location and Date of Soil Disturbance, Activities and Stabilization Initiated: _____	

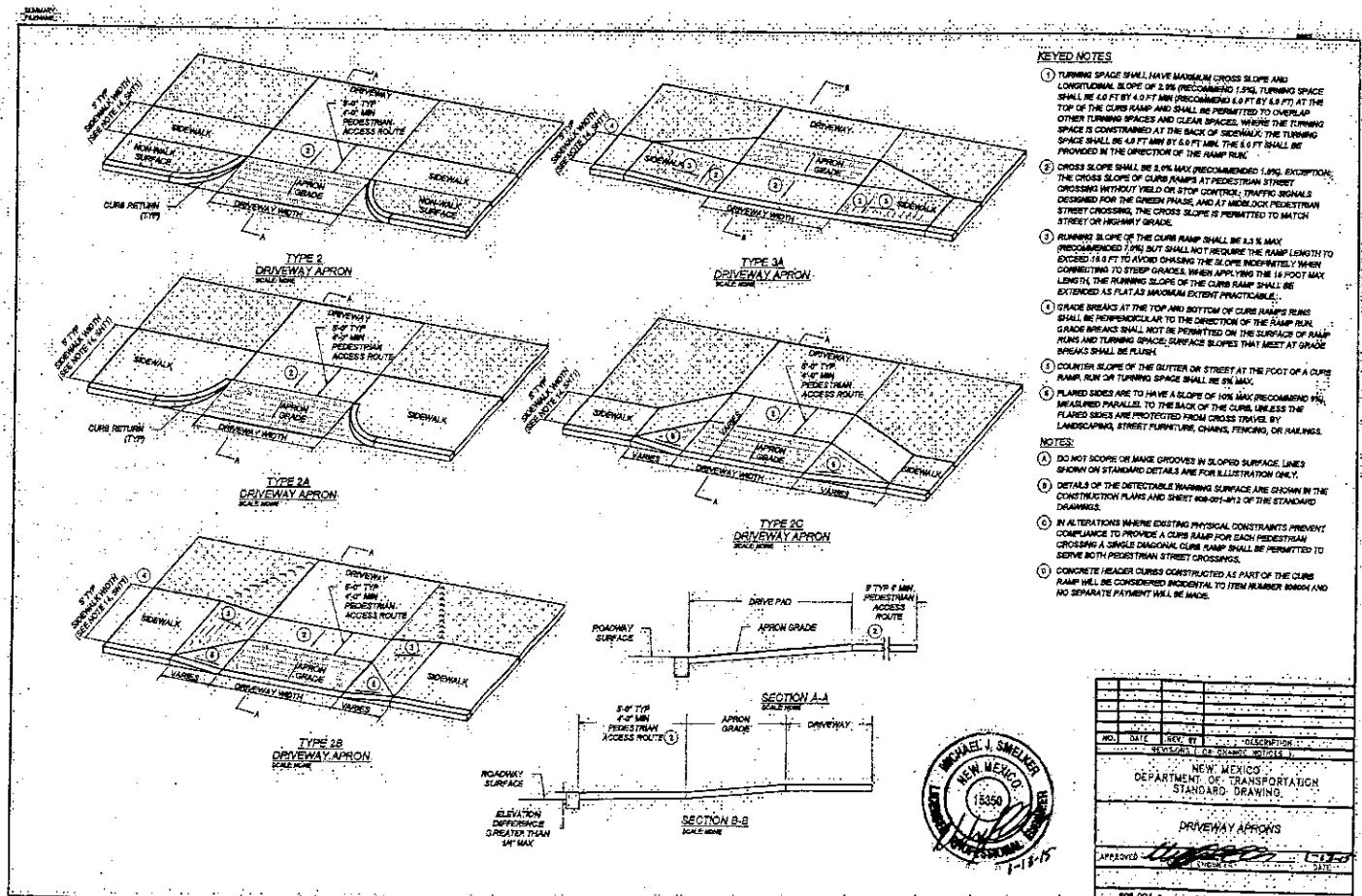
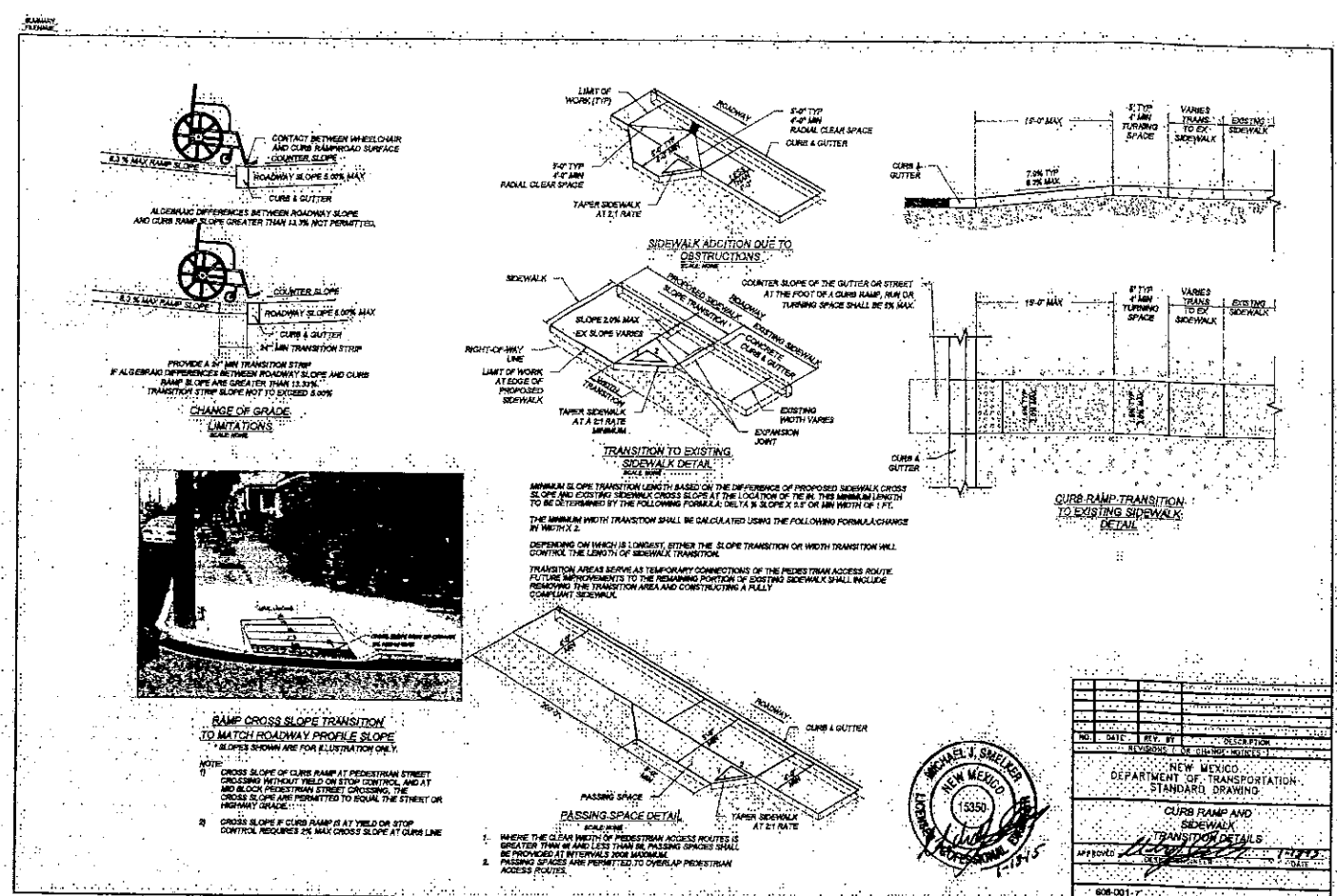
SWPP Plan Inspection Report (Continued)	
Certification Statement:	
"I certify under penalty of law that this document and all attachments were prepared under my direction or supervision in accordance with a system designed to assure that qualified personnel properly gathered and evaluated the information submitted. Based on my inquiry of the person or persons who manage the system, or those persons directly responsible for gathering the information, the information submitted is, to the best of my knowledge and belief, true, accurate, and complete. I am aware that there are significant penalties for submitting false information, including the possibility of fine and imprisonment for knowing violations."	
_____ (Signature)	
_____ (Please Print Name, Title)	

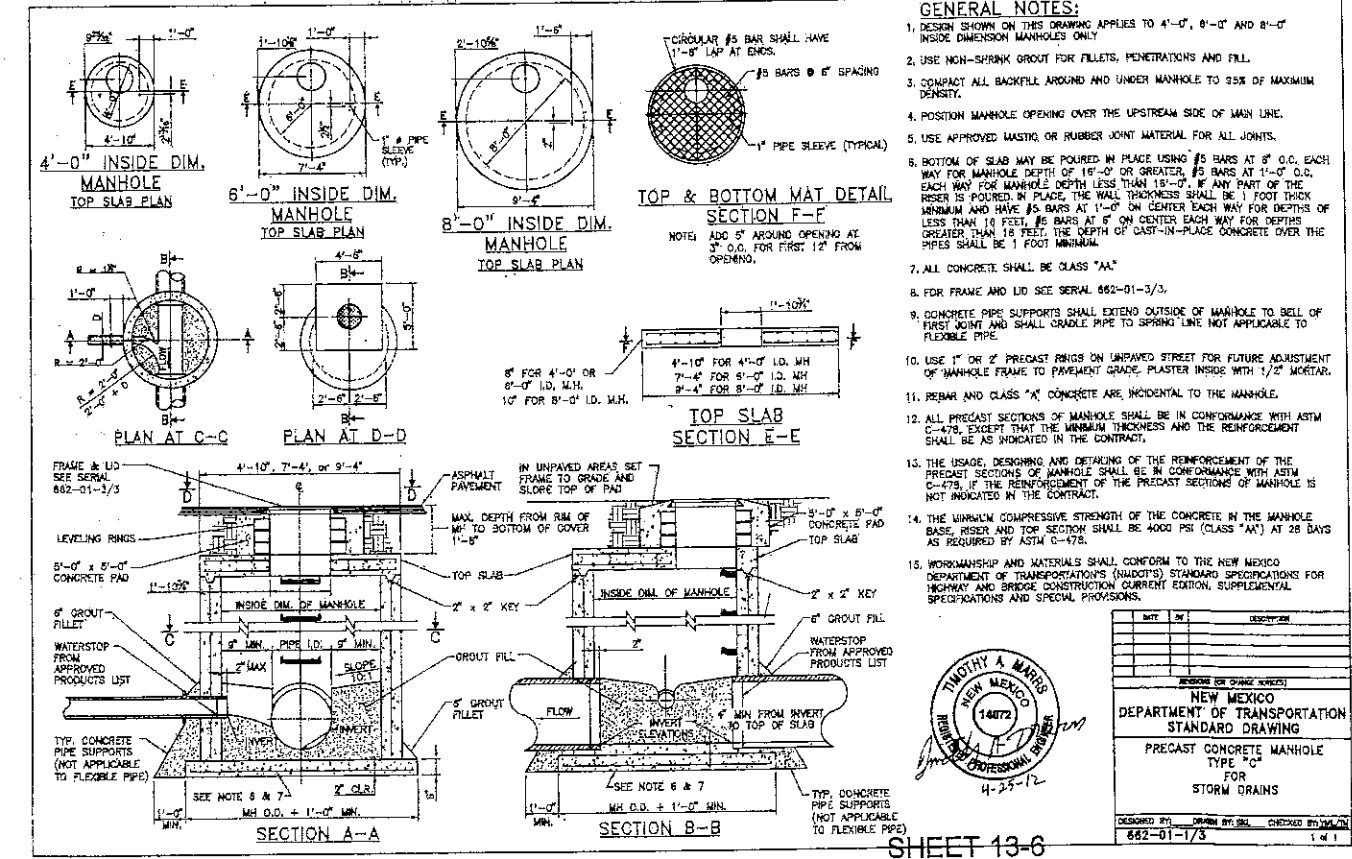
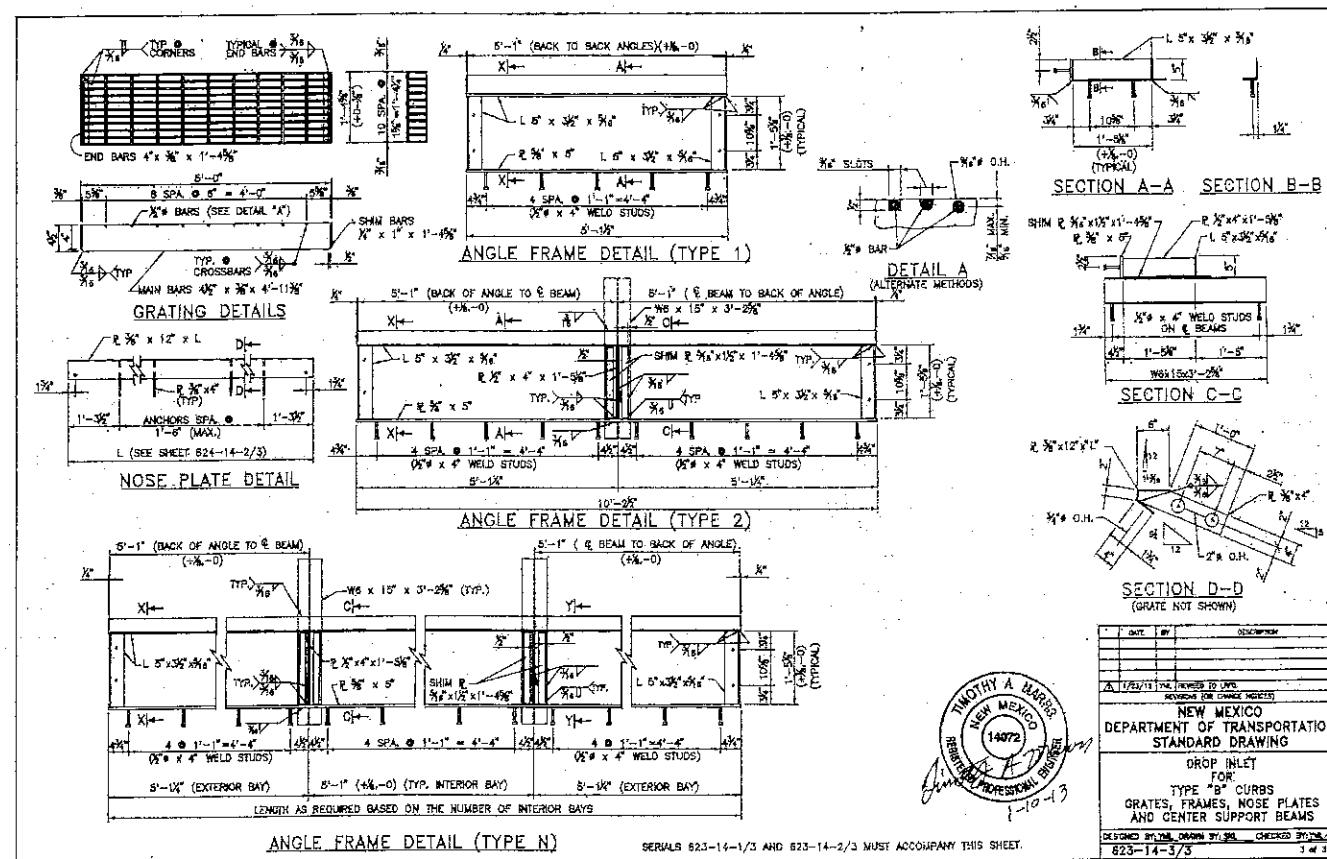
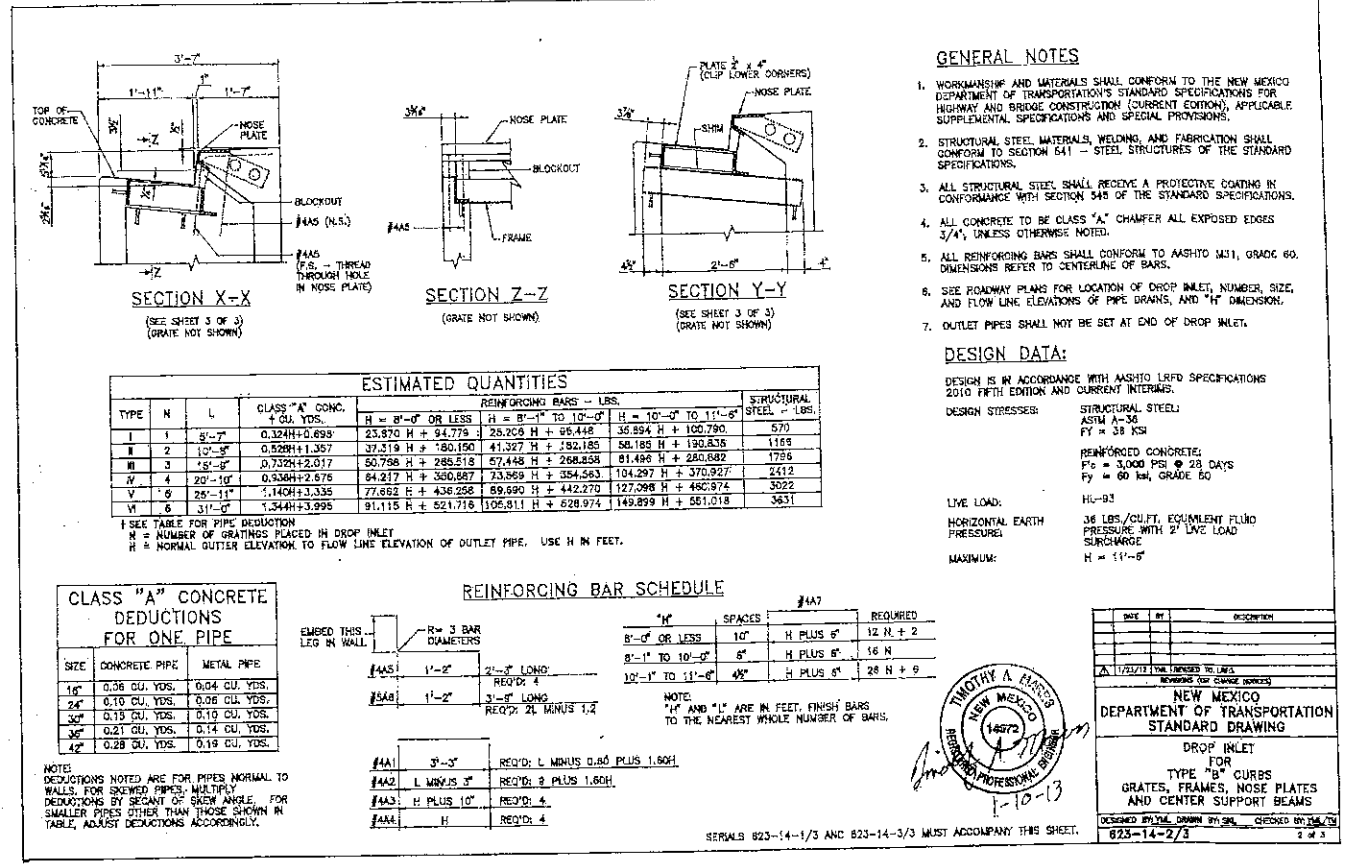
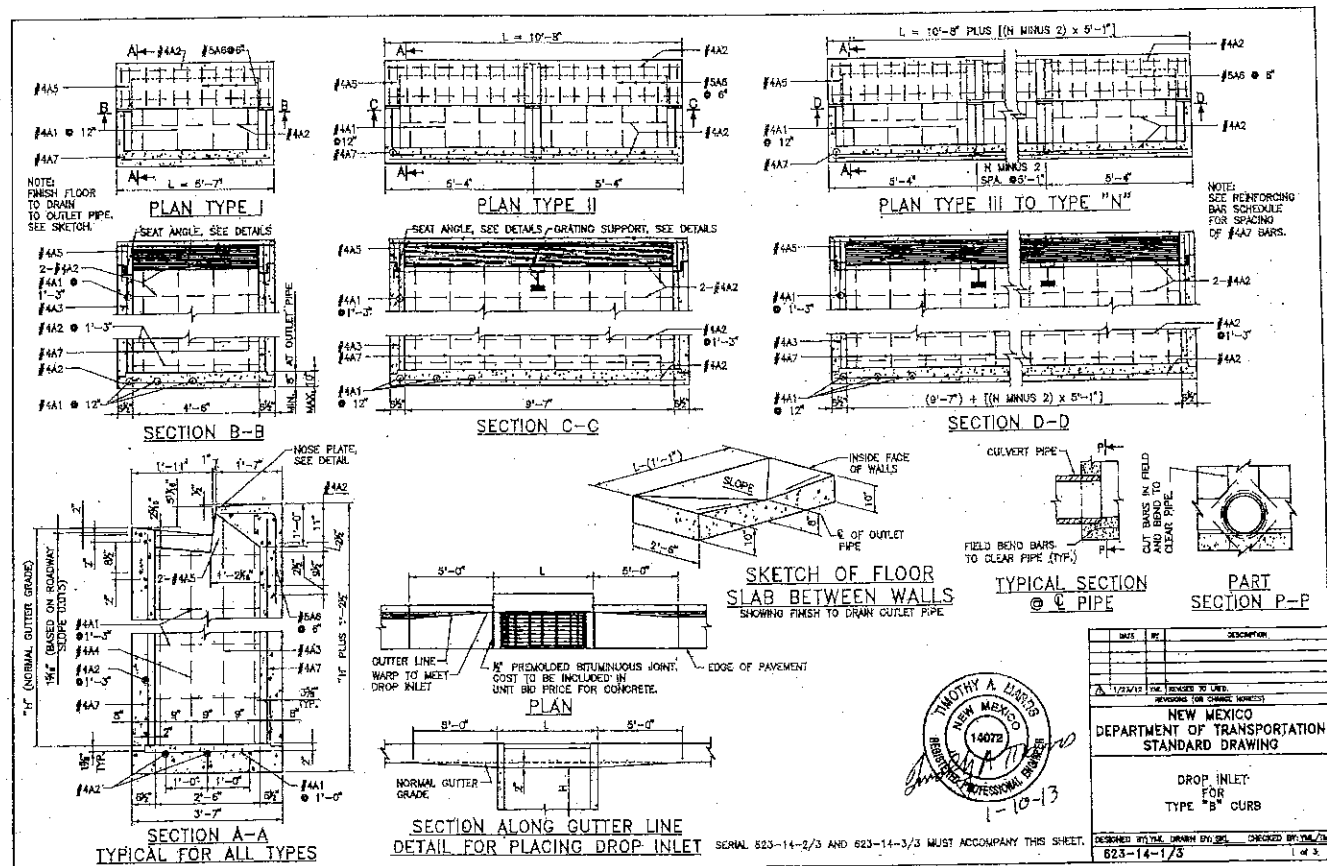
Santa Fe Engineering Consultants, LLC		PRELIMINARY SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 TO 4 FINAL SUBDIVISION PLAT SUBMITTAL FOR ST. FRANCIS SOUTH PHASES 1 & 2	
1599 St. Francis Drive, Suite B Santa Fe, N. M. 87505 (505) 982-2845 Fax (505) 982-2841 http://www.SFENGR.com		STORM WATER CONTROL DETAILS	
DATE: FEBRUARY 2017	SCALE: N.T.S.	SHEET: 12-4	

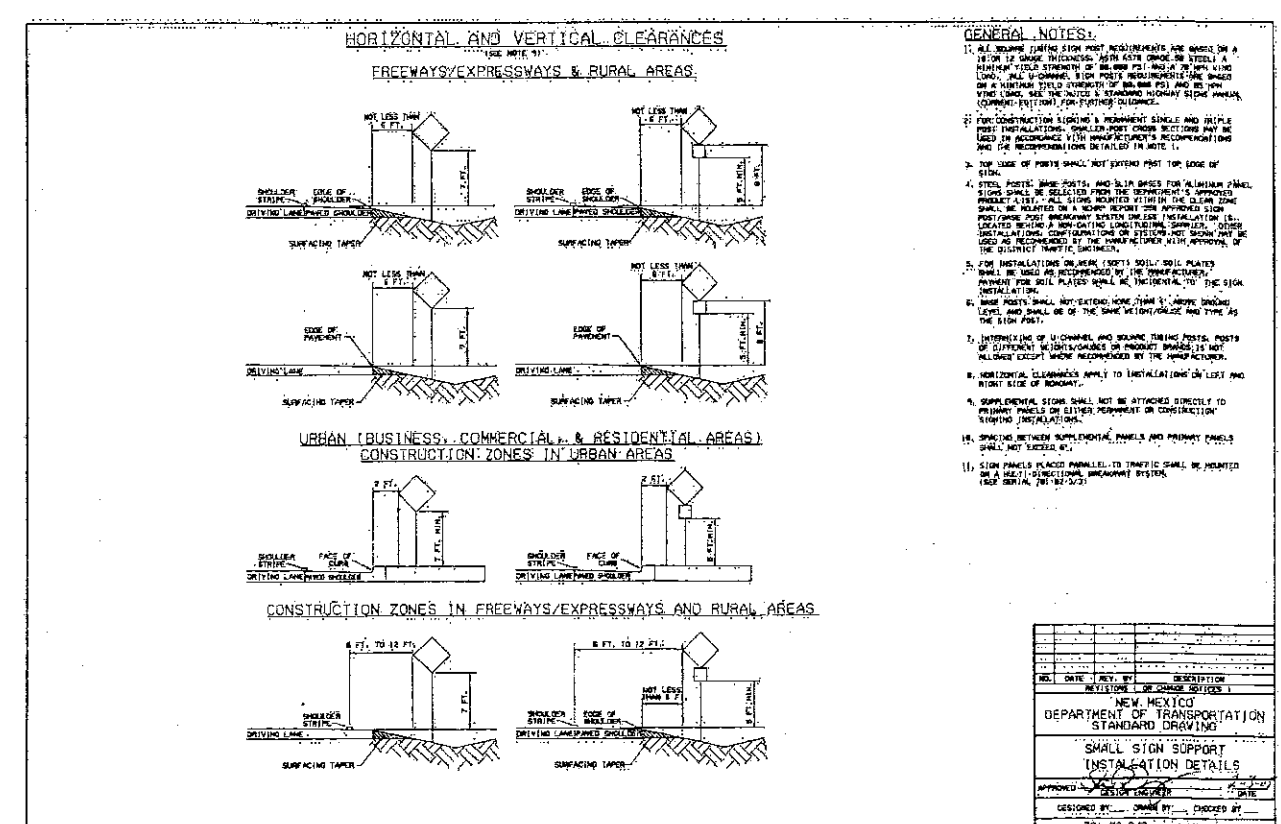
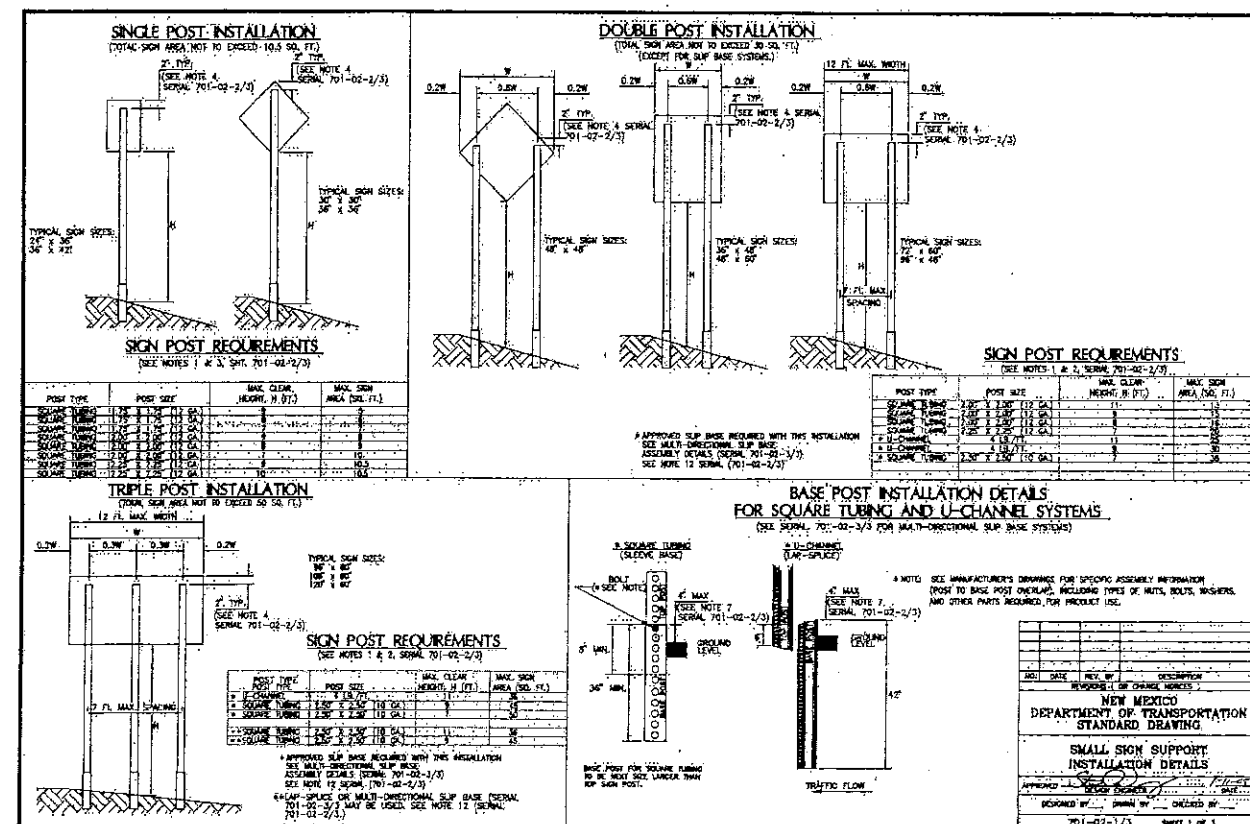
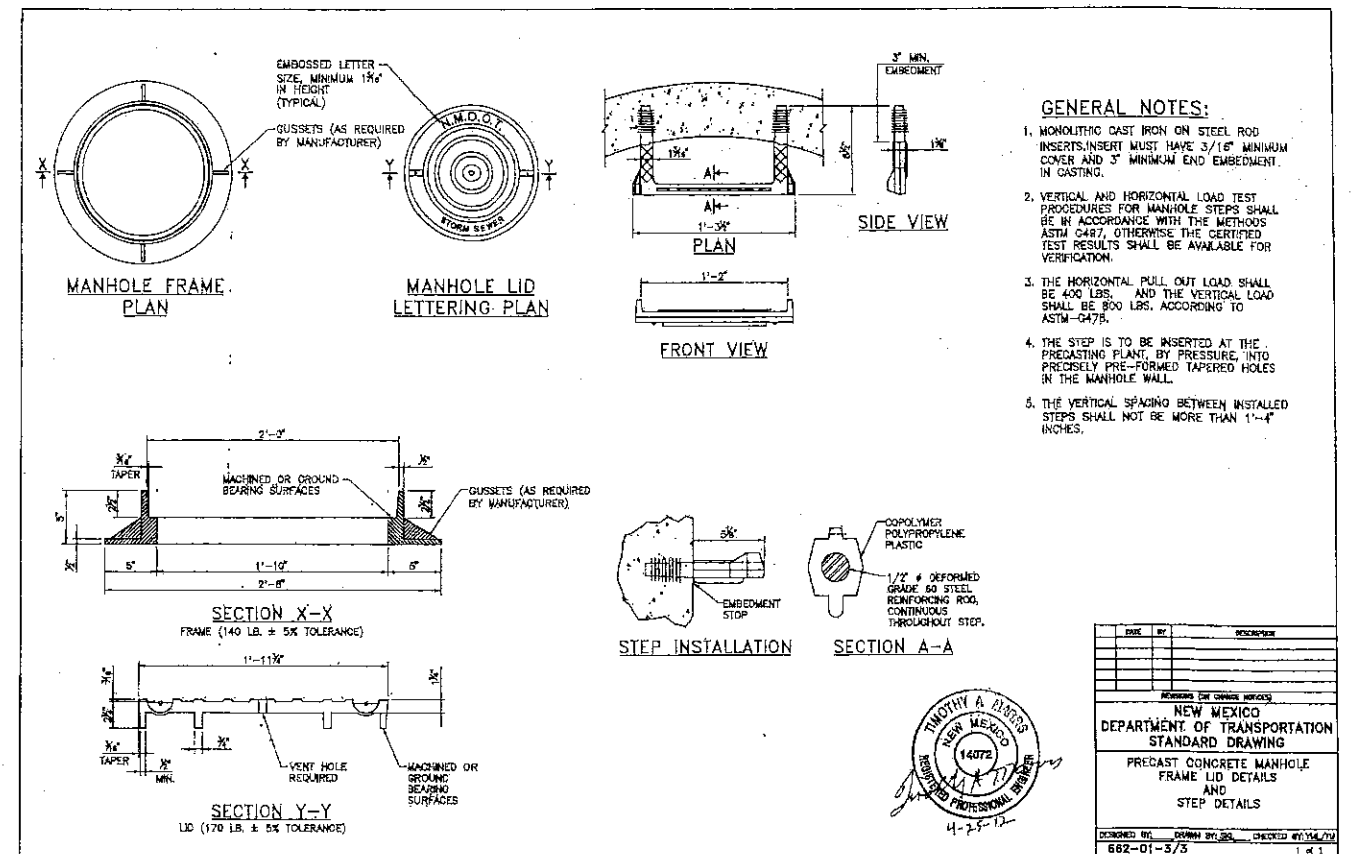
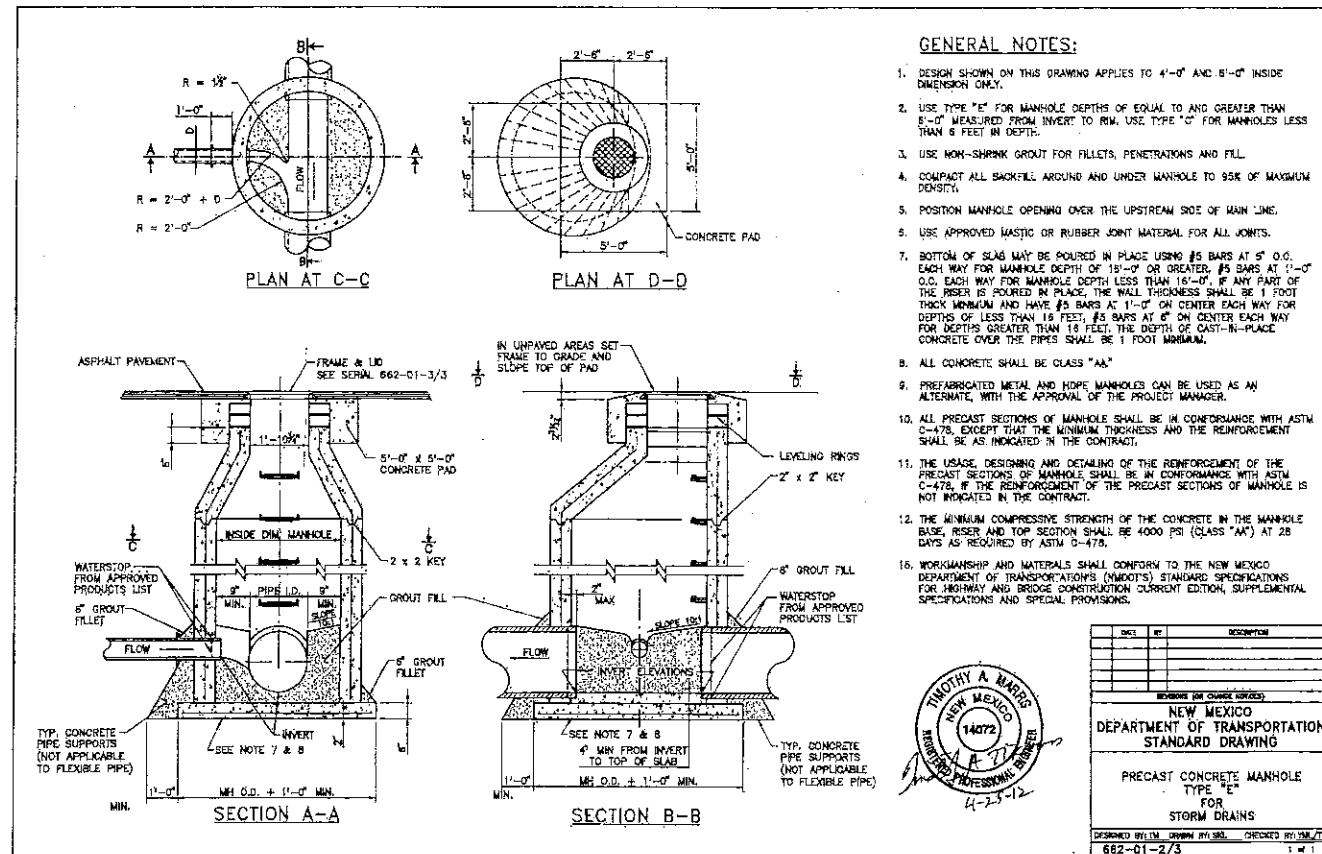
ENGINEER'S SEAL	
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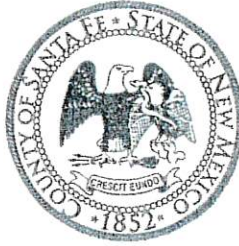




Henry P. Roybal
Commissioner, District 1

Anna Hansen
Commissioner, District 2

Robert A. Anaya
Commissioner, District 3



Anna T. Hamilton
Commissioner, District 4

Ed Moreno
Commissioner, District 5

Katherine Miller
County Manager

Date: April 17, 2017

To: Vicente Archuleta, Development Review Team Leader

From: Planning Division

Item: St. Francis South: Preliminary Plat Application for Phases 1-4 Final Plat Application for Phase 1-2

Background:

Jenkins/Gavin, representing Vegas Verdes, LLC has submitted an application for Preliminary and Final Plat approval by the BCC at their 6/13/2017 meeting. The subject property is at 199 Rabbit Road and is approximately 68.9 acres. It is zoned PDD. Preliminary Plat approval is requested for Phase 1- 4 (22 lots) and Final Plat approval for Phase 1 (4 lots) and Phase 2 (8 lots). Phase 1 completion is 2019, Phase 2 completion is 2023. On 12/14/ 2010, the BCC approved a Master Plan for this property, 22 lot, Large Scale Mixed Use Project, Case #Z 10-5360.

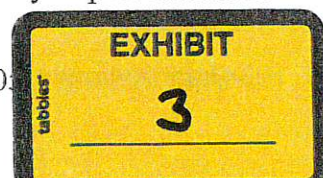
This property is zoned as a Planned Development in accordance with the SLDC. The PD zoning is based on the approved St. Francis South Business Park Master Plan in accordance with Section 8.10.10.2 of the SLDC.

Affordable Housing Review: This Preliminary and Final Plat does not include any housing units.

Open Space Review: Per SLDC 7.15 Open Space, 30% open space is required. 20% open space has been provided on the perimeter of the subject property. The additional 10% open space needs to be identified for preliminary plat approval.

Transportation Review:

A Traffic Impact Analysis has been submitted to the County and NMDOT for Phase 1 and 2 (the phases they are seeking final plat approval from the County). The main transportation impact will be to Rabbit Road which is currently a NMDOT facility. The road improvements for Rabbit Road were not included in the Planning Div. plan set but have been submitted separately to NMDOT. Under the current draft NE Connector MOU, upon completion of the NE Connector construction, Rabbit Road would be a County owned and maintained facility. NE Connector construction is currently scheduled for 2020. NMDOT will grant access permits and approve any improvements to Rabbit Road until that time.



The Consultant used ITE trip generation rates for each of the land use types proposed in Phase 1 and 2 and assumed a 1% growth in background traffic for Phase 1 and 2% for Phase 2. The developer proposes an east and west access from Rabbit Road with an internal loop road. Phase 1 improvements are construction of the west entrance, a right deceleration lane on Rabbit Road for the West entrance, a sidewalk, and on road bike lane on the north side of Rabbit Road from the west access to the Rail Trail crossing, and 700 feet of the loop road which will provide access to lots 1-4 of Phase 1. The interior loop roadway cross section indicates sidewalks on each side of the loop road. Phase 2 improvements are construction of the east entrance, a right deceleration lane on Rabbit Road for the East entrance, median and left turn lane improvements on Rabbit Road serving both entrances, and a sidewalk and bike lane from the East entrance to the West entrance. A roundabout is proposed to be constructed at the West entrance at Phase 2 or when the project's traffic generation warrants intersection control.

The proposed development is an SDA-1 area, with a corresponding Level of Service (LOS) of D; "speeds are somewhat reduced and motorists are hemmed in by other drivers". The projected traffic from this development is not likely to lower the LOS "D" for Rabbit Road or for County roads which connect to Rabbit Road for access to this development provided Phase 1 and 2 proposed TIA improvements. Staff recommends bike lanes also be constructed in Phase I on both sides of Rabbit Road providing bike access to the site from the populated areas in the Community College District.

Planning Staff Recommendation:

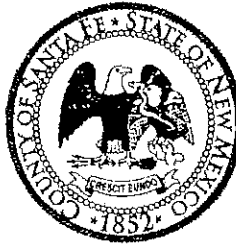
Staff recommends approval of the St. Francis South Preliminary Plat Application and Final Plat Application for Phases 1-2 with the following conditions:

1. Identification of 30% Open Space on preliminary plat.
2. Provide bike lines on both sides of Rabbit Road and internal loop road.

Henry P. Roybal
Commissioner, District 1

Anna Hansen
Commissioner, District 2

Robert A. Anaya
Commissioner, District 3



Anna T. Hamilton
Commissioner, District 4


Ed Moreno
Commissioner, District 5

Katherine Miller
County Manager

**PUBLIC WORKS DIVISION
MEMORANDUM**

Date: April 3, 2017

To: Vicente Archuleta, Development Review Team Leader

From: Johnny P. Baca, Traffic Manager Public Works 

Re: Case # 10-5364 Saint Francis South, Preliminary Plat for Phases 1-4 and Final Plat for Phases 1 & 2.

The referenced project has been reviewed for compliance of the Santa Fe County Sustainable Land Development Code (SLDC), and shall conform to roads and driveway requirements of Sustainable Design standards **Chapter 7.4 (Access and Easements)** and **Chapter 7.11 (Road Design Standards)**. The project is located south of Interstate 25, west of the Saint Francis Drive and north of Rabbit Road, the subject property is located at 199 Rabbit Road, within Section 11, Township 16 North, Range 9 East. The applicant is requesting Preliminary Plat approval for 22 lots (Phases 1-4) and Final Plat approval for Phase 1 comprising four lots and Phase 2 comprising eight lots. The \pm 68.9 subject property is zoned as a Planned Development District (PDD).

Access:

The applicant is proposing two connecting access points on Rabbit Road to the 68.94 acre tract. Rabbit Road's course is east and west. The road is approximately 24 feet wide with two 12-foot driving lanes and 5-foot shoulders and bar ditches on both sides. The posted speed limit is 40 miles per hour. This portion of road is under the jurisdiction of New Mexico Department of Transportation.

The applicant proposes that the western entrance will be the primary, full access and the east entrance will be limited access with right-in, right-out, and left-in turning movements. The Phase I access improvements are as follows: construction of the west entrance and approximately 700 linear feet of the on-site road. This access will be served with a right-turn deceleration lane. This Phase I development access is comprised of two twelve (12') foot drive lanes with curb and gutter and five (5') foot sidewalks and will be a full access driveway for the project. The Phase II access improvements include the following: completion of the on-site roadway, construction of the east entrance; median and left turn lane improvements on Rabbit Road serving both entrances; and a right turn deceleration at the east entrance. The applicant proposes that the westerly driveway will be constructed in Phase II with a signalized intersection or modified as a round-about or at such a time that the projects traffic generation warrants intersection control.

A Traffic Impact Analysis prepared by Santa Fe Engineering Consultants, LLC, dated February 2017. The purpose of the study was to assess the traffic impacts the project may have on the road system within the area and identify any necessary required road improvements.

Intersection Capacity Analysis

Santa Fe Engineering Consultants, LLC performed an Intersection Capacity Analysis, for the intersections delays. Santa Fe Engineering Consultants indicates that the intersection of St. Francis Drive / Rabbit Road, Rabbit Road / Old Galisteo Road and Rabbit Road / Rail Trail Parking Lot is operating at a Level of Service (LOS) of A's & B's, for 2015 existing conditions. Based upon the traffic counts and capacity analysis there are no existing operational deficiencies.

Santa Fe Engineering Consultants indicates that the intersection of St. Francis Drive / Rabbit Road, Rabbit Road / Old Galisteo Road and Rabbit Road / Rail Trail Parking Lot is operating at a Level of Service (LOS) of A's & B's, for 2015 existing conditions of A's, B's, & C's with and without the proposed development for 2019 implementation year conditions.

Santa Fe Engineering Consultants indicates that for 2023 NO BUILD implementation year all intersections will operate at a Level of Service (LOS) of A's, B's and C's. However, for a 2023 BUILD implementation year, two southbound intersections will operate at a Level of Service (LOS) of D's, Rabbit Road / Old Galisteo Road & Rabbit Road / West Driveway (Roundabout). Based upon the capacity analysis there are no operational deficiencies.

State Access Management Manual, September 2001 edition, of New Mexico State Highway and Transportation Department determines that Minimum Acceptable Level of Service Standards for an un-signalized intersection Rural Minor Arterial is a Level of Service D.

Intersection and Proposed Access Points

The intersection spacing required by NMDOT is 660 feet for un-signalized intersections for a 35 to 45 MPH speed lane. The intersection spacing between the full accesses is 600 feet and 300 feet for the partial access, respectfully.

Designs Requirements for Speed-change Lanes

Santa Fe Engineering Consultants has determined through the Intersection Capacity Analysis that Speed changes are warranted for proposed driveway of this project.

Conclusion:

Public Works has reviewed the Traffic Impact Analysis, dated February 2017, and feels that they can support the above mentioned project with the following conditions;

- Applicant shall provide a Geo – Technical Engineering Report prior to preliminary approval.
- Applicant shall provide proper signage/stripping plan (i.e. Bike Lanes, Speed Limit and Stop) that meet MUTCD standards.
- Stop bars will be required at all R1-1-30s.
- Applicant shall comply with all NMDOT regulatory requirements for driveway access onto Rabbit Road.

- Speed Change Lanes (east entrance & temporary west entrance) shall meet 300 foot Deceleration Distance with a 125 foot Deceleration Taper.
- Detailed Plans will be required for Round-About for Phase 2.
- All sidewalks shall comply with American Disabilities Act (ADA) requirements.
- Applicant shall provide Detectable Warning Surfaces on sidewalk/road crossings.
- Applicant shall provide crosswalks on all internal road crossings.
- A Right-of-Way permit must be obtained from NMDOT and provided to Santa Fe County.
- Applicant is required to submit a Traffic Control Plan.

Heary P. Roybal
Commissioner, District 1

Anna Hansen
Commissioner, District 2

Robert A. Anaya
Commissioner, District 3



Anna T. Hamilton
Commissioner, District 4

Ed Moreno
Commissioner, District 5

Katherine Miller
County Manager

Santa Fe County Fire Department Fire Prevention Division

Official Development Review

Date	03/21/2017		
Project Name	St. Francis South Preliminary Plat for phases 1 – 4, Final plat for phases 1 & 2.		
Project Location	St. Francis Drive and Rabbit Road		
Description	22 lot subdivision –Major Sub.	Case Manager	V. Archuleta
Applicant Name	Jenkins Gavin	County Case #	S10-5364
Applicant Address	130 Grant Ave, Suite 101 Santa Fe, New Mexico 87501	Fire District	Hondo
Applicant Phone	505-930-6149		
Review Type:	Commercial <input checked="" type="checkbox"/> Residential <input type="checkbox"/> Lot Split <input checked="" type="checkbox"/> Lot Line Adjustment <input type="checkbox"/>		
Project Status:	Family Trans <input type="checkbox"/> Inspection <input checked="" type="checkbox"/> Wildland <input type="checkbox"/> Variance <input type="checkbox"/> Zone No. _____		
	Approved <input checked="" type="checkbox"/> Approved with Conditions <input type="checkbox"/> Denial <input type="checkbox"/>		

The Fire Prevention Division/Code Enforcement Bureau of the Santa Fe County Fire Department has reviewed the above submittal and requires compliance with applicable Santa Fe County fire and life safety codes, ordinances and resolutions as indicated (Note underlined items):

Conditions of Approval

- Roadway meet County Standards (page #2)
- Driveway shall meet County Standards (page #2)
- Automatic fire protection system shall be required (page #3)
- Hydrants shall be in place and tested prior to building structures (page#3)

Fire Protection Systems

▪ Hydrants

Shall comply with Article 9, Section 903 - Water Supplies and Fire Hydrants of the 1997 Uniform Fire Code, inclusive to all sub-sections and current standards, practice and rulings of the Santa Fe County Fire Marshal.

Section 903.4.2 Required Installations. (1997 UFC) *The location, number and type of the fire hydrants connected to a water supply capable of delivering the required fire flow shall be provided on the public street or on the site of the premises or both to be protected as required and approved.*

Fire hydrants subject to possible vehicular damage shall be adequately protected with guard posts in accordance with Section 8001.11.3 of the 1997 UFC.

All fire hydrants shall be spaced so that the furthest buildable portion of a parcel shall be within five hundred feet (500') as measured along the access route.

Fire hydrant locations shall be no further than 10 feet from the edge of the approved access roadways with the steamer connections facing towards the driving surface.

Final fire hydrant locations shall be located in full view for incoming emergency responders. Landscape vegetation, utility pedestals, walls, fences, poles and the like shall not be located within a three foot radius of the hydrant per Article 10, Sections 1001.7.1 and 1001.7.2 of the 1997 UFC.

Supply lines shall be capable of delivering a minimum of 1,000 gpm with a 20-psi residual pressure to the attached hydrants. The design of the system shall be accordingly sized and constructed to accommodate for the associated demands placed on such a system through drafting procedures by fire apparatus while producing fire flows. The system shall accommodate the operation of two pumping apparatus simultaneously from separate locations on the system. All hydrants shall have NST ports. Final design shall be approved by the Fire Marshal.

No building permits shall be granted until such time as the fire hydrants have been tested and approved by the Santa Fe County Fire Marshal.

All hydrants shall comply with Santa Fe County Resolution 2000-55, Hydrant color-coding, marking and testing.

Automatic Fire Protection/Suppression

Automatic Fire Protection Sprinkler systems shall be required as per 1997 Uniform Fire, Article 10 Section 1003.2 in accordance with the Building Code as adopted by the State of New Mexico and/or the County of Santa Fe.

Fire Alarm/Notification Systems

Automatic Fire Protection Alarm systems are highly recommended per 1997 Uniform Fire and Building Codes as adopted by the State of New Mexico and/or the County of Santa Fe. Required Fire Alarm systems shall be in accordance with NFPA 72, National Fire Alarm Code for given type of structure and/or occupancy use.

▪ Life Safety

Fire Protection requirements listed for this development have taken into consideration the hazard factors of potential occupancies as presented in the developer's proposed use list. Each and every

John M. Salazar

From: Sandra J. Ely
Sent: Friday, June 30, 2017 1:09 PM
To: John M. Salazar
Cc: Bruce Frederick
Subject: St Francis South

Hi John- Here is my understanding of what needs to happen from the Utility Perspective for St. Francis to receive Preliminary Plat approval for phases 1 thru 4 and Final Plat approval for phases 1 and 2.

Preliminary Plat

St. Francis South met the requirement of 5.7.3.3 for subdivisions connecting to County Utility to obtain a ready, willing and able letter of commitment from Santa Fe County Utilities to provide utility service. This letter was issued September 2, 2015. Provided the Board of County Commissioners (Board) adopts "A Resolution Scheduling a New Water Delivery Pursuant to Resolution No. 2006-57 for the Second Six Month of 2017; And Approving a New Water Allocation Pursuant to Section 5.7.3.3 of the Sustainable Land Development Code(SLDC)" at the July 11, 2017 Board meeting, St. Francis South will have met the additional requirement of 5.7.3.3 that subdivisions that seek water service from the County Utility obtain a water allocation approval prior to preliminary plat approval by the Board.

Final Plat

Provided the Board approves the "County Utility and Line Extension and Service Agreement" between Santa Fe County and Vegas Verdes, LLC for the St. Francis South Subdivision at the July 11, 2017 Board meeting, St. Francis South will have met the requirement of 5.8.4.5 (2) of the SLDC that Subdivisions connecting to County water and waste water have entered into a water and waste water delivery agreement prior to final plat approval by the Board.

I have copied Bruce on this email in case he has something to add.

Sandra Ely
Project Manager III-Aamodt
Santa Fe County Utilities
424 Hwy 599 New Mexico Frontage Road, Santa Fe, NM, 87507
Office: 505-986-2426
Mobile: 505-699-9577

[Link to Google Map of our Office Location](#)



STATE OF NEW MEXICO

OFFICE OF THE STATE ENGINEER

CONCHA ORTIZ Y PINO BUILDING, 130 SOUTH CAPITOL, SANTA FE, NM 87501

TELEPHONE: (505) 827-6091

FAX: (505) 827-3806

TOM BLAINE, P.E.
STATE ENGINEER

April 10, 2017

Mailing Address:
P.O. Box 25102
Santa Fe, NM 87504-5102

Vicente Archuleta
Development Review Team Leader
Santa Fe County
P.O. Box 276
Santa Fe, NM 87504-0276

CERTIFIED MAIL
RETURN RECEIPT
REQUESTED

Reference: St. Francis South Preliminary Plat for Phase 1-4 and Final Plat Phase 1 and 2

Dear Mr. Archuleta:

The Water Use & Conservation/Subdivision Review Bureau of the Office of the State Engineer has reviewed the referenced subdivision proposal pursuant to the Santa Fe County Sustainable Land Development Code and the New Mexico Subdivision Act.

Based on the information provided, this office has determined that the subdivider can furnish water sufficient in quantity to fulfill the maximum annual water requirements of the subdivision, including water for indoor and outdoor domestic uses. Accordingly, a **positive** opinion is issued.

A staff memorandum providing specific comments is attached for your information. If you have any questions, please call Julie Valdez at 505-827-6790.

Sincerely,

Molly Magnuson

Molly Magnuson, P.E.
Water Use & Conservation/Subdivision Review Bureau Chief

Encl.

cc: OSE Water Rights Division, Santa Fe Office

John M. Salazar

From: Paul Kavanaugh
Sent: Friday, May 05, 2017 8:27 AM
To: John M. Salazar
Subject: FW: St. Francis South Subdivision
Attachments: DOT Memo 4.12.17.pdf

From: Duncan, Gwyneth, NMDOT [<mailto:Gwyneth.Duncan@state.nm.us>]
Sent: Wednesday, May 03, 2017 4:53 PM
To: Jennifer Jenkins; Michael Gomez; Paul Kavanaugh; Armijo, Ernest, NMDOT; Lujan, Jeremy, NMDOT; Fisher, Bradley F, NMDOT
Subject: RE: St. Francis South Subdivision

Thanks to all of you for speaking with me about the St. Francis South Subdivision. With this email I don't believe a meeting will be necessary.

My comments were conveyed in a letter to Mr. Archuleta from Jeremy Lujan (NMDOT) dated April 12. These can be found under "Environmental Bureau" in the letter.

The first bullet: For clarification, *Santa Fe County* should be including the St. Francis South project in the County's Phase A/B study for the proposed NE Connector project. I apologize if this was misinterpreted as a commitment to be met as part of the St. Francis South project.

The second bullet: Santa Fe County adopted a new code in 2016 making a request for a density waiver for this project unnecessary. Given that a request for a variance might have been denied in the past is no longer relevant.

The third bullet: The St. Francis South project would be along an existing road and should not have been compared to a proposed subdivision along an (as yet) non-existing road (i.e., SE Connector).

Fourth bullet: Any infrastructure improvements being proposed in NMDOT rights of way for the St. Francis South project will be reviewed in accordance with permit requests. The developer will need to contact the NMDOT District 5 permit agent as well as Hollis Lawrence at the NMDOT Environmental Bureau (505-331-8408) for an environmental review and certification of the right of way activities.

The Environmental Section has no further concerns or comments as part of this review. Please let me know if you have any questions. Thank you,

Gwyneth Duncan
NMDOT Environmental Bureau
P.O. Box 1149 Room 205
Santa Fe, NM 87504-1149

505-827-0751 (phone)
505-699-1633 (cell)

-----Original Appointment-----

From: Jennifer Jenkins [<mailto:Jennifer@jenkinsgavin.com>]



New Mexico DEPARTMENT OF
TRANSPORTATION

April 12, 2017

Mr. Vicente Archuleta
102 Grant Avenue
Santa Fe, NM 87501

RE: St. Francis South Subdivision

Dear Mr. Archuleta,

The appropriate engineers of the New Mexico Department of Transportation have reviewed the submitted material on the above referenced development and comments or concerns to be addressed are as follows:

Environmental Bureau: The proposed project is to develop 68.9 acres at Rabbit Road and St. Francis Drive in four phases. The proposed development will include offices, warehouses, multi-family housing, and rehabilitation/assisted living facilities, as well as improvements to Rabbit Road for the development. There also will be a need for utilities from the City to the County, some of which will need to be bored under I-25. On June 30, 2015, the NMDOT Environmental Bureau responded to a review of the proposed project. Although some of our comments have been addressed, the following still apply:

- Currently Santa Fe County and the NMDOT, in cooperation with FHWA, are conducting a Phase A/B study for the Northeast Connector, which is Rabbit Road from St. Francis Drive to Richards Avenue. The St. Francis South Subdivision has not been included in the Phase A/B study. Given the potential impacts to Rabbit Road by the proposed subdivision, and the changes being proposed to Rabbit Road by the developer, the proposed development should be included in the Phase A/B study. This is stated in both the Traffic Study and the Environmental Impact Report prepared by the developer as supporting documentation to the Application Submittal.
- In 2013, Santa Fe County MPO obtained data showing Vehicles Per Day totaling 5400 cars along Rabbit Road. Since the developer is requesting a waiver from the County to increase lot-density, any increase in density approved by the County should be factored into the Phase A/B study in accordance with the NMDOT's Location Study Procedures.
- Santa Fe County's CDRC denied a permit request for a similar subdivision near Santa Fe Community College pending construction of the Southeast Connector. In keeping with that decision, it would seem the Northeast Connector infrastructure

Susana Martinez
Governor

Tom Church
Cabinet Secretary

Commissioners

Ronald Schmeits
Chairman
District 4

Dr. Kenneth White
Secretary
District 1

David Sepich
Commissioner
District 2

Keith Mortensen
Commissioner
District 3

Butch Mathews
Commissioner
District 5

Jackson Gibson
Commissioner
District 6

should be in place before such an increase in capacity? But, again, that would need to be evaluated in the Phase A/B study.

- Rabbit Road is a NMDOT facility, also known as Frontage Road 2101. There are AASHTO requirements for frontage roads, which include certain requirements to be met for roundabouts. Since a roundabout is being proposed on Rabbit Road as part of the infrastructure for the subdivision, this and all other modifications to Rabbit Road being proposed by the developer (bike lanes, sidewalks, etc.) need to be evaluated following our Location Study Procedures and discussed in the Phase A/B Study.

Drainage Design Bureau:

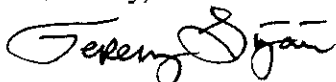
1. Sheet 5-1, note 1. Change the note to reflect reference to the 2014 NMDOT Standard Specifications.
2. Add note indicating that Check Dam No. 1 is to be constructed at the beginning of Phase 1.
3. Add note that Check Dam No. 2 is to be constructed at the beginning of Phase 2, despite being located in the area designated Phase 3.
4. Constructing these check dams at the beginning of construction will aid in compliance with erosion control requirements of the EPA 2017 Construction General Permit.

With the above changes incorporated into the Drawings, the Drainage Design Bureau has no objections. The following comments are presented as a courtesy to the design engineer and have no bearing on the review. "Recommend all MDIs with both inlet and outlet have the outlet invert 0.1' lower than the inlet invert. This will encourage flow through the MDI to counter losses, especially where turning 90 degrees."

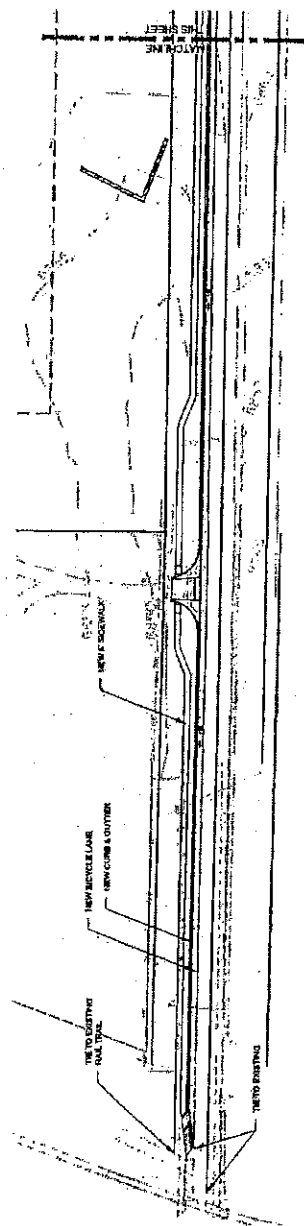
Traffic Technical Design Bureau/District 5 Traffic Engineer: The executive summary needs to give a better summary of what the report actually proposes, the figures need to have more information such as road names. See attached comments.

If there are any questions you may contact me at (505) 827-5249 or by email at jeremy.lujan@state.nm.us.

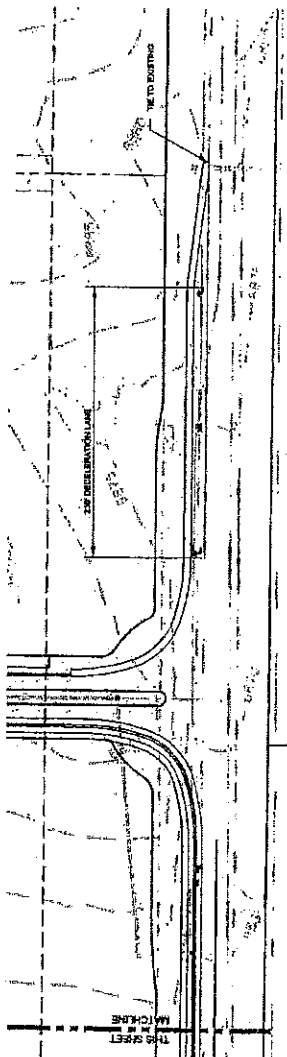
Sincerely,



Jeremy Lujan
Property Management Agent



Ⓐ OFFSITE ROADWAY IMPROVEMENTS
SECTION 11

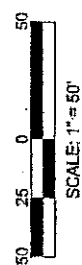


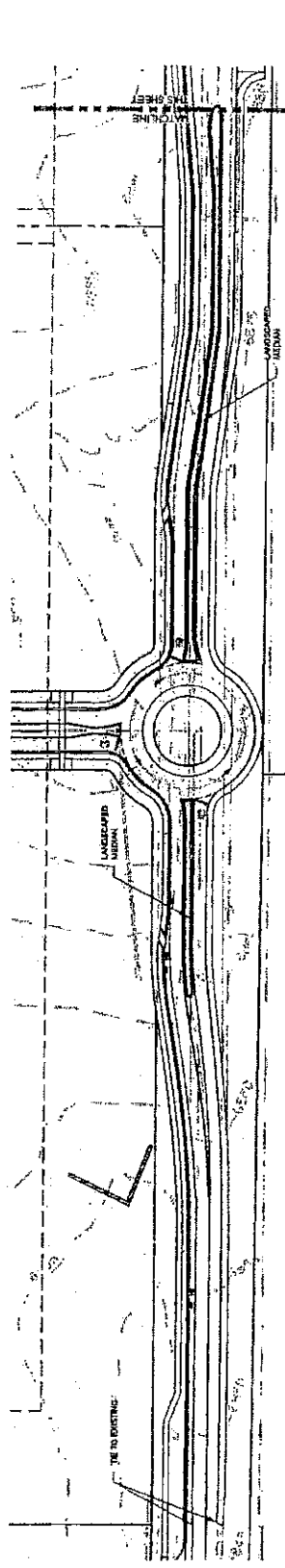
Ⓐ OFFSITE ROADWAY IMPROVEMENTS
SECTION 12

Location?

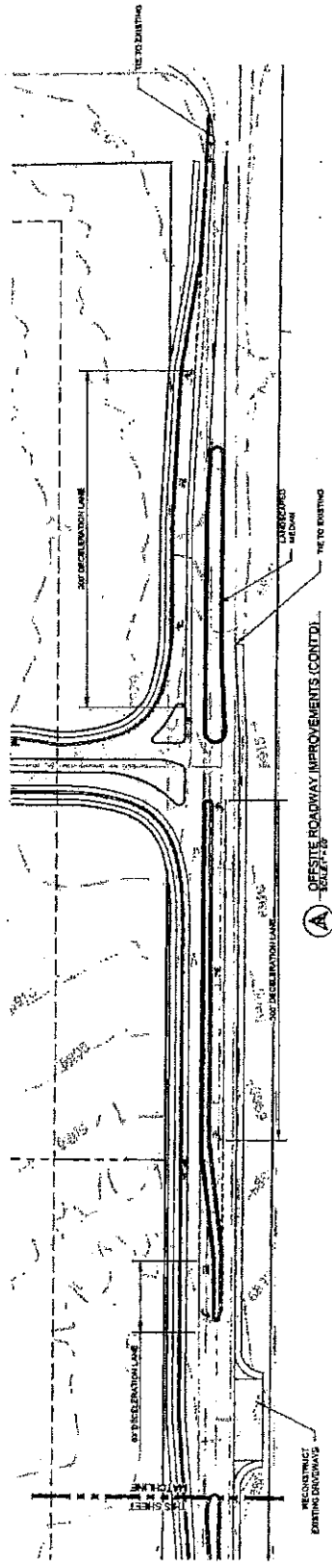
put Roadway names?

OFFSITE ROADWAY
IMPROVEMENTS
PHASE 1
FIGURE 20





Ⓐ OFFSITE ROADWAY IMPROVEMENTS
SHEET 18



Ⓐ OFFSITE ROADWAY IMPROVEMENTS (CONTD.)
SHEET 19

*location ?
put roadway names*

OFFSITE ROADWAY
IMPROVEMENTS
PHASE 1 & 2
FIGURE 21



**Santa
Fe
Engineering
Consultants, LLC**

Civil and Traffic Engineering
Construction Management
Land Development

**TRAFFIC IMPACT ANALYSIS
FOR ST. FRANCIS SOUTH
FINAL SUBDIVISION PLAT SUBMITTAL
FOR PHASES 1 AND 2
SANTA FE, NEW MEXICO**

Prepared By:

**Santa Fe Engineering Consultants, LLC
1599 S. St. Francis Drive, Suite B
Santa Fe, New Mexico 87505**

February 2017

EXECUTIVE SUMMARY

Based upon the traffic counts and capacity analysis there are no existing operational deficiencies.

The accident data indicates that there were two accidents between 2012 and 2014 at the South end St. Francis Drive at the intersection with Rabbit Road where St. Francis ends. The NMDOT and the County of Santa Fe are studying the North-East Connector and Rabbit Road. This location should be a part of their considerations.

According to the State Access Management Manual, the operational performance of an urban minor arterial should meet LOS D standards at a minimum. For the implementation year 2018 there are no are deficient Levels of Service.

For the implementation year 2023 there are no are deficient Levels of Service.

A bicycle lane will be constructed along the frontage of the property on Rabbit Road. Non-motorized facilities shall be designed in accordance with the Americans with Disabilities Act and applicable NMDOT standards. Curb ramps shall be provided on urban sections where sidewalk and curb returns exist.

There are no deficient Levels for the horizon year. A traffic signal or roundabout may be required to mitigate these deficiencies. Future studies will be performed as per the conditions of approval.

*Improve this
not clear
what improvements will occur ex. Rabbit Road*

MEMORANDUM
New Mexico Office of the State Engineer
Water Use and Conservation Bureau

DATE: April 10, 2017

TO: Molly Magnuson, P.E., Water Use and Conservation Bureau Chief

FROM: Julie Valdez, Senior Water Resource Specialist *JMV*

SUBJECT: St. Francis South Preliminary Plat for Phase 1-4 and Final Plat
Phase 1 and 2

SUMMARY

On March 14, 2017 the Water Use & Conservation Bureau of the Office of the State Engineer (OSE) received a request to re-review the St. Francis South Preliminary Plat for Phase 1-4 and Final Plat Phase 1 and 2.

This office reviewed and provided comments for the St. Francis South Master Plan Amendment/ Variance, Preliminary Plat and Preliminary Development Plan on January 10, 2014 and February 26, 2014. For details, please refer to those letters.

The applicant seeks approval from Santa Fe County for a Preliminary Plat Phase 1-4 for 22 lots, and a Final Plat phase 1-2. The New Mexico Subdivision Act (Act) does not require an opinion from the OSE for a Final Plat plan. Therefore, this opinion is only for the Preliminary Plat.

The proposal provides an outline to subdivide a 68.9 acre tract into a 22-lot large scale mixed use project with parcels ranging in size from 1.04 to 2.9 acres. At build out, the gross building area is anticipated to be approximately 760,000 square feet with a combination of office, community service, retail, warehouse and residential uses. The property is bordered by I-25 to the north, St. Francis Drive to the east, Rabbit Road (the Northeast Connector) to the south and two large residential lots to the west. The proposed water will be supplied by the Santa Fe County Utilities.

The subdivision proposal was reviewed pursuant to the Santa Fe County Land Development Code (Code) adopted by Ordinance 2016-9 dated December 13, 2016 and New Mexico Subdivision Act (Act). Based on the information provided, the water supply proposal is in compliance with the requirement of Section 7.13.2.3 of the Code and Section 47-6-11.F (1) of the Act. Accordingly, a **positive** opinion should be issued.

The documents submitted to this office include a Disclosure Statement, a Ready, Willing, and Able Letter dated September 2, 2015, the Preliminary/Final Plat Application dated February 24, 2017, a Water Budget, and Plat Maps.

WATER DEMAND ANALYSIS AND WATER CONSERVATION

The proposal contains a Water Budget that estimates the water demand for the entire development to be 54.44 acre feet per year (AFY) assuming 5.87 AFY for an office space, 1.57 AFY for a warehouse, 32 AFY for multifamily housing, 12 AFY for a rehabilitation facility, and 3 AFY for landscaping. Detailed information regarding the 12 AFY water use for the 50,000 square-foot rehabilitation facility was not provided. It is recommended that the developer update

the water budget and include detailed information regarding water use at the proposed rehabilitation facility. The water budget estimate for the other aforementioned facilities is consistent with the findings presented in the City of Santa Fe's November 2009, *Resolution No. 2009-116* that adopted standard formulas by water use category for calculating development water budgets.

According to the proposal *"at the time of lot development, active water harvesting will be provided in accordance with the (Code) Section 7.13.11.7. The cisterns for each facility will be sized in accordance with the landscaping water budget and based on the highest monthly water demand."*

WATER AVAILABILITY ASSESSMENT

The proposed water supply will be provided by Santa Fe County Utilities (SFCU). A letter from the utility, stating they are ready, willing, and able to provide water service for the development is included in the proposal as required by Section 7.13.2.3 of the Code. It is noted that a water budget of 62.81 AFY was anticipated at the time when the letter was drafted.

Section 47-6-11.F (1) of the Act requires that the developer provide documents demonstrating that water sufficient in quantity to fulfill the maximum annual water requirements of the subdivision/development is available. Therefore, the OSE reviews the water rights and the physical water availability. Based on OSE records, the County currently holds sufficient water rights to provide service to the proposed development.



SUPERVISORS
Alfredo Roybal
José Varela López
Sigmund Silber
Shann Stringer
Tim Henry

SANTA FE – POJOAQUE SOIL AND WATER CONSERVATION DISTRICT

4001 Office Court Drive, Suite 1000

Santa Fe, New Mexico 87507

Telephone (505) 471-0410 Extension 5

Fax (505) 471-0933

April 11, 2017

Vicente Archuleta
Development Review Team Leader
Santa Fe County
102 Grant Avenue
Santa Fe, NM 87504

Re: Case #S 10-5364 St. Francis South Preliminary Plat for Phases 1-4 and Final Plat for Phases 1 and 2

Dear Mr. Archuleta:

Upon review of the aforementioned submittal, Santa Fe-Pojoaque Soil and Water Conservation District finds no deficiencies with regard to terrain management nor flood control as outlined in sections 7.17 and 7.18, respectively, of the Sustainable Land Development Code. The District does however have one suggestion:

- With regard to Ponding area 1, rip-rap or a similarly effective anti-erosion armoring method should be utilized for an additional 400-425 feet to prevent further incision of the arroyo and premature sedimentation of the ponding area.

If there are any questions please don't hesitate to call me at (505) 660-5828 or email josevarelalopez@aol.com

Sincerely,

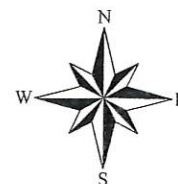
José J. Varela López
Vice-chairman



Legend

- ROADS
- DRIVEWAYS
- Parcels

1:4,200
1 inch represents 350 feet
0 95 190 380 570 760 Feet



2015 Orthophotography
2 FOOT CONTOURS

This information is for reference only.
Santa Fe County assumes no liability for
errors associated with the use of these data.
User are solely responsible for
confirming data accuracy.



May 30, 2017

The newspapers of **New Mexico** make public notices from their printed pages available electronically in a single database for the benefit of the public. This enhances the legislative intent of public notice - keeping a free and independent public informed about activities of their government and business activities that may affect them. Importantly, Public Notices now are in one place on the web (www.PublicNoticeAds.com), not scattered among thousands of government web pages.

County: Santa Fe

Printed In: Santa Fe New Mexican

Printed On: 2017/05/23

LEGAL #82680

BCC CASE # 10-5364 St. Francis South Preliminary Plat Approval for Phases 1-4 and Final Plat Approval for Phase 1 and 2

NOTICE OF PUBLIC HEARING

Notice is hereby given that a public hearing will be held to consider a request by Vegas Verdes, LLC, Applicant, JenkinsGavin, Inc., Agent, requesting Preliminary Plat approval for Phases 1-4, which consists of 22 lots and Final Plat approval of Phase 1, consisting of 4 lots and Phase 2, consisting of 8 lots, on 68.94 acres, of the St. Francis South Mixed-use Subdivision. The property is located at 199 Rabbit Road, via St. Francis Drive, within Section 11, Township 16 North, Range 9 East, (Commission District 4).

A public hearing will be held in the County Commission Chambers of the Santa Fe County Courthouse, corner of Grant and Palace Avenues, Santa Fe, New Mexico on the 13th day of June 2017, at 5 p.m. on a petition to the Board of County Commissioners.

Please forward all comments and questions to the County Land Use Administration Office at 986-6225.

All interested parties will be heard at the Public Hearing prior to the Commission taking action.

All comments, questions and objections to the proposal may be submitted to the County Land Use Administrator in writing to P.O. Box 276, Santa Fe, New Mexico 87504-0276; or presented in person at the hearing.

Published in the Santa Fe New Mexican on May 23, 2017.

Public Notice ID:



VII. F. Case V 17-5030, St. Francis South Road Standards Variance. Vegas Verdes LLC, Applicant, Jenkins Gavin, Inc., Agent, is requesting a variance of the Sustainable Land Development Code Chapter 7.11 Road Design Standards, Table 7-12, Urban Road Classification and Design Standards (SDA-1) to allow a right-of-way width of 65 to 70 feet in different locations rather than the 120 feet of right-of-way as required in Table 7-12 which classifies the interior loop road as a minor arterial. The 69.84-acre property is located at 199 Rabbit Road within Section 11, Township 16 North, Range 9 East (Commission District 4), SDA-1

Ms. Lucero recited the case caption and provided the following report:

MS. LUCERO: On March 21, 2017, the Application was presented to the Hearing Officer. The Hearing Officer recommended approval of the requested variance with the following condition: That the right-of-way width shall be a minimum of 70 feet.

The Applicant is requesting a variance of SLDC Table 7-12, Urban Road Classification and Design Standards for SDA-1, with regard to the right-of-way width for the interior loop road servicing 22 lots within the mixed-use subdivision. Table 7-12 classifies the loop road as a minor arterial based on the estimated average daily traffic, which requires 120 feet of right-of-way intended to accommodate a four-lane road. The Applicant is proposing a right-of-way ranging from 65 to 70 feet in width.

The Applicant's Agent argues that "a minor arterial is a high-capacity urban road with the primary function of delivering traffic to collector roads or highways. Rodeo Road is a minor arterial connecting opposite sides of Santa Fe. The project's interior loop road clearly does not function as a minor arterial. Granting of the variance will allow the road to be improved to a standard that is appropriate for the development, similar to the loop road within the Rodeo Business Park, which will be sufficient for the number of lots, the zoning, and the approved uses."

The Applicant's agent addresses the variance criteria as stated in the staff report and staff has also provided responses to those variance criteria.

The applicable requirements under the Sustainable Land Development Code which governs this Application are the following: Chapter 7.11 Road Design Standards, Table 7-12 Urban Road Classification and Design Standards; Chapter 4, Section 4.9.7.1, Variances; Chapter 4, Section 4.9.7.4, Variance review criteria; and Chapter 4, Section 4.9.7.5, Variance conditions of approval.

Recommendation: The subject road is an internal loop road that provides access to this development/subdivision only. A two-lane road with curb and gutter, sidewalks, and bike lanes meeting the standards of Table 7-12 will be sufficient to serve the development. Therefore, a 70-foot right-of-way is sufficient. Staff recommends approval of the Applicant's request for a variance of Table 7-12, Urban Road Classification and Design Standards to reduce the right-of-way width to 70 feet.

This matter came before the Hearing Officer for a hearing on March 21, 2017. The Hearing Officer recommended approval of the Application with the following condition: That the right-of-way width shall be a minimum of 70 feet.

If the decision of the Planning Commission is to approve the application, you may consider adopting the Hearing Officer's findings of fact and conclusions of law in the written recommendation.



Thank you, Mr. Chair, I stand for questions.

CHAIR KATZ: Thank you. Questions, Mr. Katz?

COMMISSIONER KATZ: I don't disagree with what your recommendation is but it is puzzling to me why you would recommend approval here and not in the past case. This is a road that is going to vastly more traffic, serve lots more people and yet you're saying that what's required by the code really is too much. As I say, I don't disagree necessarily, but why such hard nature on the first one and then – all right, this will do here?

MS. LUCERO: Mr. Chair, Commission Member Katz, the only variance being sought here is of the right-of-way so they don't have to dedicate the 120 feet. They are complying with all of the other requirements of the roads standards. The road width, they're complying with that. They've got their sidewalks that they're complying with, their bike trails, a median, everything complies; it is just the fact that they can fit what is required within a 70-foot right-of-way rather than a 120-foot right-of-way. And the reason for the 120-foot right-of-way is to accommodate up to a four-lane road in which case it won't be warranted with this type of development. It is only servicing the development itself and not any other properties outside of this. It's an internal road.

COMMISSIONER GONZALES: Chair Anaya.

CHAIR ANAYA: Mr. Gonzales.

COMMISSIONER GONZALES: Vicki, regarding this plat, I guess it's Exhibit 2, the back of Exhibit 2. NBF-16. It's on the back of Exhibit 2. These lots have a zoning of mixed use. Is that correct?

MS. LUCERO: Mr. Chair, Commission Member Gonzales, yes, this property is a mixed-use subdivision.

COMMISSIONER GONZALES: Does that allow residential as well as commercial? What does that do?

MS. LUCERO: Mr. Chair, Commission Member Gonzales, it does allow for a mix of residential and non-residential or commercial uses.

COMMISSIONER GONZALES: Okay, so the way that Rabbit Road runs east and west, and when you're coming off of St. Francis, and you turn on to Rabbit Road, the first entrance is maybe a few hundred feet, is the flow of the traffic going to go, and they can go in, but they've got to exit at the further exit. Is that correct?

MS. LUCERO: Mr. Chair, Commission Member Gonzales, yes, I believe that is correct.

COMMISSIONER GONZALES: Thank you. I was just a little concerned that the first entrance would be pretty close to St. Francis and that would be kind of [inaudible] but they make it you can only go in there but you can't exit, which is good, because the people that are going to come across to get on to Rabbit Road and head east it will be a lot better exit. Thank you.

CHAIR ANAYA: I guess this would be a question for the architects, but I noticed that there's somewhat of a turnaround on NBF-17. I'm sure you'll explain that to us. Any other questions for staff at this particular time? Having none, let's hear from the owners.

Duly sworn, Jennifer Jenkins testified as follows:]

JENNIFER JENKINS: Thank you, Commissioners. I'm Jennifer Jenkins with JenkinsGavin. Also here is Mike Gomez from Santa Fe Engineering on behalf of Vegas Verdes, LLC. We're also joined by Mr. Ernie Romero, who is one of the

principals of Vegas Verdes.

We are here this evening in request for the variance that is before you for a reduction of right-of-way width for the St. Francis South Subdivision. St. Francis South was approved as a large-scale mixed-use project. The master plan was approved by the Board of County Commissioners in 2010. We are also currently in a process for a – we have been working out utility connections and a lot of other infrastructure details and we are now in the process. We approached the County that we are ready for our subdivision application to create these individual lots, and as we were looking at the standards and looking at the SLDC we realized that the 120 feet of right-of-way was excessive as staff has stated, because again, a minor arterial is intended, like Rodeo Road is a minor arterial. It connects the east side of Santa Fe to the west side of Santa Fe. Obviously, this is not how this internal, private roadway is functioning.

And so we are completely complying with all of the standards as far as lanes and pedestrian circulation and everything else. We are not asking for anything to reduce any of the standards in terms of what is actually going to be built on the ground. I think that's a very important distinction. The only thing is that swath of real estate that accommodates the road. And so when we approached staff and realized we wanted to pursue a variance of the right-of-way width we were told that because the Planning Commission has the purview over variance that this was going to be a separate, standalone process. So we are running a parallel path also with our subdivision application.

The subdivision application under the SLDC goes straight to the Board of County Commissioners. So we had a requirement that we complete the variance process prior to our subdivision request being heard by the Board of County Commissioners, which is tentatively scheduled for June. So originally our intent was we were going to lump it all together. We were going to take the variance request and the subdivision request as one application, as joint applications, it would be more accurate to say, and send them to the BCC. And then as we got a little deeper into it and looking at it with staff they realized that the code doesn't permit that type of process. The code is very clear that this body has the jurisdiction over variance requests, period, as was stated by Vicki at the beginning of the hearing in the informational item that addressed how the code addressed the authority of this body.

So this body has the authority over the variance. The Board of County Commissioners has the authority over the subdivision. So we are doing this process first and then we will proceed with the subdivision process with the Board of County Commissioner.

So I am going to approach and I have some imagery to explain this and I'll be brief. It's pretty straightforward. And I apologize; it's overlaid on an aerial so it might be a little challenging to see. This is St. Francis Boulevard here. This is Rabbit Road. This is an almost 69-acre parcel. This is I-25 right here. So the I-25/St. Francis Boulevard interchange is right here.

CHAIR ANAYA: Hold on. The diagram that she has up is in our packet. It's NBF-17.

MS. JENKINS: Okay. Thank you. So as you come off St. Francis and take a right heading westbound on Rabbit Round you can enter the site. And you see this is the loop road of question this evening that serves the lots coming off of the loop road. So you can turn in here and depending on the nature of the future users we are proposing a

potential roundabout in this location as a traffic control in this area once that particular level of traffic control is warranted. And then there would be an exit here to either go westbound or eastbound.

At this first entrance, Commissioner Gonzales, there are right-in privileges here; there are right-out privileges here. But as you correctly pointed out, because of our proximity to St. Francis, there are no left-out privileges here. So if you want to head back to St. Francis you would exit the property at the eastern entrance. So this is really the primary entrance. This is really the front door of the project.

So what we have here on this top image here, this is the actual roadway section. So this shows in cross-section what we are proposing. And what we have here, this shows this is the right-of-way line, so this shows the proposed 70 feet of right-of-way here. We have a five-foot sidewalk. We have a five-foot landscaped area, two feet of curb and gutter. We have a five-foot bike lane. We have a 12-foot driving lane going in one direction. We have a 12-foot landscaped median in the center. The 12-foot landscaped median is actually not even a requirement for a minor arterial but we are doing it anyway for several reasons which I will explain.

So we're actually exceeding the standards in terms of what we're building on the ground to the requirements for a minor arterial. And then we have a mirror image on the other side. Twelve-foot driving lane, bike land, landscaped strip and then the five-foot sidewalk. So the key element that we have to address is vehicular circulation, pedestrian circulation and bicycle circulation, and this absolutely accomplished that. And as Vicki accurately stated, this is a private driveway serving one project. It's never going to be a four-lane road. It will never be warranted to be a four-lane road. So the 120-foot right-of-way becomes unnecessary at that point, hence the request for the variance.

But the key thing about accommodating cars and accommodating and supporting appropriate traffic flow, the key element to that is turning movements. That's why on Rabbit Road we are constructing a right turn decel lane here, so as cars want to turn right into this project they get out of the travel lane so they do not obstruct the flow of vehicles. There's a right turn decel lane here. There will be a right turn decel lane here in the initial phase. So it's about getting turning movements out of the travel lanes that keeps the flow of traffic moving appropriately.

So this is just kind of a blow-up of a section of the onsite roadway. So again, we have the landscaped median, the travel lanes, the bike lanes as you can see here, the landscaped strip and then the sidewalks on the outside. So let's say there is a user that needs a right turn decel lane. They have the entire frontage of their lot in which they could construct a right turn decel lane. If this is a driveway to a medical clinic, for example, say this is a driveway to a medical clinic and the County Public Works Department determines based upon the cars that are going to be visiting this clinic, we think it's appropriate that they build a right turn decel lane. So they will build that as part of their lot and the access easement in this area would be enlarged to accommodate that.

CHAIR ANAYA: Mr. Katz.

COMMISSIONER KATZ: That works really well if that medical clinic is going on the corner, but what happens if it's going five lots up?

MS. JENKINS: It's still –

COMMISSIONER KATZ: But they don't have the property to do that decel lane.

MS. JENKINS: Looking in here we have lots, lots, lots, lots, lots. So all of

this, as lots are being developed and acquired, there will be a due diligence process for each developer to determine what are their access requirements going to be? And if for example, every single one of these lots has frontage, and so that can be accommodated. And if there's one lot that the frontage is not quite appropriate, it's like, well, I'm going to have to come over here. So in terms of any lot development that happens, that has to happen in accordance with County rules and regulations. So if there's a user that's pursuing a particular lot and it is determined that a right turn decel lane is warranted, which I think in this situation is going to be fairly unlikely in the future, but if it is, that is accommodated on the lot frontages itself.

COMMISSIONER KATZ: There are several lots in the back that don't have frontage.

MS. JENKINS: Yes, that is correct. That is correct. So like I said, every single one of these users is not going to require that. There are going to be instances where it may be required so that will be done on a case-by-case basis as the lots are developed.

COMMISSIONER KATZ: Does the 70-foot right-of-way give enough room for somebody back in Lot 5 to get a decel lane?

MS. JENKINS: No.

COMMISSIONER KATZ: So how is that going to work, if somebody in Lot 5 needs the decel lane?

MS. JENKINS: Because then they won't buy Lot 5. They would acquire a lot that would accommodate those turning movements.

COMMISSIONER KATZ: Okay. Thank you.

MS. JENKINS: And then also with respect to the potential for accommodating right turns we also have the potential for accommodating left turns. So that is where the median that is being constructed – as you can see here, we have the landscaped median here and then we have the opportunity to construct a left turn lane in that median, because it is 12 feet wide. So that is constructed here and you would enter the driveway in this area.

So again, 120 feet of right-of-way is for a four-lane road. It is never going to be warranted in this particular application of a private – this is essentially a private driveway serving one project. And so what we are trying to convey here is that with all of these improvements we are accommodating pedestrians, cyclists and vehicles appropriately, and we also have opportunities to accommodate turn lanes, if and when those would be warranted. And so with that I would be happy to stand for any questions.

CHAIR ANAYA: Any questions from the board?

COMMISSIONER GONZALES: Mr. Chair.

CHAIR ANAYA: Mr. Gonzales.

COMMISSIONER GONZALES: When you addressed Mr. Katz, when you said that they wouldn't buy that particular lot, you're basing that on a need and they wouldn't need the frontage for that right turn in and the ones that are in the back, they're serviced with easement and if they bought it that would service their needs. Is that what you meant?

MS. JENKINS: Yes. And I think there's a two-fold response to that. Any acquisition of a parcel here by any future user – a medical clinic, an office building or whatever, there is going to be a due diligence process to determine: Does this lot meet my needs? Can I build what I want in compliance with County requirements? If this

particular lot, for whatever reason – maybe it's the terrain or the whatever or the access or this, doesn't meet my needs I would either pursue a different lot within St. Francis South, or I'd pursue a different piece of property. That's standard development practice. And so what I can tell you, like if you drive through Rodeo Business Park, there aren't any right turn decel lanes. There aren't any left turn lanes. It is atypical for those types of turn lanes to be warranted, because this is not a high traffic, where you have cars driving across town like you do on Rodeo Road.

What I was trying to convey is that should there be a circumstance as the property develops, we are in a position to accommodate turn lanes. Every single office building out here is not going to need a turn lane. It is just not a reality in terms of the amount of traffic that is generated. So what we are trying to convey here is that we can accommodate this should it be necessary in the future.

COMMISSIONER GONZALES: Thank you.

COMMISSIONER KATZ: Mr. Chair.

CHAIR ANAYA: Mr. Katz.

COMMISSIONER KATZ: What would it mean if you had to have a 120-foot right-of-way?

MS. JENKINS: It is giving up a lot of real estate that would sit vacant for a long time.

COMMISSIONER KATZ: It would stay vacant forever, wouldn't it?

MS. JENKINS: Yes. Thank you. It would basically be an extra 50 feet, so you would have an extra 25 feet from here all the way over, and you would have an extra 25 feet from here all the way over.

COMMISSIONER KATZ: How would that impact the usage of those lots?

MS. JENKINS: It would create a lot less usable area. It would limit the possibility for the street to be building dominated and to create that sense of when you drive in instead of buildings being shoved far back, we liked the idea of having a streetscape and having buildings relate to the street. And it also becomes a long-term maintenance issue. That's going to be off-lot so there be a lot owners association that is going to be maintaining the roadway, maintaining the improvements, but it becomes a no-man's land. You have 25 feet that nobody is taking ownership of that. In the future, I think in the long term, I don't think it's ideal in terms of how the project matures in maintaining something that is aesthetic and beautiful for this area.

COMMISSIONER KATZ: Is there anything that requires a decel lane?

MS. JENKINS: It's really on a case-by-case basis. It's based upon the number of cars, so we would come in and say I've got this project. We're looking at traffic generation of this. This is how many cars are currently traveling on this loop road and then the Public Works Department, Johnny Baca or Paul Kavanaugh would make a determination. Like, well, the thing is decel lanes are often a function of background traffic. It's like how many cars on a daily basis are going back and forth on this loop road.

So if you're concerned about – and this is going to be a very low speed roadway. It's probably going to be 25, 30 miles an hour. The higher speed the road, the more decel lanes are required. Because if cars are going fast and all of a sudden somebody is slowing down and stopping to turn, that creates a traffic hazard. Right? So we have several things in our favor with respect to the need for potential turn lanes. We have a low speed,

private roadway that is not delivering cars from one part of town to another. It is only delivering cars internally to these users. And like I said, those are the elements that really reduce the need for these types. But should something occur where the Public Works Department determines that it's warranted we have opportunities to comply with that.

COMMISSIONER KATZ: I guess my concern is clearly you don't need 120 feet. You might need 12 additional feet where there could be a decel lane. And would you be willing to plan for that? To make it 70 feet for most of it but where there would be basically, judging from your lot structure you know there are only four or five lots that are in back that you would need to have in those places for whatever the length of a decel lane is and I'm sure Public Works –

MS. JENKINS: I'm maybe thinking where we have situations where there might be access points to do that. So I totally get what you're saying and one thing I would like to add and maybe we could craft something around that. We are showing a 22-lot subdivision here, but we know as users come in, somebody's going to say, well, I want all four of these lots. I'm putting my building – I need all four of them. So where we have multiple lots end up maybe as one lot. So I think we could definitely craft something that would require the provision as we move through the phasing of the subdivision platting that could require the provision for additional right-of-way to accommodate right turn decel lanes in certain areas where we had shared access serving multiple lots. So I think there might be an opportunity to craft something that would accommodate that.

COMMISSIONER KATZ: When would we craft that? Would we craft that now?

MS. JENKINS: We could craft that now as a condition and then that would sort of travel with the project. And then as we were moving through subdivision processes we'd have the opportunity – let's say we ended up needing some additional right-of-way to potentially accommodate a right turn decel lane here, and then let's say we ended up consolidating all those lots for a larger building, then we would have the opportunity to vacate that additional right-of-way if it ended up becoming unwarranted or unnecessary. So I think we could definitely craft something along those lines.

COMMISSIONER KATZ: Thank you. Twelve additional right-of-way for the length that Public Works would require for a decel lane in those areas where there are back lots. Thank you.

CHAIR ANAYA: This is a public meeting and if we have anybody from the public that's willing to stand up and speak on this particular issue of the widening of the turn lanes then we're happy to hear those statements.

[Those wishing to speak were placed under oath.]

[Duly sworn, Richard Ratto testified as follows:]

RICHARD RATTO: Good evening, Chairman Anaya and Commissioners. My name is Richard Ratto. I'm the president of the Campo Conejos Homeowners Association. We're on Rabbit Road, a 75-lot subdivision. We're a registered organization as well. We attended a March 21st meeting regarding this variance before it came before you today and our recommendation was to deny the variance. I think I heard from the discussions by the applicant and the questions some hesitation on your part. I'm an engineer registered in the state of New Mexico, both civil and structural and there are concerns I hear from being an engineer and even those of you who are not engineers, you can kind of pick up on this. There are some things that just don't make sense.

This is the beginning of the project. Nothing's been built yet but there's already concessions being made, so to speak, with the variance request. I think what they're asking for is a variance because of something written into the code based upon the volume of traffic. So Commissioners, I'd like you to consider that. It was described before you tonight as a driveway. I think even in common terms everybody understands what a driveway is. This is not a driveway. It's going to require signalization or a roundabout on St. Francis Drive in its full build-out and as you know, planning right-of-way, you need to consider the ultimate condition of the project.

The code requires 120 feet and we understood that's for a four-lane road. Well, if you take off two of those lanes you can subtract 24 feet from 120 feet, so if we're going with the intent of the code but you're giving a variance to a full build-out a two-lane road instead of a four-lane then basically, to be consistent with the code you should only be reducing it by 24 feet.

I was not provided the opportunity to see the typical section but it sounds like this is as concise a section as they can possibly build. In other words, an urban section, not a rural section. No ditches but rather curb and gutter. Anytime you build sidewalk you'd want to have curb and gutter so in that scenario, medians are typically 16 feet, not 12 feet to allow the curb and gutter in the median to be constructed and not interfere. If you have a 12-foot median you're only limited to about nine to ten feet in your turning lane. So that's just insufficient.

On their calculation, the agenda states 65 to 70 feet is what they're requesting. That only leaves a foot of extra land between the back of the sidewalk and the property line. That's insufficient even for dead flat conditions. But this is very – there's some nice terrain out there, sloping terrain so you're going to need some room for grading. There was a very good point by Commissioner Katz that pointed out the turning lane. I don't think you want to give up that turning lane right-of-way. So the turn lanes need to be factored in.

So I think what they're asking for, even at 70 feet is entirely too narrow. You've been presented a representation of a streetscape that's not fitting in the county at this location and if you go look on Rabbit Road there's nothing that would look like this at all, even over in the College District itself. So it's already out of place. We've been following this project for some time and we're concerned about it. And on this particular variance I'd ask that you stick with the code. I concur that four lanes is excessive for this location but what they're asking for is inappropriate. I would say that you look at the 120 minus 24 as a starting place and maybe negotiate from there. Thank you.

CHAIR ANAYA: Any questions? Thank you. Next.

[Previously sworn, Eve Cohen testified as follows:]

EVE COHEN: My name is Eve Cohen. I'm a resident in Campo Conejos, which is east of the proposed development. And I think, as I'm not an engineer but I do live in the county and have appreciated this property being in its natural state for some time and I understand that there are interests in developing it, but I think our concerns as residents are traffic and density. And trying to create more buildable area in this development is only going to increase the amount of traffic that's already existing on Rabbit Road. It's a major east-west connection now to avoid going through city streets and I think that this is not going to serve this concern as well as having an urban cityscape with buildings immediately adjacent to a sidewalk is certainly not in keeping with the nature of the county. And so I think that having a wider easement or right-of-

way certainly the Rodeo Park Business Center has been cited but there are much bigger setbacks there and you don't have this immediate proximity to the very edge of the sidewalk. So I think those are major concerns for us as residents. Thank you.

CHAIR ANAYA: Thank you. Next.

[Previously sworn, Peggy Fino testified as follows:]

PEGGY FINO: Hi. I'm Peggy Fino. I'm also a resident of Campo Conejos and I've sat on the ACC board for the past 15 years. Our main concern is safety and of course aesthetic reasons too. If they stay with the 120 feet it will appear as dense and it will certainly be a lot safer. It will create hundreds and hundreds more vehicles and I have lived there a long time, at least 15 years in that area and we have seen the increase since they opened Rabbit Road in the past few years and that road is deteriorating rapidly and of course it will be worse once there's a lot more cars on there. So I really would like to see you stick to your original plan, what the regulation is of 120 feet and thank you.

CHAIR ANAYA: Thank you, ma'am.

[Previously sworn, John Singleton testified as follows:]

JOHN SINGLETON: My name is John Singleton. I'm a resident of Campo Conejos for the last ten years. And my concern is similar to those –

CHAIR ANAYA: Sir, let me ask you one question, because you're the third one that has come up and said that you all are part of the ACC. Where is that?

MR. SINGLETON: The Architectural Control Commission of our homeowners association.

CHAIR ANAYA: And how far are you from the project?

MR. SINGLETON: We're about a mile to the east of the project. Almost exactly a mile along Rabbit Road.

CHAIR ANAYA: Along Rabbit Road.

MR. SINGLETON: The entrance to our development is on Rabbit Road exactly one mile east of the intersection between St. Francis and Rabbit Road.

CHAIR ANAYA: Okay. Thank you.

MR. SINGLETON: And our concern is mostly about traffic on Rabbit Road and also noise, although that I think is a more minor concern. If there's the possibility of a freight depot in this development with large trucks coming in and out day and night using Rabbit Road as access to I-25, that's not limited as far as I'm concerned by anything I've heard so far. So we're concerned about traffic on Rabbit Road and how it will impact our development.

CHAIR ANAYA: Thank you, sir. Anyone else from the public. Having seen none the public hearing is closed. Let's go back to Jennifer.

MS. JENKINS: If I may, Chair and Commissioners, I have a condition that I would like to read for your consideration, stating – based on the conversation that we had with you and with Commissioner Katz I have crafted a condition for your consideration: The developer shall provide additional right-of-way on the interior loop road to accommodate right-turn decel lanes if required by the County Public Works Department. So I would be happy to hand this to you.

And in conclusion, you know, we worked very closely with the County Public Works Department on this request. They analyzed our plans very, very closely and have determined that 70 feet is more than adequate to accommodate the roadway section that is being built on the ground in accordance with the minor arterial standards and we are building a median, that is not even a requirement of the code, that will create a more

beautiful environment as well as accommodating potential left-turn lanes. And, again, this is a private road that is serving one project. Note, everybody agrees this will never be a four-lane road. That one element of this is not in question. So we are asking for the opportunity for an appropriate width of right-of-way as demonstrated on our section that has been approved by the Public Works Department and we really appreciate staff's time on this and we appreciate staff's recommendation for approval. It may be my first recommendation for approval of a variance so I'm very happy about it I have to say. I am happy to stand for any additional questions. Thank you very much for your time and attention.

CHAIR ANAYA: Does staff have any more questions for the owners?

Sir, yes.

COMMISSIONER KATZ: I have a question for the attorney. You heard the concern that they don't know who's going to buy this. All they want to do is sell them and they'll sell them to anybody who will buy them. And in order to make sure that if somebody who buys one of those back lots will not cause difficulty if it's the kind of business that will have sufficient traffic to merit the decel lane, is the language that Jennifer has suggested provide the County the protection to make sure that that additional right-of-way will be there? Do you want me to read what she said? "Developers will provide additional right-of-way on the interior loop road to accommodate right-turn decel lanes if required by the County Public Works Department."

MS. BROWN: Mr. Chair, Committee Member Katz, I don't know whether – well certainly delegating to Public Works the decision that additional right-of-way would be required is one method of addressing that concern. I understand that that would not be the median but additional right-of-way beyond the roadway that has already been proposed. So whether or not that is an appropriate modification to justify the variance is not my place to say but granting Public Works the authority to require that as proposed development comes forward and needs a permit would certainly give an opportunity if that right-of-way is still owned by the developer and hasn't already been sold to a lot owner.

COMMISSIONER KATZ: That is a concern. Okay, thank you.

MS. JENKINS: And if I may offer on that point. I had the exact same thought that Ms. Brown has stated here. And, so, the good thing about this process coming first before we proceed with the subdivision is that we have the opportunity to incorporate this and so there's going to be appropriate setback along all of these lot frontages and so we can incorporate this mandate into the subdivision plat. So that is something that is easily done and that would make this requirement sort of, you know, run with the land and so basically, sort of, setting it up that in those kind of, you know, requisite setback areas anyway. So this is easily accomplished, you know, with the subdivision platting process that we are engaged in right now. And we'd be happy – maybe the best way to craft this is that we would work with Land Use staff and Public Works staff to address this particular element and craft proper notification, you know, on the subdivision plat. So, it's easily handled. And in a coordinated master plan development like this it is actually not unusual at all that these types of contingencies would be addressed in the project.

MS. BROWN: Mr. Chair, if I may?

CHAIR ANAYA: Yes, ma'am.

MS. BROWN: One option for assessing whether the proposed language

would be sufficient is to have staff consider that language and evaluate that and bring it back at your next meeting.

COMMISSIONER KATZ: Mr. Chair.

CHAIR ANAYA: Mr. Katz.

COMMISSIONER KATZ: I think that they're trying to get this done and I think my preference would be to remove the last "if required" and just simply provide that they – it could be 70 feet or whatever we agree on for the most of it but for the six areas where there is an access easement off to the right that it would be 82 feet for those areas.

CHAIR ANAYA: Any other questions? Having none, what would be the pleasure of the Board? Mr. Gonzales – I mean Lopez.

COMMISSIONER LOPEZ: I recommend approval of Case V 17-5030, St. Francis South variance with staff recommendation.

CHAIR ANAYA: Would you like to add anything else to the recommendations or with the condition as discussed? It's up to you.

COMMISSIONER LOPEZ: Yes, with staff recommendation as specified.

COMMISSIONER GONZALES: I'd like to second it with Member Katz's conditions.

MS. LUCERO: Mr. Chair, I'm sorry. If I could just get clarification, is that with adoption of the Hearing Officer's findings of fact and conclusions of law?

CHAIR ANAYA: That's what we're discussing. We have a first from Mr. Lopez with staff conditions and then we have a second with Mr. Gonzales with Mr. Katz's recommendations.

COMMISSIONER KATZ: And may I express those?

CHAIR ANAYA: Please.

COMMISSIONER KATZ: Which would be that the right-of-way be 82 feet wide in the areas where there is a access easement for a right-turn and there are six of them according to the plat for the length that the Public Works Department – and I'm not sure what the length would be and that's something that would be the lengths that the Public Works Department would require.

CHAIR ANAYA: Mr. Lopez, we have a friendly amendment and if you would like to make that into your motion, you may at this time.

COMMISSIONER LOPEZ: Okay.

COMMISSIONER KATZ: It would be 12 additional feet. If I can clarify that where there is a access easement that would have a right-turn that there be 12 additional feet because there actually are some that are right opposite each other so it wouldn't be 82 feet there but it would be larger – but where there is a right-turn there is an additional 12 feet for a decel lane for each of those right-turns and there are six of them on the plat now and that could change. But it would take care of the right-turn decel lanes.

CHAIR ANAYA: Ms. Lucero, do you have all of that in the motion?

MS. LUCERO: Mr. Chair, I believe we have the recommended conditions but in addition to that are the Hearing Officer's recommendations and conclusions also incorporated into the motion?

CHAIR ANAYA: Absolutely.

MS. BROWN: Mr. Chair, if you can clarify that that maker of the motion has accepted the friendly amendments.

COMMISSIONER LOPEZ: You can include that also with the motion.

CHAIR ANAYA: All right. We have a motion for approval with friendly amendment and also approval of staff recommendation and do we have a second?

COMMISSIONER GONZALES: Well, I'd already seconded it so it would just be the condition and that's okay.

MS. BROWN: Have you completed your vote? I need a point of clarification. I'm sorry.

CHAIR ANAYA: Please.

MS. BROWN: I heard you say that you're moving to approve with staff recommendations and the additional condition of Mr. Katz. I didn't hear the adoption of Hearing --

CHAIR ANAYA: Hearing Officer, I consider that as staff. I'm sorry, and the Hearing Officer, I'm sorry, I thought that was considered as staff. I'll make that correction mentally. Thank you

So we have a first and a second.

The motion passed without opposition.

G. Possible Action on the Findings of Fact and Conclusions of Law for Case #V 17-5030, St. Francis South Road Standards

MS. LUCERO: Mr. Chair, and I could just clarify, we did have as item G, the adoption of the findings of fact and conclusions of law, so as in the other case, since we made changes to the conditions, staff will take this and revise it and bring it back to the Planning Commission next month.

CHAIR ANAYA: Okay, and we have to just approve those at the next meeting then.

MS. LUCERO: Yes. Correct.

H. Petitions from the Floor

None were presented.

I. Communications from Commission Members

Commissioner Martin announced she would be unable to attend the next meeting.

J. Communications from the Attorney

None were presented.

K. Next Planning Commission Meeting: June 15, 2016

Penny Ellis-Green, Director
Growth Management Department/Land Use
Santa Fe County
102 Grant Avenue, Santa Fe, New Mexico 87501

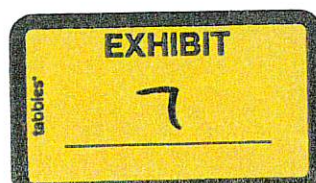
Dear Ms. Ellis-Green:

I am President of the Santiago Subdivision Road User Association. My neighbors and I on Rabbit Road/Entrada de Santiago and other nearby communities are very concerned by the proposed high density Large Scale Mixed-Use development at the entrance to our neighborhood at South Francis and Rabbit Road (CASE # 10-5364 St. Francis South). This proposed development will inalterably change the nature of our neighborhood and will inevitably be accompanied by numerous negative impacts. In particular, we are concerned by the following:

- 1) Traffic on Rabbit Road will increase substantially. It has already increased from 500 cars a day to over 6000 since the opening of the extension of Rabbit Road to Richards Avenue via Oshara Village some nine years ago. This has created real safety risks at several intersections including South St Francis and Rabbit Road and Rabbit Road and Entrada de Santiago;
- 2) The construction of the proposed office buildings (150,000 sq ft), warehouses (300,000 sq ft) and a rehabilitation facility (50,000 sq ft) is completely out of keeping with our rural residential area with single family homes on an average of 2.5 acre lots;
- 3) This type of commercial development will likely negatively impact property values in the surrounding residential areas and considerably increase traffic noise and nighttime light pollution.

We continue to be concerned by the lack of detail concerning specific development plans of lots proposed for BCC approval.

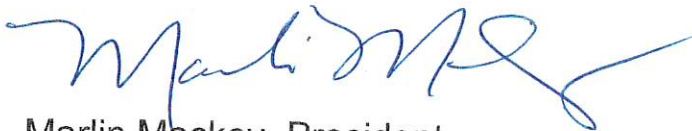
However, we were heartened by the November 10, 2015 decision of the Board of County Commissioners to deny the variance request to increase the residential zoning density of the development beyond the present county code restrictions. We thank the BCC for their sensitivity to our neighbors' concerns expressed at that meeting.



We, the undersigned, request that the property be developed as residential single family lots as are all the surrounding properties, that community initiated traffic calming structures be in place before any further development is permitted and that full disclosure of specific development plans be provided.

We hope that the county will pause in the approval process to allow the wishes and concerns of all the neighbors of this development to be clearly stated and taken into account.

Respectfully Submitted,

A handwritten signature in blue ink, appearing to read 'Marlin Mackey', with a long horizontal flourish extending to the right.

Marlin Mackey, President

Santiago Road Users Association

5 Calle Jacinta

Santa Fe, N.M. 87508

6-12-2017

and attached signers (with addresses)

Santiago Road User Association Homeowners

James M Pauls 49 ENTRADA DE SANTIAGO

Name

Address

James Pauls 49 Entrada de Santiago

Name

Address

Bill Beasly 39 Entrada de Santiago

Name

Address

Mary Beasly 39 Entrada de Santiago

Name

Address

Karin B. Hall 8 Calle Jacinta

Name

Address

Jane Mitchell 14 Calle Jacinta

Name

Address

Mitchell 14 Calle Jacinta

Name

Address

Sandra May 5 Calle Jacinta

Name

Address

Santiago Road User Association Homeowners

Douglas Brenner J.D. 15 Entrada de Santiago, Santa Fe, N.M.
Name Address

Sam SHAW 5 TRAVIESA de Camilo, Santa Fe, NM
Name Address

Eckhardt Koch 13 Traviesa De Camilo E, Santa Fe, NM
Name Address

Tutor P Baca 06 E. TRAVIESA De Camilo S.F. NM
Name Address

Nari R Baca 06 E Traviesa de Camilo S.F. N
Name Address

Frank Ayth 26 Entrada de Santiago
Name Address

Irene Angel 26 Estrada de Santiago
Name Address

Angel 27. Entrada De Santiago
Name Address

Santiago Road User Association Homeowners

John J. Redd 12 Traviesa de Camilo E
Name Address

Penelope Redd Same
Name Address

Name Address

Name Address

Name Address

Name Address

Name Address

Name Address

Santiago Road User Association Homeowners

Name Phy Brenner Address 196 Rabbit Rd

Name Eddie Gil Address 33 Calle Jacinta

Name Ramiro Vigil Address 33 Calle Jazmin

Name	Address
Sandra Machy for (as per the June 12, 2017 e-mail to Marilyn Machy, attached)	
Andrew Drom / Barbara Lerner	13 Calle Jacinto

[illegible]

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Name	Address
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From: Barbara Larsen ba_larsen@msn.com
Subject: Re: Tuesday County meeting re. new development project
Date: Yesterday at 3:40 PM
To: Marlin Mackey marlin.mackey@gmail.com

Hi Marlin,

Andy and I are both supportive of your letter. please sign on our behalf.

Thanks so much.

Barbara

From: Marlin Mackey <marlin.mackey@gmail.com>

Sent: 12 June 2017 04:23

To: Chris and Jane Mitchell; Jim and Susan Parks; Bernadette Redd; Jay and Barbara Clack; John Redd; Karin B. Hall; Ekkehard and Melanie Koch; Frank and Irene Angel; Sam and Liz Shaw; Eddie and Rosalie Vigil; Mark and Vanessa Angel; Bill and Mary Beacham; Douglas Brenner; Rudy and Marcela Gonzalez; Victor and Navi Baca; Andrew Drom and Barbara Larsen; Jonathan Gonzales

Subject: Tuesday County meeting re. new development project

This Tuesday the Santa Fe County Commission will be meeting at the downtown County office to review the proposed multi-unit development at St. Frances and Rabbit Road. Many of us have attended several meetings in the past to review and comment on this high volume development and its impact on the area including Rabbit Road traffic.

Attached is a letter to the County that I will deliver on Tuesday that outlines several concerns. If you would like to sign please let me know and I will bring by your house tomorrow or Tuesday. The meeting starts at 5:00 and has a short agenda.

Several of our neighbor associations will also be delivering a similar letter and will have members attending the meeting. These include:

1. Old Galisteo Way
2. Camp Conejo
3. Rabbit Road

302C Rabbit Rd
Santa Fe, NM 87508

June 12, 2017

Penny Ellis-Green, Director
Growth Management Department Land Use
Santa Fe County
102 Grant Avenue
Santa Fe, NM 87501

Dear Ms. Ellis-Green,

Please accept this letter on behalf of the Tapia family members, residing on Rabbit Road, Los Tapias Lane, and Old Galisteo Way.

We are extremely concerned with the proposed high-density Large Scale Mixed-Use development at the entrance to our family community at South St. Francis Drive and Rabbit Road (CASE# 10-5364 St. Francis South). We concur with our neighbors in the Old Galisteo Way Road Association that this development will inalterably change the nature of our family community and will inevitably be accompanied by numerous negative impacts. We offer the following concerns:

1. Many of our driveways are on Rabbit Road. Already traffic has increased from 500 cars a day to over 6000 since the opening of the extension of Rabbit Road to Richards avenue via Oshara Village. This unimagined increase of traffic poses a daily safety risk as we attempt to either exit or enter our driveway. Often, drivers are exceeding the posted "40 miles per hour" speed limit by 10 to 15 miles per hour. In many instances, when we are attempting to enter our driveway, drivers ignore our turning signal and often pass us on the shoulder of the road, or in some instances on the side that we are turning;
2. The construction of the proposed office buildings (150,000 sq ft), warehouses (300,000 sq ft) and a rehabilitation facility (50,000 sq ft) is completely out of keeping with our rural residential area with single family homes on an average of 2.5 acre lots;
3. This type of commercial development will likely negatively affect property values in the surrounding residential areas and considerably increase traffic noise and nighttime light pollution.

We, too, are concerned regarding the lack of detail concerning the specific development plans of lots proposed for BCC approval.

Our family patriarch, Filiberto Tapia, envisioned a family compound where his descendants would continue to live and nurture the land that he purchased. The current county code restrictions regarding lot sizes have not allowed the delegation of inheritance for this purpose.

We request that the Board of County Commissioners continue to deny the variance request to increase the residential zoning density of the development beyond the present county code restrictions.

We also support our neighbors' request that community initiated traffic calming structures be in place before any further development is permitted and that full disclosure of specific development plans be provided.

As native Santa Feans, we recognize that progress is inevitable, Santa Fe will continue to grow, and our small hamlet will be affected. However, we request that the Board of County Commissioners pause in the approval process and allow our concerns and wishes to be taken into account.

Sincerely,

Terri Tapia

Terri Tapia

Tapia Family Members in Agreement:

Signature:

Frank C. Buntamant

Frank C. Buntamant

Terri Tapia

Kenneth Tapia

Jessie Tapia

Joleen Tapia

Shirley Tapia

Mrs. & Mrs. Sharon Peña

Mary Ann Tapia

Mrs. & Mrs. Joe Larrazola

Mari MacAllister

Pick MacAllister

Gloria Tapia

Jackie Tapia

Mark Tapia

Felicit A Tapia

Address:

286 RABBIT RD SF 87505

286 Rabbit Rd. Santa Fe 87505

284 A RABBIT RD.

284 B. Rabbit Rd.

21-B Los Tapias Ln.

21-A-Los Tapias LN 87508

21 AB Los Tapias Ln 87508

21 AB Los Tapias Lane 87508

21-B Los Tapias LN. 87508

37 Los Tapias Ln - 87508

42 A Los Tapia LN

42 A Los Tapia N

45 A Los Tapia 87508

43 LOS TAPIAS LN 87508

302 B Rabbit Rd Santa Fe NM

#43 LOS TAPIA LAKE SANTA FE NM.

CAMPO CONEJOS HOMEOWNERS' ASSOCIATION

PO BOX 6759, Santa Fe, N.M. 87502

June 12, 2017

Penny Ellis-Green, Director
Growth Management Department/ Land Use
Santa Fe County
102 Grant Avenue Santa Fe, NM 87501

Dear Ms. Ellis-Green,

I am President of the Campo Conejos Home Owner's Association. My neighbors and I in Campo Conejos and other nearby communities are very concerned by the proposed high density Large Scale Mixed-Use development at the entrance to our neighborhood at South Francis and Rabbit Road (CASE # 10-5364 St. Francis South). This proposed development will inalterably change the nature of our neighborhood and will inevitably be accompanied by numerous negative impacts. In particular, we are concerned by the following:

1. Traffic on Rabbit Road will increase substantially. It has already increased from 500 cars a day to over 6000 since the opening of the extension of Rabbit Road to Richards Avenue via Oshara Village some nine years ago. This has created real safety risks at several intersections including South St. Francis and Rabbit Road and Rabbit Road and Old Galisteo Road;
2. The construction of the proposed office buildings (150,000 sq. ft), warehouses (300,000 sq. ft) and a rehabilitation facility (50,000 sq. ft) is completely out of keeping with our rural residential area with single family homes on an average of 2.5 acre lots;
3. This type of commercial development will likely negatively impact property values in the surrounding residential areas and considerably increase traffic noise and nighttime light pollution.

We continue to be concerned by the lack of detail concerning specific development plans of lots proposed for BCC approval.

However, we were heartened by the November 10, 2015 decision of the Board of County Commissioners to deny the variance request to increase the residential zoning density of the development beyond the present county code restrictions. We thank the BCC for their sensitivity to our neighbors' concerns expressed at that meeting.

(over, please)

6.12.17

Penny Ellis-Green, Director
Growth Management Department/ Land Use
Santa Fe County
102 Grant Avenue Santa Fe, NM 87501

June 12, 2017

Dear Ms. Ellis-Green,

My wife and I are very concerned about the proposed development at St. Francis and Rabbit Road - CASE # 10-5364 St. Francis South.

This scale and intent of use of this proposal will greatly affect the character of our neighborhood. It seems to me that more specific information about the ultimate use of this property is essential before this project continues. Additionally, with a proposal of this size, and with such a certain negative impact on the surrounding neighborhood, a traffic impact report is very important. I definitely feel that further study needs to be undertaken before continuing the permitting process.

This proposed development will inalterably change the nature of our neighborhood and will inevitably be accompanied by numerous negative impacts. In particular, we are concerned by the following:

- 1) Traffic on Rabbit Road will increase substantially. It has already increased from 500 cars a day to over 6000 since the opening of the extension of Rabbit Road to Richards Avenue via Oshara Village some nine years ago. This has created real safety risks at several intersections including South St Francis and Rabbit Road and Rabbit Road and Old Galisteo Road;
- 2) The construction of the proposed office buildings (150,000 sq ft), warehouses (300,000 sq ft) and a rehabilitation facility (50,000 sq ft) is completely out of keeping with our rural residential area with single family homes on an average of 2.5 acre lots;
- 3) This type of commercial development will likely negatively impact property values in the surrounding residential areas and considerably increase traffic noise and nighttime light pollution.

Lynn Robbie LYNN ROBBIE 82 OLD GALISTEO WAY
David Hering DAVID HERING 82 OLD GALISTEO WAY

June 12, 2017

Ms. Penny Ellis-Green, Director
Growth Management Department/Land Use
Santa Fe County
102 Grant Avenue
Santa Fe, New Mexico 87501

Dear Ms. Ellis-Green:

I am President of the Old Galisteo Way Road Association. My neighbors and I on Old Galisteo Way and other nearby communities are very concerned by the proposed high density Large Scale Mixed-Use development at the entrance to our neighborhood at South St. Francis and Rabbit Road (Case # 10-5364 St. Francis South). This proposed development will inalterably change the nature of our neighborhood and will inevitably be accompanied by numerous negative impacts. In particular, we are concerned by the following:

- 1) Traffic on Rabbit Road will increase substantially. It has already increased from 500 cars a day to over 6000 since the opening of the extension of Rabbit Road to Richards Avenue via Oshara Village nine years ago. This has created real safety risks at several intersections including South St. Francis and Rabbit Road and Rabbit Road and Old Galisteo Road;
- 2) The construction of the proposed office buildings (1500 sq.ft.), warehouses (300,000 sq.ft.) and a rehabilitation facility (50,000 sq.ft.) is completely out of keeping with our rural residential area of single family homes on an average of 2.5 acre lots;
- 3) This type of commercial development will likely negatively impact property values in the surrounding residential areas and considerably increase traffic noise and nighttime light pollution.

We continue to be concerned by the lack of detail concerning specific development plans of the lots proposed for BCC approval.

However, we were heartened by the November 10, 2015 decision of the BCC to deny the variance request to increase the residential zoning density beyond the present county code.

We, the undersigned, request that the property be developed as residential single family as are all the surrounding properties, that community initiated traffic

**Ian and Lois Alsop
Vasundhara Alsop
38 Old Galisteo Way
Santa Fe NM 87508**

Penny Ellis-Green, Director
Growth Management Department/ Land Use
Santa Fe County
102 Grant Avenue Santa Fe, NM 87501

June 12, 2017

Dear Ms. Ellis-Green,

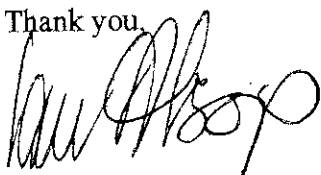
We are opposed to the proposed high density Large Scale Mixed-Use development at South St. Francis Drive and Rabbit Road (CASE # 10-5364 St. Francis South). Unfortunately, we only heard about this development project via the articles appearing in August of 2016 in the Santa Fe New Mexican and the Albuquerque Journal North. Had we heard about it earlier we would have registered our opposition earlier. Many who were there spoke out against it.

This proposed development, at the entrance from town into our neighborhood, will inalterably change the nature of our neighborhood and affect the safety of our commute. Traffic on Rabbit Road will increase substantially (it has already increased more than tenfold since 2008 when Rabbit Road was extended without any substantial road improvements). There are real traffic hazards, especially at the intersection of Rabbit Road and Old Galisteo Road..

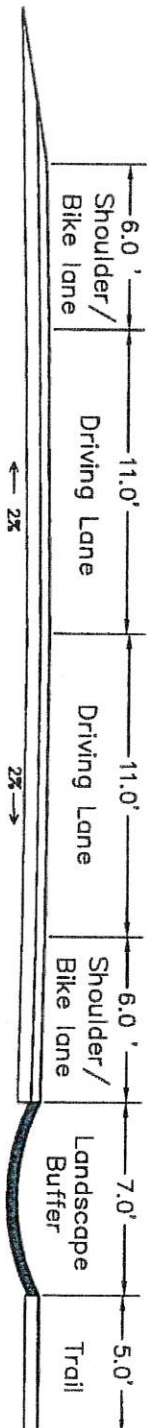
The Large-Scale mixed-use construction of office spaces and warehouses is completely out of keeping with the surrounding residential neighborhoods. There is plenty of that just before I -25 at Sawmill and Zia Roads. Since the present developers are only planning the permissions and lot divisions for sale, there is in fact no plan of what will eventually be built on these lots, and how long construction will be going on. We are concerned that this type of development will have a negative impact on the value of our property and that of our neighbors. We will lose the character of our Santa Fe County lifestyle and certainly the "City Different" will be less different.

It is our hope that the BCC will table this proposal of final plat approval for the first two phases of this project in order to allow the wishes and concerns of all the neighbors of this development to be clearly stated and taken into account.

Thank you.



Rabbit Road Typical Section



4" Hot Mix Asphalt
 6" Basecourse
 2% Crown required in subgrade, basecourse and asphalt

General Notes



No.	Revisions	Date

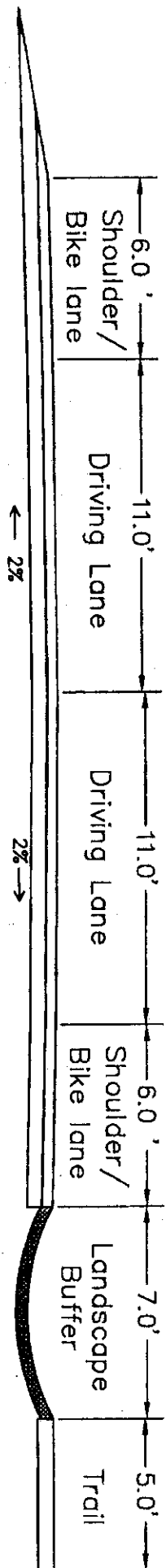
Rabbit Road
 Typical Section

Project Rabbit Road	Date 6/30/17
Scale NTS	1

EXHIBIT

8

Rabbit Road Typical Section



4" Hot Mix Asphalt

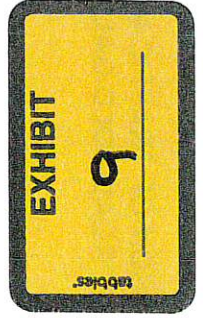
6" Basecourse

2% Crown required in subgrade, basecourse and asphalt

COMPARISON OF TRAFFIC VOLUMES INTERSECTION OF ST. FRANCIS DRIVE / RABBIT ROAD

St. Francis Drive / Rabbit Road - AM Peak Hour											
	Eastbound			Westbound			Northbound			Southbound	
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Right
Traffic Count 2010	347	46	0	0	48	26	NC	NC	NC	17	175
Traffic Count 2013	349	34	0	0	53	16	NC	NC	NC	18	206
Percent Change between 2010 and 2013	0.6%	-26.1%			10.4%	-38.5%				5.9%	17.7%
Traffic Count 2015	342	36	0	0	47	22	0	0	0	19	203
Percent Change between 2013 and 2015	-2.0%	5.9%			-11.3%	37.5%				5.6%	-1.5%
Percent Change between 2010 and 2015	-1.4%	-21.7%			-2.1%	-15.4%				11.8%	16.0%

St. Francis Drive / Rabbit Road - PM Peak Hour											
	Eastbound			Westbound			Northbound			Southbound	
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Right
Traffic Count 2010	154	32	0	0	35	20	NC	NC	NC	32	292
Traffic Count 2013	164	24	0	0	53	21	NC	NC	NC	33	319
Percent Change between 2010 and 2013	6.5%	-25.0%			51.4%	5.0%				3.1%	9.2%
Traffic Count 2015	145	18	0	0	54	20	0	0	0	30	341
Percent Change between 2013 and 2015	-11.6%	-25.0%			1.9%	-4.8%				-9.1%	6.9%
Percent Change between 2010 and 2015	-5.8%	-43.8%			54.3%	0.0%				-6.3%	16.8%

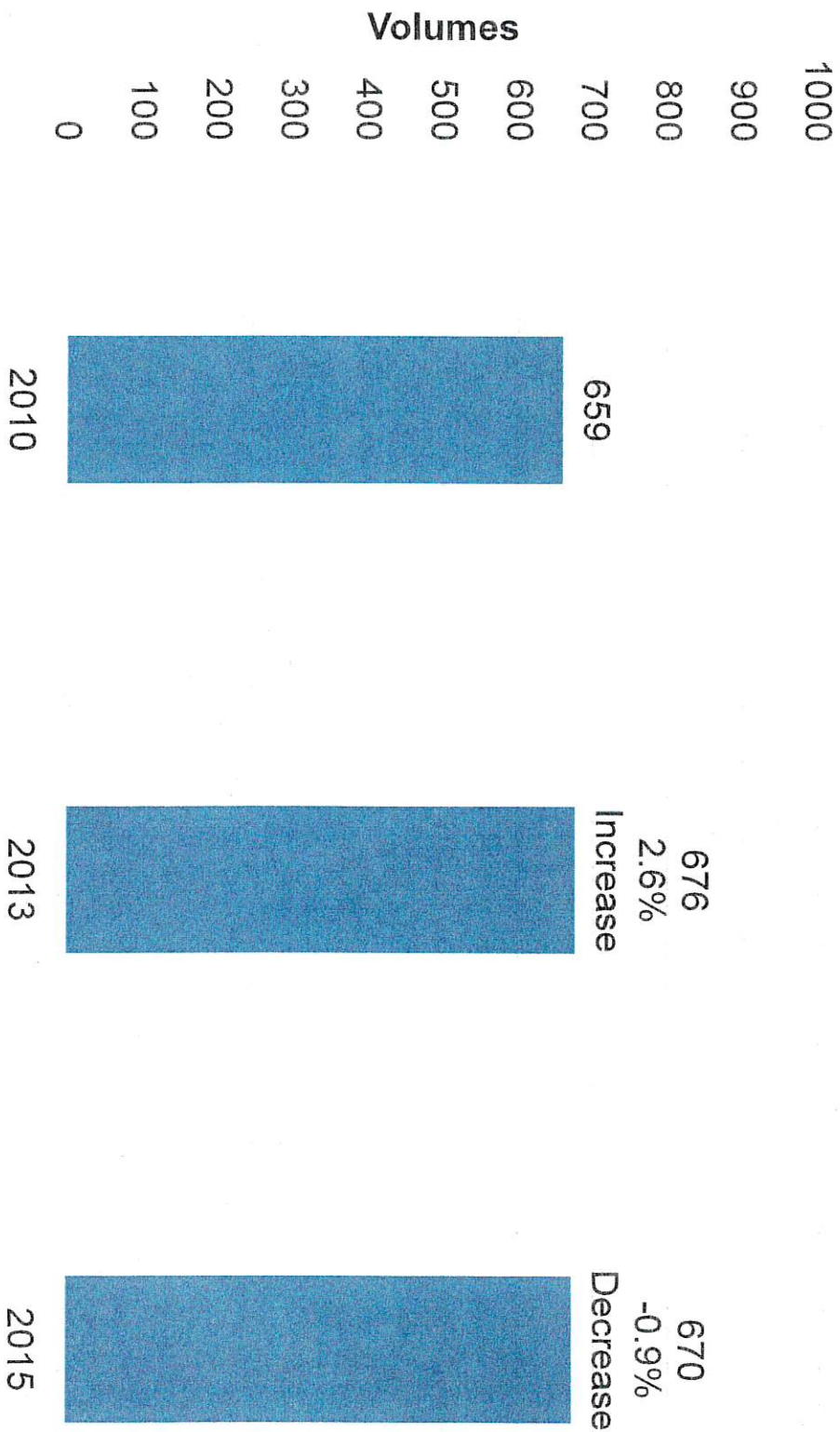


COMPARISON OF TRAFFIC VOLUMES INTERSECTION OF RABBIT ROAD / OLD GALISTEO ROAD / DRIVEWAY

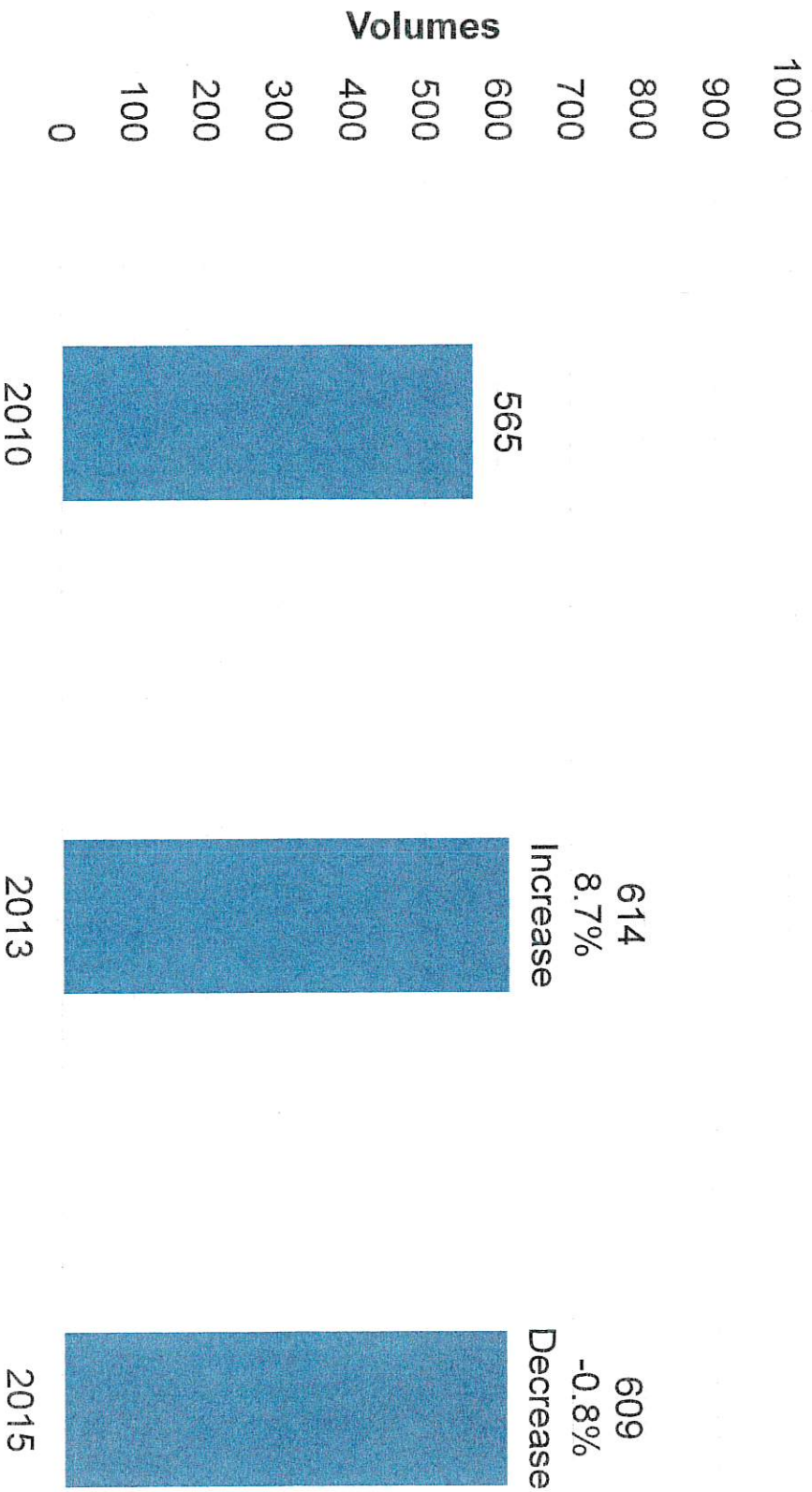
Rabbit Road / Old Galisteo Road / Driveway - AM Peak Hour												
	Eastbound			Westbound			Northbound			Southbound		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Traffic Count 2010	0	255	5	43	192	0	13	0	132	0	0	0
Traffic Count 2015	0	294	3	27	226	0	9	0	91	0	0	0
Percent Change between 2010 and 2013		15.3%	-40.0%	-37.2%	17.7%		-30.8%		-31.1%			

Rabbit Road / Old Galisteo Road / Driveway - PM Peak Hour												
	Eastbound			Westbound			Northbound			Southbound		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Traffic Count 2010	0	126	12	94	234	0	10	0	56	0	0	0
Traffic Count 2015	0	167	6	83	285	1	4	0	38	1	0	0
Percent Change between 2010 and 2013		32.5%	-50.0%	-11.7%	21.8%		-60.0%		-32.1%			

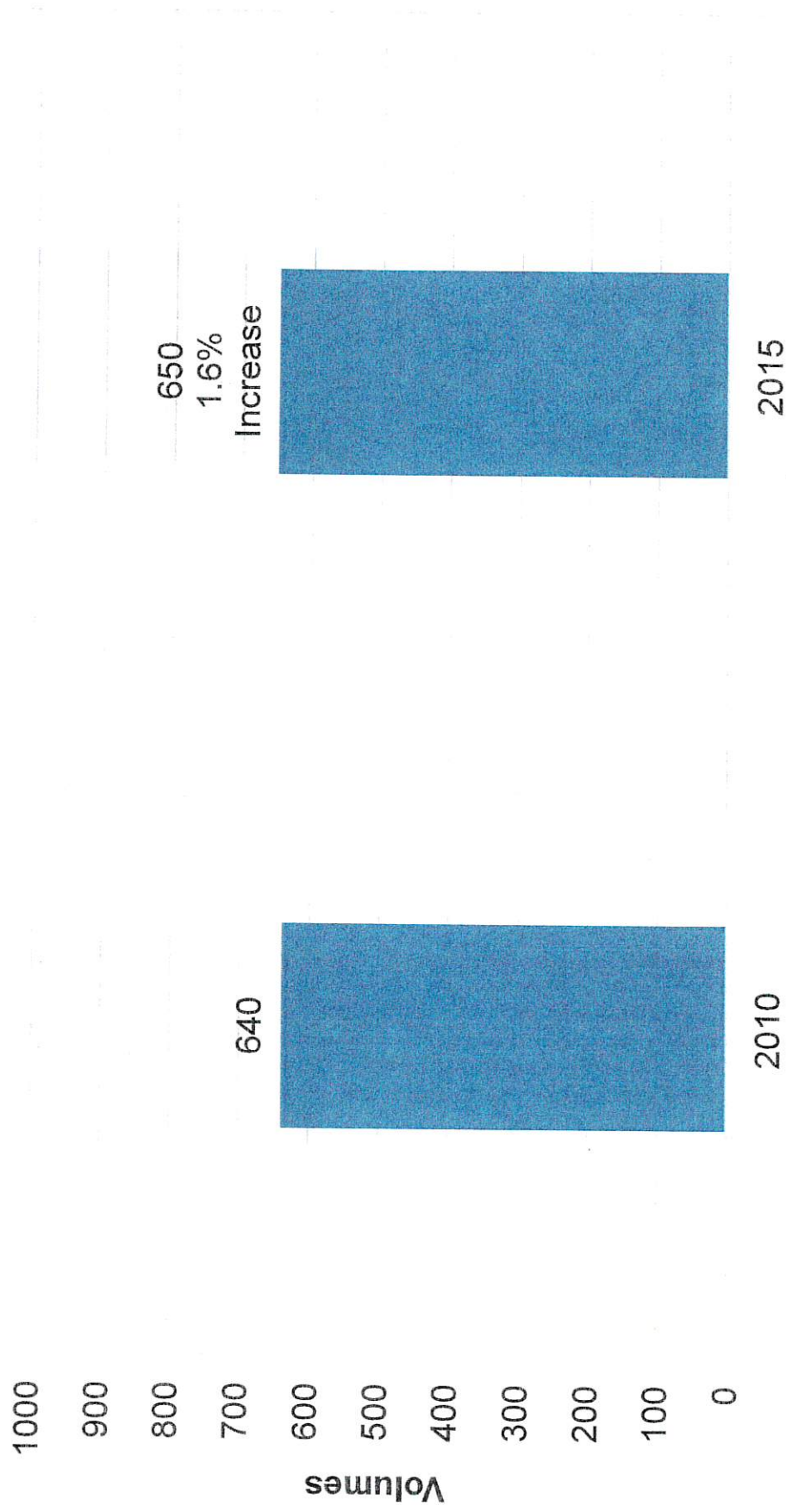
Total Intersection Traffic St. Francis Dr. / Rabbit Rd AM



Total Intersection Traffic St. Francis Dr. / Rabbit Rd PM



Total Intersection Traffic Rabbit Rd / Old Galisteo Rd AM



Total Intersection Traffic Rabbit Rd / Old Galisteo Rd PM

Volumes

1000
900
800
700
600
500
400
300
200
100
0

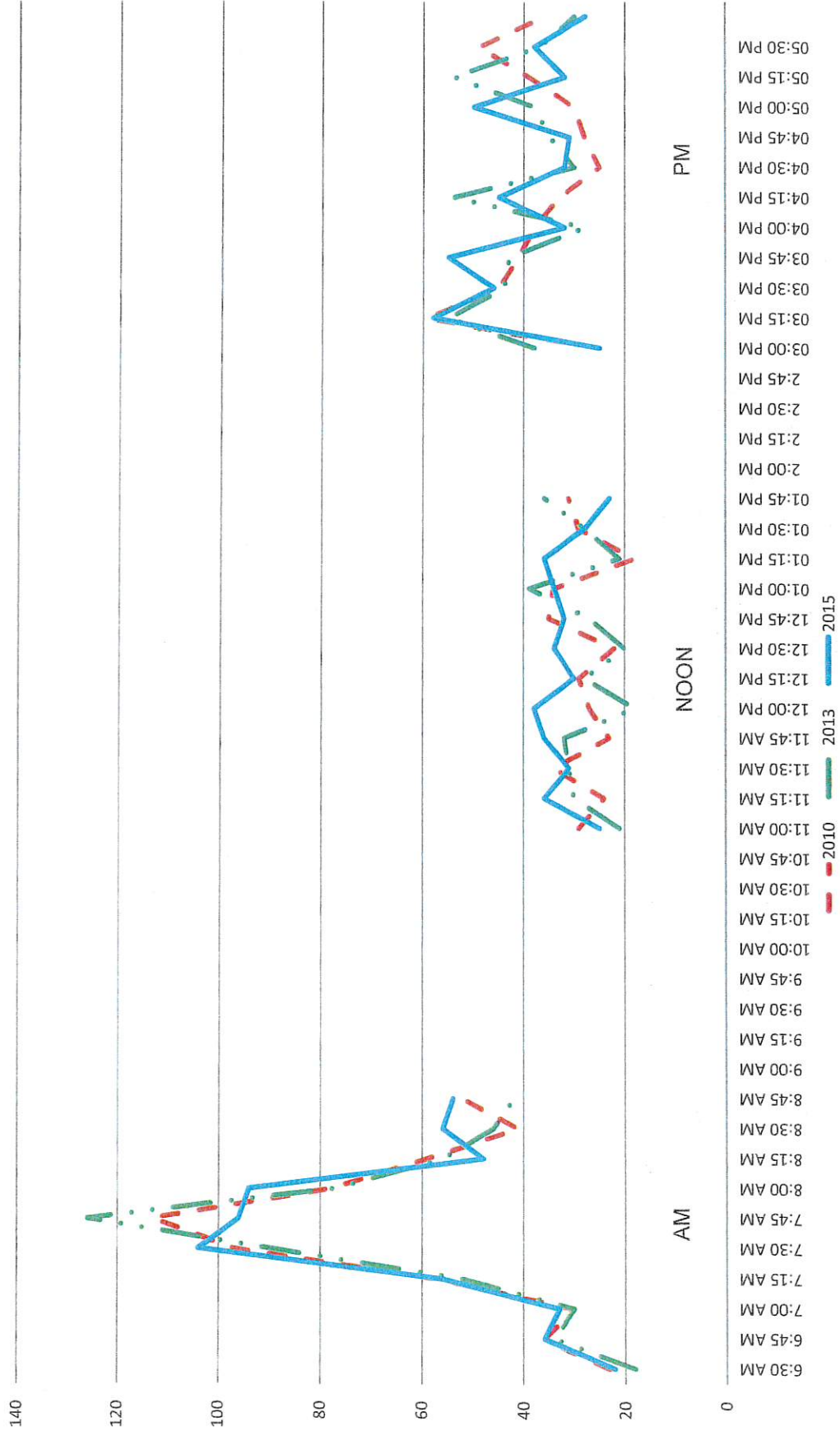
585
10.0%
Increase



2015

2010

**ST FRANCIS DRIVE / RABBIT ROAD
EASTBOUND LEFT
(PEOPLE HEADED TO TOWN FROM W. RABBIT ROAD OR COMMUNITY COLLEGE AREA)
CRITICAL MOVEMENT FOR LEVEL OF SERVICE**



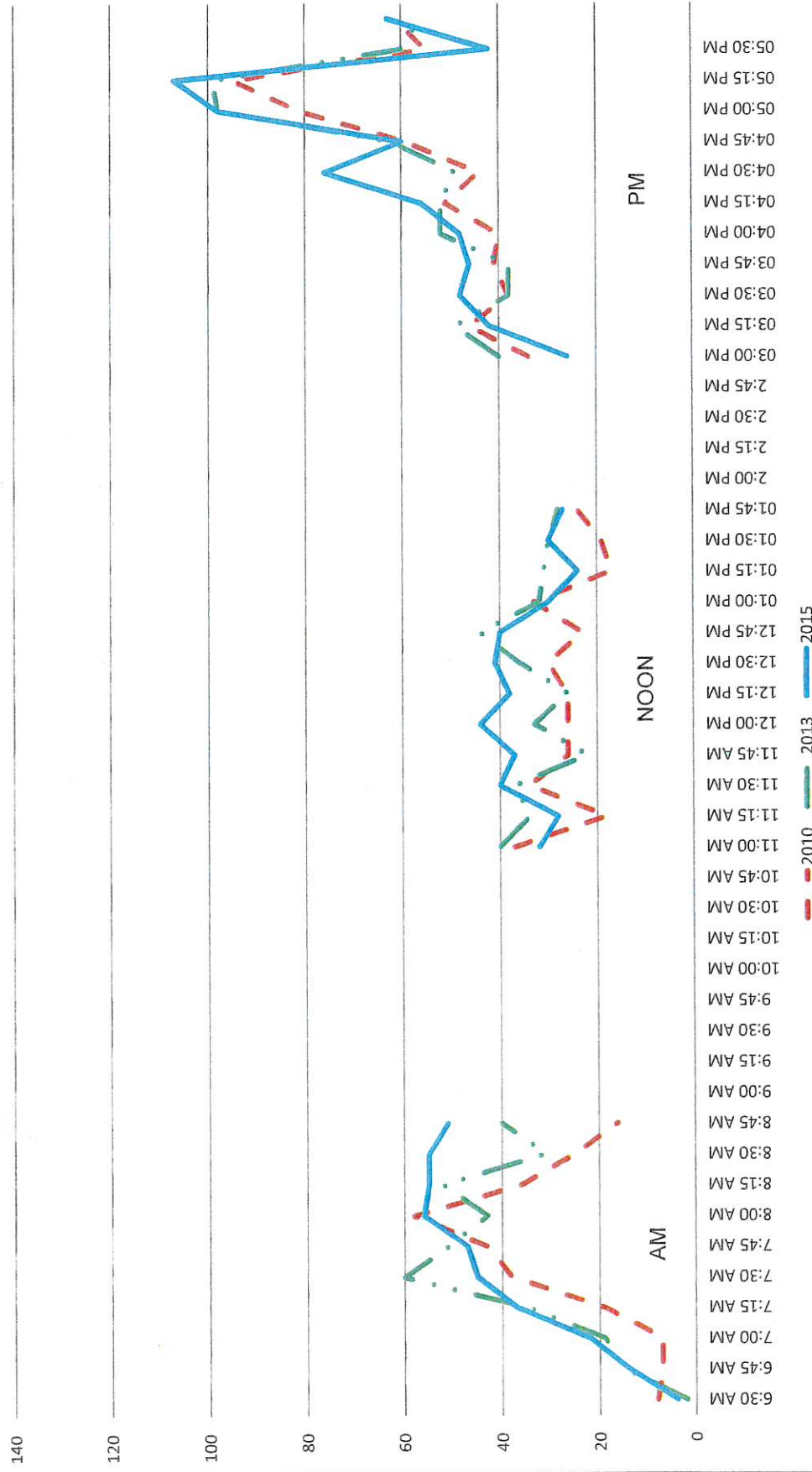
ST FRANCIS DRIVE / RABBIT ROAD

SOUTHBOUND RIGHT

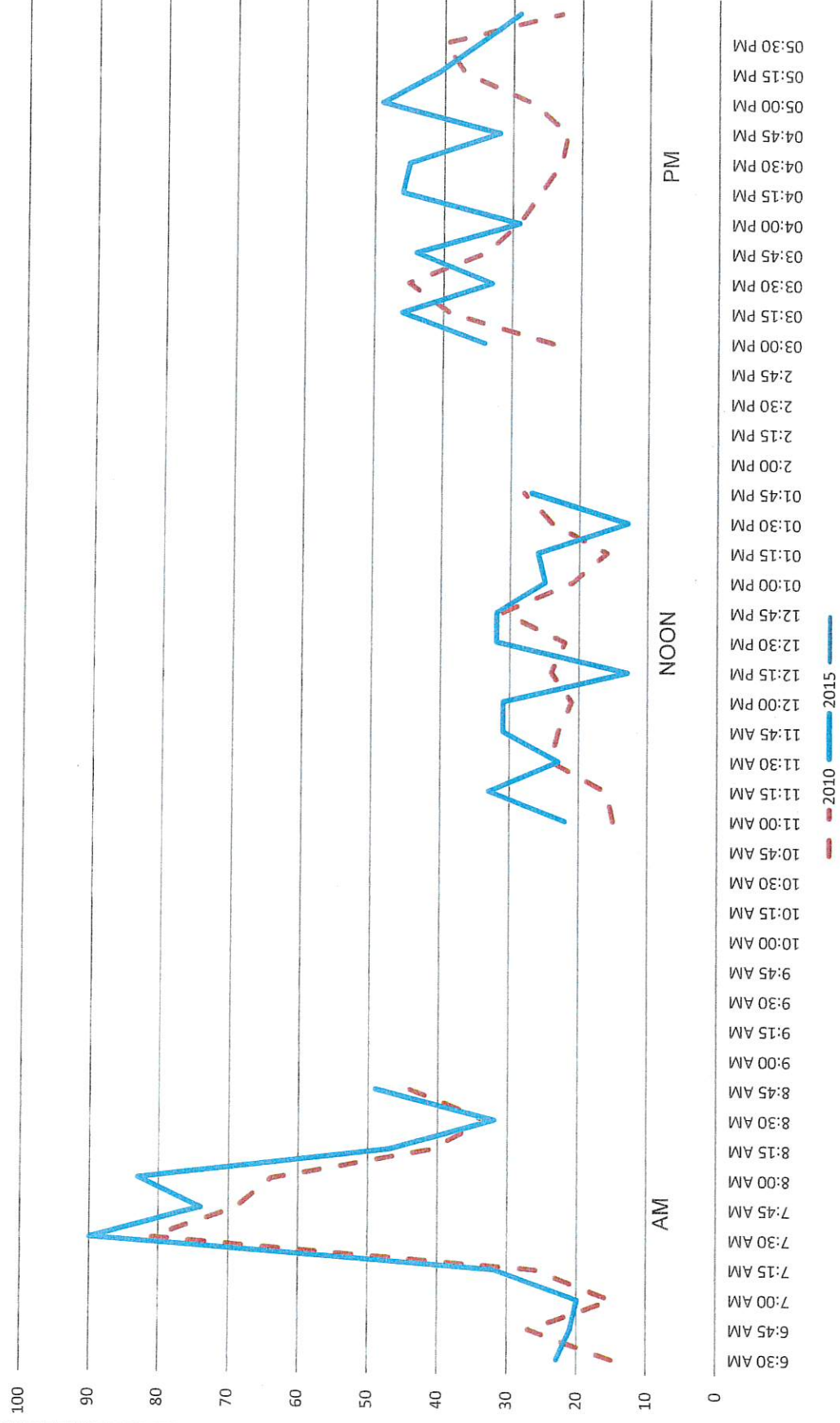
TRAFFIC TOWARD ST. FRANCIS DR. SOUTH AND COMMUNITY COLLEGE

TRAFFIC FAIRLY CONSTANT

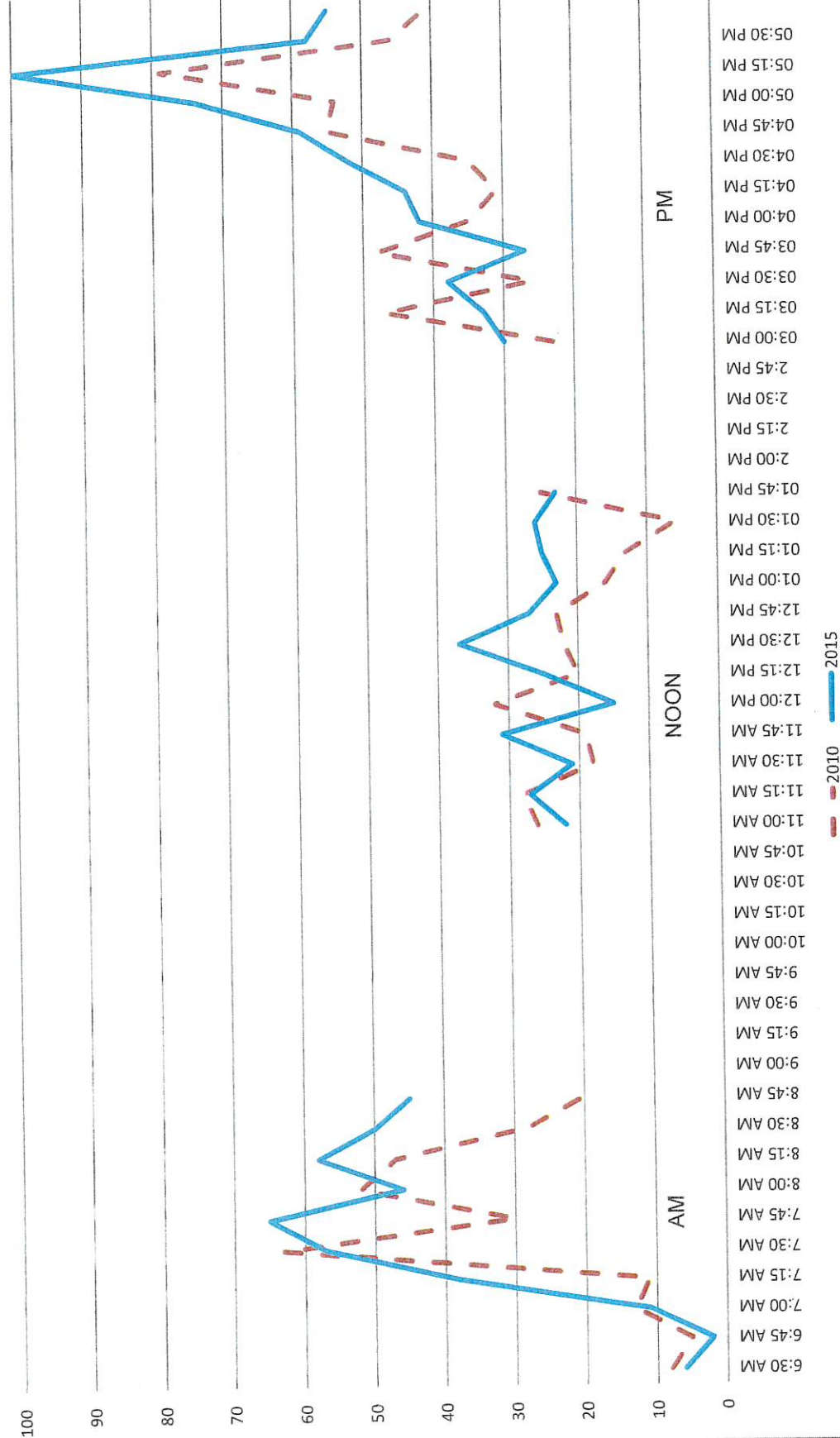
(PEOPLE GOING HOME TO RABBIT ROAD WEST AREA AND SOME TO THE COMMUNITY COLLEGE AREA)



RABBIT ROAD / OLD GALISTEO ROAD EASTBOUND THRU



RABBIT ROAD / OLD GALISTEO ROAD WESTBOUND THRU



Santa Fe Engineering Consultants, LLC

Civil and Traffic Engineering
Construction Management
Land Development

**TRAFFIC IMPACT ANALYSIS
FOR ST. FRANCIS SOUTH
FINAL SUBDIVISION PLAT SUBMITTAL
FOR PHASES 1 AND 2
SANTA FE, NEW MEXICO**

Prepared By:

**Santa Fe Engineering Consultants, LLC
1599 S. St. Francis Drive, Suite B
Santa Fe, New Mexico 87505**

February 2017



EXECUTIVE SUMMARY

Based upon the traffic counts and capacity analysis there are no existing operational deficiencies.

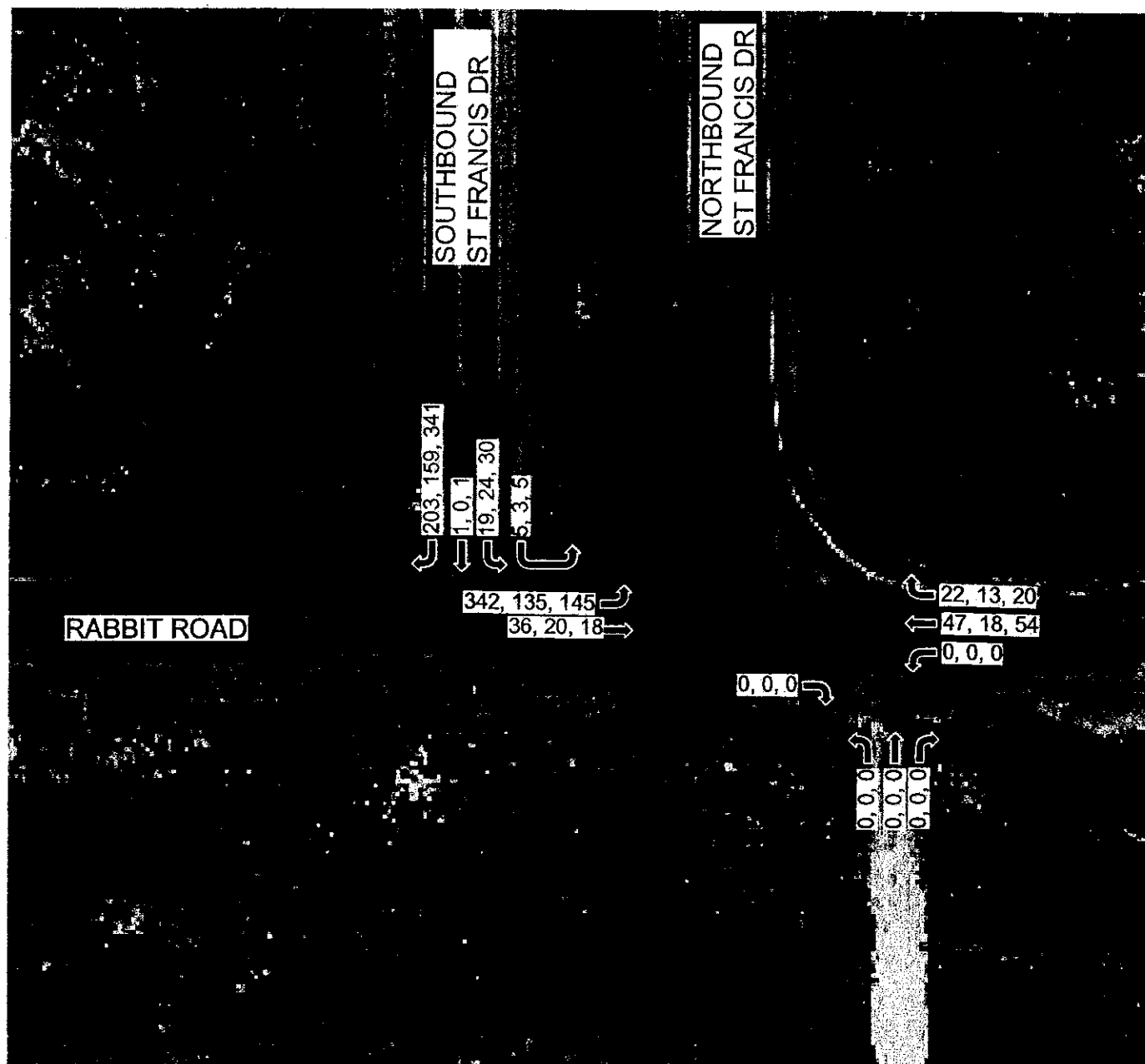
The accident data indicates that there were two accidents between 2012 and 2014 at the South end St. Francis Drive at the intersection with Rabbit Road where St. Francis ends. The NMDOT and the County of Santa Fe are studying the North-East Connector and Rabbit Road. This location should be a part of their considerations.

According to the State Access Management Manual, the operational performance of an urban minor arterial should meet LOS D standards at a minimum. For the implementation year 2018 there are no are deficient Levels of Service.

For the implementation year 2023 there are no are deficient Levels of Service.

A bicycle lane will be constructed along the frontage of the property on Rabbit Road. Non-motorized facilities shall be designed in accordance with the Americans with Disabilities Act and applicable NMDOT standards. Curb ramps shall be provided on urban sections where sidewalk and curb returns exist.

There are no deficient Levels for the horizon year. A traffic signal or roundabout may be required to mitigate these deficiencies. Future studies will be performed as per the conditions of approval.



PEAK HOURS FOR TRAFFIC COUNT DATED SEPTEMBER 23, 2015

AM	NOON	PM
7:15 AM to 8:15 AM	11:15 AM to 12:15 PM	4:30 PM to 5:30 PM

LEGEND

DENOTES PEAK HOUR
TRAFFIC VOLUME
AM, NOON, PM

DENOTES MOVEMENT

11, 23, 25



EXISTING TRAFFIC
VOLUMES
ST. FRANCIS DRIVE
& RABBIT ROAD
FIGURE 10



PEAK HOURS FOR TRAFFIC COUNT DATED
SEPTEMBER 22, 2015

AM	NOON	PM
7:30 AM to 8:30 AM	12:30 PM to 1:30 PM	4:30 PM to 5:30 PM



SCALE: 1" = 50'

LEGEND

DENOTES PEAK HOUR
TRAFFIC VOLUME
AM, NOON, PM

DENOTES MOVEMENT

11, 23, 25



EXISTING TRAFFIC
VOLUMES
RABBIT ROAD & RAIL
TRAIL PARKING LOT
FIGURE 12

File Name: AM_ST. FRANCIS DRIVE and RABBIT ROAD
 Start Date: 9/23/2015
 Peak Hour 7:15 to 8:15 AM
 Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						KADASEE WAY NORTHBOUND						ST. FRANCIS DRIVE SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
7:30 AM	104	9	0	0	0	1	0	18	8	0	0	0	0	0	0	0	0	0	3	0	45	0	0	0	187
7:45 AM	96	10	0	0	0	0	0	11	6	0	0	0	0	0	0	0	0	0	8	0	47	1	0	0	179
8:00 AM	94	13	0	0	0	0	0	8	5	0	0	0	0	0	0	0	0	0	2	1	56	2	0	0	181
8:15 AM	48	4	0	0	0	0	0	10	3	0	0	1	0	0	0	0	0	0	6	0	55	2	0	0	128
Total	342	36	0	0	0	1	0	47	22	0	0	1	0	0	0	0	0	0	19	1	203	5	0	0	675
PHF	0.82	0.69						0.65	0.69										0.59	0.25	0.91				0.90

Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

RABBIT ROAD EASTBOUND				RABBIT ROAD WESTBOUND				KALASEEWAY NORTHBOUND				ST FRANCIS DRIVE SOUTHBOUND				Total				
Start Time	Left	Thru	Right	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds					
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0				
6:30 AM	22	0	0	0	0	1	2	0	0	0	0	0	4	1	0	30				
6:45 AM	36	2	0	0	0	0	3	0	0	0	0	0	14	1	0	58				
7:00 AM	33	1	0	0	0	0	0	0	0	0	0	0	22	0	0	66				
7:15 AM	56	2	0	0	0	0	3	0	0	1	0	0	37	0	0	115				
7:30 AM	104	9	0	0	0	18	8	0	0	0	0	0	45	0	0	187				
7:45 AM	96	10	0	0	0	11	6	0	0	0	0	0	47	1	0	179				
8:00 AM	13	0	0	0	0	8	5	0	0	0	0	0	56	2	0	181				
8:15 AM	48	4	0	0	0	10	3	0	0	0	0	0	55	2	0	128				
8:30 AM	56	3	0	0	0	13	3	0	0	0	0	0	55	1	0	135				
8:45 AM	54	8	0	0	0	15	6	0	0	0	0	0	51	2	0	143				
Total	599	52	0	0	0	1	0	35	47	0	0	1	0	0	42	1	388	9	0	1222

File Name: NOON_ST. FRANCIS DRIVE and RABBIT ROAD
 Start Date: 9/23/2015
 Peak Hour 11:15 AM to 12:15 PM
 Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						KAD SEEWAY NORTHBOUND						ST FRANCIS DRIVE SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
11:30 AM	31	4	0	0	0	0	0	0	0	5	4	0	0	0	0	0	0	0	8	0	0	40	0	0	92
11:45 AM	36	3	0	0	0	0	0	0	9	3	0	0	1	0	0	0	0	0	5	0	37	0	0	0	93
12:00 PM	38	8	0	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	6	0	44	0	0	0	99
12:15 PM	30	5	0	0	0	0	0	0	3	4	0	0	1	0	0	0	0	0	5	0	38	3	0	0	88
Total	135	20	0	0	0	0	0	0	18	13	0	0	2	0	0	0	0	0	24	0	159	3	0	0	372
PHF	0.89	0.63							0.50	0.81									0.75		0.90				0.94

File Name: NOON_ST. FRANCIS DRIVE and RABBIT ROAD

Start Date: 9/23/2015

Start Time: 11:00 AM

Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						KALASIEWAY NORTHBOUND						ST. FRANCIS DRIVE SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
11:00 AM	25	3	0	0	0	0	0	0	8	5	0	0	0	0	0	0	0	0	10	0	32	2	0	0	85
11:15 AM	36	4	0	0	0	2	0	4	4	0	0	0	0	0	0	0	0	0	3	0	28	1	0	0	80
11:30 AM	31	4	0	0	0	0	0	5	4	0	0	0	0	0	0	0	0	0	8	0	40	0	0	0	92
11:45 AM	36	3	0	0	0	0	0	9	3	0	0	1	0	0	0	0	0	0	5	0	37	0	0	0	93
12:00 PM	38	8	0	0	0	0	0	1	2	0	0	0	0	0	0	0	0	0	6	0	44	0	0	0	99
12:15 PM	30	5	0	0	0	0	0	3	4	0	0	1	0	0	0	0	0	0	5	0	38	3	0	0	88
12:30 PM	34	2	0	0	0	0	0	5	2	0	0	0	1	0	0	0	0	0	4	0	41	0	0	0	89
12:45 PM	32	2	0	0	0	0	0	1	6	0	0	0	0	0	0	0	0	0	4	0	40	2	0	0	87
01:00 PM	34	7	0	0	0	0	0	5	6	0	0	0	0	0	0	0	0	0	4	0	30	2	0	0	88
01:15 PM	36	7	0	0	0	0	1	7	9	0	0	0	0	0	0	0	0	0	3	1	24	0	0	0	88
01:30 PM	28	3	0	0	0	0	0	3	7	0	0	0	1	0	0	0	0	0	4	0	30	0	0	0	76
01:45 PM	23	3	0	0	0	0	0	11	3	0	0	0	0	0	0	0	0	0	5	0	27	1	0	0	73
Total	383	51	0	0	0	2	1	62	55	0	0	2	0	0	2	0	0	0	61	1	411	11	0	0	1036

File Name: PM_ST. FRANCIS DRIVE and RABBIT ROAD
 Start Date: 9/23/2015
 Peak Hour 4:30 to 5:30 PM
 Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						KALASEE WAY NORTHBOUND						ST. FRANCIS DRIVE SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
04:30 PM	32	1	0	0	0	0	0	0	0	6	5	0	0	0	0	0	0	0	0	8	0	76	1	0	129
04:45 PM	31	4	0	0	0	0	0	14	3	0	0	0	0	0	0	0	0	0	3	0	60	3	0	0	118
05:00 PM	50	9	0	0	0	0	0	13	6	0	0	0	0	0	0	0	0	0	7	1	98	1	0	0	185
05:15 PM	32	4	0	0	0	0	0	0	21	6	0	0	0	0	0	0	0	0	12	0	107	0	0	2	182
Total	145	18	0	0	0	0	0	54	20	0	0	0	0	0	0	0	0	0	30	1	341	5	0	2	614
PHF	0.73	0.50						0.64	0.83										0.63	0.25	0.80				0.83

File Name: PM_ST. FRANCIS DRIVE and RABBIT ROAD

Start Date: 9/23/2015

Start Time: 03:00 PM

Person Counting: Chris Catanach / Veronica Cordova / Mark Lopez

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						KALISEE WAY NORTHBOUND						ST. FRANCIS DRIVE SOUTHBOUND						Total	
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds		
03:00 PM	25	5	0	0	0	0	0	0	0	7	7	0	0	0	0	0	0	0	0	6	0	26	0	0	77	
03:15 PM	58	4	0	0	0	0	0	0	0	5	5	0	0	0	0	0	0	0	0	2	1	42	0	0	118	
03:30 PM	46	7	0	0	0	0	0	0	0	12	5	0	0	0	0	0	0	0	0	1	1	48	0	0	120	
03:45 PM	55	11	0	0	0	0	0	0	0	3	5	0	0	0	0	0	0	0	0	5	0	46	0	0	125	
04:00 PM	32	5	0	0	0	0	0	0	0	3	3	0	0	0	0	0	0	0	0	3	0	48	2	0	96	
04:15 PM	45	9	0	0	0	0	0	0	0	3	9	0	0	0	0	0	0	0	0	6	0	56	2	0	130	
04:30 PM	32	1	0	0	0	0	0	0	0	6	5	0	0	0	0	0	0	0	0	8	0	76	1	0	129	
04:45 PM	31	4	0	0	0	0	0	0	0	14	3	0	0	0	0	0	0	0	0	3	0	60	3	0	118	
05:00 PM	50	9	0	0	0	0	0	0	0	13	6	0	0	0	0	0	0	0	0	7	1	98	1	0	185	
05:15 PM	32	4	0	0	0	0	0	0	0	21	6	0	0	0	0	0	0	0	0	12	0	107	0	2	182	
05:30 PM	38	6	0	0	0	0	0	0	0	10	7	0	0	0	0	0	0	0	0	9	0	42	1	0	113	
05:45 PM	28	10	0	0	0	0	0	0	0	3	4	0	0	0	0	0	0	0	0	11	0	63	1	0	121	
Total	472	75	0	0	0	0	0	0	0	100	86	0	0	0	0	0	0	0	0	73	3	712	11	0	2	1514

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						RAIL TRAIL PARKING LOT						SOUTHBOUND						Total						
	Left			Thru			Left			Thru			Left			Thru			Left			Thru				Left			Thru		
7:30 AM	0	120	0	0	0	0	1	59	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	180		
7:45 AM	0	102	0	0	0	0	0	74	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	176		
8:00 AM	0	102	0	0	0	0	0	56	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	160		
8:15 AM	0	61	1	0	0	0	2	64	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	128		
Total	0	385	1	0	0	0	3	263	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	0	1	844		

PHF	0.80	0.38	0.85	0.25	0.89
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File Name: AM_RAIL TRAIL PARKING LOT and RABBIT ROAD

Start Date: 9/22/2015

Start Time: 6:30:00 AM

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND			RABBIT ROAD WESTBOUND			RAIL TRAIL PARKING LOT NORTHBOUND			RAIL TRAIL PARKING LOT SOUTHBOUND			Total
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
6:30 AM	0	27	0	0	0	0	0	0	0	0	0	0	33
6:45 AM	0	35	0	0	0	4	0	0	0	0	0	0	39
7:00 AM	0	27	2	0	0	18	0	0	0	0	0	0	47
7:15 AM	0	51	0	0	0	1	47	0	0	0	0	0	99
7:30 AM	0	120	0	0	0	1	59	0	0	0	0	0	180
7:45 AM	0	102	0	0	0	0	74	0	0	0	0	0	176
8:00 AM	0	102	0	0	0	0	56	0	0	0	0	0	160
8:15 AM	0	61	1	0	0	2	64	0	0	0	0	0	128
8:30 AM	0	49	0	0	0	0	55	0	0	0	0	0	104
8:45 AM	0	60	0	0	0	1	58	0	0	0	0	0	119
Total	0	634	3	0	0	5	441	0	0	2	0	0	1085

Person Counting; Chris Catanach

RABBIT ROAD EASTBOUND										RABBIT ROAD WESTBOUND										RAIL TRAIL PARKING LOT NORTHBOUND										SOUTHBOUND									
Start Time	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Total														
12:30 PM	0	32	0	0	0	0	0	1	44	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	3	77													
	0	32	0	0	0	0	0	0	37	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	69													
01:00 PM	0	25	0	0	0	0	0	1	28	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	55													
01:15 PM	0	26	0	0	0	0	0	1	39	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	66													
Total	0	115	0	0	0	0	0	3	148	0	0	0	0	0	0	1	0	0	4	0	0	0	0	0	4	267													

PHF	0.90		0.75	0.84		0.25																				0.87
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Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						RAIL TRAIL PARKING LOT NORTHBOUND						SOUTHBOUND							
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Total	
11:00 AM	0	23	0	0	0	0	0	1	36	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1	61
11:15 AM	0	33	1	0	0	0	0	1	32	0	0	0	0	0	0	2	0	0	0	0	0	0	0	0	0	69
11:30 AM	0	23	0	0	0	0	0	0	31	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6	54
11:45 AM	0	31	0	0	0	0	0	0	39	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3	70
12:00 PM	0	31	0	0	0	0	0	0	23	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	54
12:15 PM	0	13	0	0	0	0	0	0	38	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1	52
12:30 PM	0	32	0	0	0	0	0	1	44	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	3	77
12:45 PM	0	32	0	0	0	0	0	0	37	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	69
01:00 PM	0	25	0	0	0	0	0	1	28	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	55
01:15 PM	0	26	0	0	0	0	0	1	39	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	66
01:30 PM	0	13	0	0	0	0	0	0	37	0	0	0	0	0	0	1	0	0	3	0	0	0	0	0	0	51
01:45 PM	0	27	0	0	0	0	0	0	30	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	57
Total	0	309	1	0	0	0	0	5	414	0	0	0	0	0	0	8	0	0	9	0	0	0	0	0	15	736

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						RAIL TRAIL PARKING LOT NORTHBOUND						RAIL TRAIL PARKING LOT SOUTHBOUND																	
	Left			Thru			Right			Left			Thru			Right			Left			Thru			Right			Left			Thru			Right		
	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds	Utm	Truck	Peds			
04:30 PM	0	65	1	0	0	0	0	0	1	76	0	0	0	0	0	1	0	2	0	0	16	0	0	0	0	0	0	17	146							
04:45 PM	0	37	0	0	0	0	0	0	2	75	0	0	0	0	0	0	0	0	0	2	0	0	0	0	0	0	0	114								
05:00 PM	0	54	0	0	0	0	0	0	1	92	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	147							
05:15 PM	0	50	0	0	0	0	0	0	2	126	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	178							
Total	0	206	1	0	0	0	0	0	6	369	0	0	0	0	0	1	0	2	0	18	0	0	0	0	0	0	18	585								

PHF	0.79	0.25					0.75	0.73			0.25		0.25																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																						
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Person Counting: Chris Catanach

[illegible]

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND				RABBIT ROAD WESTBOUND				OLD GALISTEO ROAD NORTHBOUND				DRIVEWAY SOUTHBOUND				Total			
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn		Truck	Peds	
7:30 AM	0	90	0	0	0	0	2	57	0	0	0	1	4	0	30	0	0	0	0	183
7:45 AM	0	74	1	0	0	0	9	65	0	0	0	0	1	0	28	0	0	0	0	178
8:00 AM	0	83	1	0	0	0	10	46	0	0	0	0	2	0	19	0	0	0	0	161
8:15 AM	0	47	1	0	0	0	6	58	0	0	0	0	2	0	14	0	0	0	0	128
Total	0	294	3	0	0	0	27	226	0	0	0	1	9	0	91	0	0	0	0	650
PHF		0.82					0.68	0.87				0.56		0.76						0.89

File Name: AM_OLD GALISTEO ROAD and RABBIT ROAD

Start Date: 9/22/2015

Start Time: 6:30:00 AM

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						OLD GALISTEO ROAD NORTHBOUND						DRIVEWAY SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
6:30 AM	0	23	0	0	0	0	0	6	0	0	0	0	1	0	4	0	0	0	0	0	0	0	0	0	34
6:45 AM	0	21	0	0	0	0	2	2	0	0	0	0	0	0	14	0	0	0	0	0	0	0	0	0	39
7:00 AM	0	20	0	0	0	0	7	11	0	2	0	0	0	0	7	0	0	0	0	0	0	0	0	0	47
7:15 AM	0	32	0	0	0	0	9	38	0	0	0	0	5	0	19	0	0	0	0	0	0	0	0	0	103
7:30 AM	0	90	0	0	0	2	57	0	0	0	0	1	4	0	30	0	0	0	0	0	0	0	0	0	183
7:45 AM	0	74	1	0	0	0	9	65	0	0	0	0	1	0	28	0	0	0	0	0	0	0	0	0	178
8:00 AM	0	83	1	0	0	0	10	46	0	0	0	0	2	0	19	0	0	0	0	0	0	0	0	0	161
8:15 AM	0	47	1	0	0	0	6	58	0	0	0	0	2	0	14	0	0	0	0	0	0	0	0	0	128
8:30 AM	0	32	0	0	0	0	5	50	0	0	0	0	1	0	16	0	0	0	0	0	0	0	0	0	105
8:45 AM	0	49	0	0	0	0	13	45	0	0	0	0	2	0	11	0	0	0	0	0	0	0	0	0	120
Total	0	471	3	0	0	0	63	378	0	2	0	1	18	0	162	0	0	0	0	1	0	0	0	0	1998

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND				RABBIT ROAD WESTBOUND				OLD GALISTEO ROAD NORTHBOUND				DRIVEWAY SOUTHBOUND				Total			
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn		Truck	Peds	
12:30 PM	0	32	0	0	0	0	0	7	37	0	0	0	3	0	4	0	0	0	0	83
12:45 PM	0	32	0	0	0	0	0	10	27	0	0	0	0	0	9	0	0	0	0	78
01:00 PM	0	25	0	0	0	0	0	5	23	0	0	0	2	0	4	0	0	0	0	59
01:15 PM	0	26	0	0	0	0	0	14	25	0	0	0	1	0	14	0	0	0	0	80
Total	0	115	0	0	0	0	0	36	112	0	0	0	6	0	31	0	0	0	0	300
PHF		0.90						0.64	0.76				0.50		0.55					0.90

File Name: NOON_OLD GALISTEO ROAD and RABBIT ROAD

Start Date: 9/22/2015

Start Time: 11:00 AM

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						OLD GALISTEO ROAD NORTHBOUND						OLD GALISTEO ROAD SOUTHBOUND						Total
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	
11:00 AM	0	22	0	0	0	0	0	14	22	0	0	0	1	0	9	0	0	0	1	0	0	0	0	0	69
11:15 AM	0	33	1	0	1	0	5	27	0	0	0	0	2	0	9	0	0	0	0	0	0	0	0	0	77
11:30 AM	0	23	2	0	0	0	10	21	0	0	0	1	2	0	7	0	0	0	0	0	0	0	0	0	65
11:45 AM	0	31	1	0	0	0	8	31	0	0	0	0	0	0	10	0	0	0	0	0	0	0	0	0	81
12:00 PM	0	31	0	0	0	0	8	15	0	0	0	0	1	0	9	0	0	0	0	0	0	0	0	0	64
12:15 PM	1	13	0	1	0	0	13	25	0	0	0	0	0	0	4	0	0	0	0	0	0	0	0	0	57
12:30 PM	0	32	0	0	0	0	7	37	0	0	0	0	3	0	4	0	0	0	0	0	0	0	0	0	83
12:45 PM	0	32	0	0	0	0	10	27	0	0	0	0	0	0	9	0	0	0	0	0	0	0	0	0	78
01:00 PM	0	25	0	0	0	0	5	23	0	0	0	0	2	0	4	0	0	0	0	0	0	0	0	0	59
01:15 PM	0	26	0	0	0	0	14	25	0	0	0	0	1	0	14	0	0	0	0	0	0	0	0	0	80
01:30 PM	0	13	0	0	0	0	11	26	0	0	0	0	0	0	16	0	0	0	0	0	0	0	0	0	66
01:45 PM	0	27	1	0	0	1	7	23	0	0	0	2	1	0	9	0	0	0	0	0	0	0	0	0	68
Total	1	306	5	1	1	1	112	302	0	0	0	3	13	0	104	0	0	0	1	0	0	0	0	0	847

File Name: PM_OLD GALISTEO ROAD and RABBIT ROAD

Start Date: 9/22/2015

Peak Hour 4:30 to 5:30 PM

Person Counting: Chris Catanach

Start time	RABBIT ROAD EASTBOUND					RABBIT ROAD WESTBOUND					OLD GALISTEO ROAD NORTHBOUND					DRIVEWAY SOUTHBOUND					Total	
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds				
04:30 PM	0	45	1	0	0	0	24	52	0	0	0	1	0	19	0	0	0	1	0	0	0	143
04:45 PM	0	32	1	0	0	0	16	59	0	0	0	1	0	5	0	0	0	0	0	0	0	114
05:00 PM	0	49	2	0	0	1	17	74	1	0	0	1	0	5	0	0	0	0	0	0	0	149
05:15 PM	0	41	2	0	0	0	26	100	0	0	0	1	0	9	1	0	0	0	0	0	0	180
Total	0	167	6	0	0	1	83	285	1	0	0	4	0	38	1	0	0	1	0	0	0	586
PHF		0.85					0.80	0.71	0.25				1.00		0.50			0.25				0.81

File Name: PM_OLD GALISTEO ROAD and RABBIT ROAD

Start Date: 9/22/2015

Start Time: 03:00 PM

Person Counting: Chris Catanach

Start Time	RABBIT ROAD EASTBOUND						RABBIT ROAD WESTBOUND						OLD GALISTEO ROAD NORTHBOUND						DRIVEWAY SOUTHBOUND						Total	
	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds	Left	Thru	Right	Uturn	Truck	Peds		
03:00 PM	0	34	2	0	0	1	13	30	0	0	0	0	0	0	0	8	0	0	0	0	0	0	0	0	87	
03:15 PM	0	46	1	0	0	0	13	33	0	0	0	0	4	1	12	0	0	0	0	0	0	0	0	0	110	
03:30 PM	0	33	0	0	0	0	15	38	0	0	0	0	4	0	7	1	0	0	0	0	1	0	0	0	99	
03:45 PM	1	44	1	0	0	0	10	27	1	1	0	0	2	0	12	0	0	0	0	0	0	0	0	0	99	
04:00 PM	0	29	1	0	0	0	19	42	0	0	0	0	1	0	13	0	0	0	1	0	0	0	0	0	106	
04:15 PM	0	46	1	0	0	1	20	44	0	0	0	0	2	0	12	0	0	0	0	0	0	0	0	0	125	
04:30 PM	0	45	1	0	0	0	24	52	0	0	0	0	1	0	19	0	0	0	1	0	0	0	0	0	143	
04:45 PM	0	32	1	0	0	0	16	59	0	0	0	0	1	0	5	0	0	0	0	0	0	0	0	0	114	
05:00 PM	0	49	2	0	0	1	17	74	1	0	0	0	1	0	5	0	0	0	0	0	0	0	0	0	149	
05:15 PM	0	41	2	0	0	0	26	100	0	0	0	0	1	0	9	1	0	0	0	0	0	0	0	0	180	
05:30 PM	0	35	4	0	0	0	16	58	0	0	0	0	2	0	3	0	0	0	0	0	0	0	0	0	118	
05:45 PM	0	29	1	0	0	1	17	55	0	0	0	0	2	0	11	0	0	0	0	0	0	0	0	0	115	
Total	1	463	17	0	0	4	206	612	2	1	0	0	21	1	116	2	0	0	2	0	1	0	0	0	0	1445